



Kitsap County Department of Community Development

Staff Report and Recommendation Annual Comprehensive Plan Amendment Process for 2019 Downtown Kingston (Phase 2)

Summary

Report Date	12/13/2019
Hearing Date	1/7/2020
Amendment Type	County-sponsored Amendment
Description	<p>The proposed amendment includes the following changes to:</p> <p><u>Kitsap County Code</u></p> <ul style="list-style-type: none"> • Repeal redundant design standards • Define a high capacity transit station area around the Kingston ferry • Create new parking standards for high capacity transit station areas • Update allowed uses for the Kingston UVC zone <p><u>Design Standards for the Community of Kingston</u></p> <ul style="list-style-type: none"> • Improve clarity and administration as well as consistency with other development regulations • Update the applicability of the design standards • Incorporate the Kingston Complete Streets Plan • Designate pedestrian-oriented commercial street frontages • Designate alleys • Add off-site flexibility for some open space/landscaping requirements • Incentivize alternatives to surface parking and count some on-street parking • Increase allowed building heights in the Old Town/Waterfront and Lindvog Commercial design districts from 35-feet to 45-feet • Repeal the required palette of exterior colors <p><u>2016 Kitsap County Comprehensive Plan – Kingston Subarea Plan (Chapter 8)</u></p> <ul style="list-style-type: none"> • Reorganize, update, and retire relevant goals and policies
Geographic Area Affected	Kingston Unincorporated UGA
SEPA	Determination of Non-Significance
Department Recommendation	Adopt as proposed, including the Kingston UVC Workgroup recommendations with the Department’s modifications, additions, and alternatives

This report and recommendation are based on information available at the time of publication. If new relevant and material facts are discovered, this staff report will be revised and the department recommendation may change.

1. Background

The Kitsap County Comprehensive Plan describes the 20-year vision for unincorporated Kitsap County and how that vision will be achieved. The plan covers land use, economic development, environment, housing and human services, transportation, capital facilities and utilities as well as parks, recreation, and open space. The Comprehensive Plan is mandated by the Washington State Growth Management Act (GMA, RCW 36.70A).

A. Authority

The GMA mandates that Kitsap County's Comprehensive Plan and development regulations be reviewed and, if needed, revised at least every 8-years [RCW 90.70A.130(5)]. The most recent Kitsap County 8-year update concluded with the adoption of the 2016 Kitsap County Comprehensive Plan on June 27, 2016 by Ordinance 534-2016. The GMA also mandates that Kitsap County's Comprehensive Plan and development regulations be subject to continuing review and evaluation, allowing for annual amendments to the Comprehensive Plan and development regulations [RCW 36.70A.130(1)].

Kitsap County Code sets forth a process and criteria for making amendments to the Comprehensive Plan [KCC 21.08]. In making amendments, the County must consider:

- Whether the proposed amendments are consistent with and supports other plan elements and or development regulations, and if not, what additional amendments to the plan and/or development regulations will be required to maintain consistency;
- Whether the proposed amendment to the plan and/or regulation will more closely reflect the goals and policies of the Comprehensive Plan;
- Whether the proposed amendment is consistent with the Kitsap County-wide Planning Policies; and
- Whether the proposed amendment complies with the requirements of the GMA.

The revised final docket adopted by the Board of County Commissioners on August 21, 2019 (Resolution No. 133-2019) allows for consideration of this amendment during Kitsap County's annual Comprehensive Plan amendment process for 2019.

B. Proposed Amendment

The proposed amendment is provided in Attachment A, which includes the recommendations of the Kingston UVC Workgroup as modified by all of the Department's recommendations. This amendment expands on and implements the Phase 1 (Kingston UVC) amendment adopted in 2018, is intended to remove regulatory barriers to achieving the existing adopted vision for downtown Kingston, and includes the following substantive changes:

- **Kitsap County Code**
 - Repeal redundant design standards in Chapter 17.480 (UVC Design Criteria) and move non-redundant design standards to Chapter 17.470 (Multifamily Design Criteria) and the Design Standards for the Community of Kingston (Amendments #1 & #2)
 - Define a high capacity transit station area around the Kingston ferry terminal consistent with the assumptions for the proposed VISION 2050 regional growth Plan (Amendment #5)
 - Create new parking standards for high capacity transit station areas with generally reduced parking requirements, increased flexibility for off-site parking, and incentivized alternatives to surface parking (Amendments #6 & #7)
 - Update KCC 17.410.044 (allowed uses) for the Kingston UVC zone (Amendment #8)

- **Design Standards for the Community of Kingston** (all in Amendment #2, Exhibit 1)
 - Revisions to improve the clarity of the design standards, administration of the design standards, and consistency of the design standards with other development regulations (throughout Exhibit 1)
 - Update the applicability of the design standards to: (Introduction, Section A)
 - Remodels and additions
 - Single-family dwellings, duplexes, accessory dwelling units, and accessory living quarters
 - Incorporate the existing adopted Kingston Complete Streets Plan (throughout Exhibit 1)
 - Designate pedestrian-oriented commercial street frontages and require first floors are built for commercial use or eventual conversion to commercial use after initial residential use (Sections II.A.1.a, IV.A.1.c, & IV.B.1.e; Appendix B)
 - Designate alleys and clarify access priorities (Sections II.A.1.a & III.B.1.c; Appendix C)
 - Increase flexibility for some open space and landscaping requirements to be located off-site (Section II.A.4.c)
 - Incentivize alternatives to surface parking and count certain on-street parking toward commercial parking requirements (Sections III.B.2.i & IV.C.1.a)
 - Increase maximum allowed building heights in the Old Town/Waterfront and Lindvog Commercial Design Districts from 35-feet to 45-feet with a required 10-foot upper story setback for buildings over 35-feet high (Sections IV.A.2 & IV.C.1.a)
 - Repeal the required palette of exterior colors (Section IV.D.2; Appendix D)

- **2016 Kitsap County Comprehensive Plan – Kingston Subarea Plan** (Chapter 8)
 - Reorganize, update, and retire goals and policies relevant to the code described above (Amendment #9)

C. Geographic Description

This amendment to the Kitsap County Comprehensive Plan affects the Downtown portion of the Kingston Unincorporated Urban Growth Area.

2. Department Recommendation

Having analyzed the proposed amendment and other alternatives, if applicable, the Department recommends:

- Adoption of the amendment:
 - as proposed above
 - as described in Alternative ___ below
 - with revisions described below
 - with conditions described below
- Deferral of the amendment to a future docket
- Denial of the amendment

A. Revisions

None.

B. Conditions

None.

C. Rational

The primary reasons for considering these amendments are to:

- Implement existing policies in the Kingston Subarea Plan which call for various reviews and updates.
- Address the identified barriers that have stagnated development in downtown Kingston, which are impeding the achievement of the existing vision for a pedestrian-oriented downtown and accommodation of planned growth in Kingston.
- Increase the availability as well as the diversity in size and cost of market rate housing in downtown Kingston
- Improve consistency with regional and countywide planning policies.
- Improve the clarity, administration, and consistency of the Kingston Subarea Plan and development regulations.

3. Other Alternatives Considered

No formal alternatives were proposed during the development of this amendment. The Kingston UVC Workgroup considered preliminary alternatives for many topics as part of their deliberative process but reached consensus on all recommendations and therefore did not propose formal alternatives.

One issue that could be viewed as having formal alternatives is the one instance where the Department's recommendations and the Workgroup's recommendations directly disagree:

- Designation of pedestrian-oriented commercial street frontages (Amendment #2, Exhibit 1 - Section IV.A.1.c and Appendix B)

The Workgroup specifically recommended against designating commercial street frontages for first floor commercial development, even if those commercial spaces could be put first into residential use before eventual conversion to commercial use as Kingston grows. Given existing development patterns in the downtown area, the Workgroup felt the market would likely maintain and build commercial street frontages in at least most of the small core area of downtown without the need for regulations that could be a barrier to development being responsive to market demands like in recent years when mixed-use development had been required throughout the UVC zone. This mixed-use requirement was a barrier to development in the UVC zone and was removed during the Phase 1 amendments in 2018.

The Department agrees with the Workgroup that additional residential development needs to occur in downtown Kingston before the market would likely support much additional commercial development (see Market Assessment and Priority Policy Barriers in Attachment C2 with showing little commercial development; and growth projection for only 23,600 square feet of retail space throughout Kingston between 2013 and 2025 in Attachment C7). The Department has recommended the designation of pedestrian-oriented commercial street frontages as a means to preserve the opportunity for a small core area of pedestrian-oriented commercial streets within the downtown over the long-term while providing flexibility for property owners to maximize occupancy of their buildings.

4. Analysis

Amendments to the Comprehensive Plan must be consistent with the criteria outlined in Kitsap County Code (KCC) Chapter 21.08. Applicable criteria are analyzed below. A summary of the State Environmental Policy Act (SEPA) review of this amendment is located at the end of this section.

A. General Decision Criteria (KCC 21.08.070.A)

For each proposed amendment to the Comprehensive Plan, the Planning Commission in reaching its recommendation, and the Board of Commissioners in making its decision, shall develop findings and conclusions, which demonstrate:

1. *How circumstances related to the proposed amendment and/or the area in which the property affected by the proposed amendment is located have substantially changed since the adoption of the Comprehensive Plan or applicable development regulations;*

Staff Analysis: The Kingston Subarea Plan in the 2016 Comprehensive Plan contains policies (e.g. Policies 36, 37, 41, 42, 50, & 51) to consider updates to the Design Standards for the Community of Kingston, parking requirements, and code provisions to support redevelopment in Kingston. This amendment implements those policies.

The following substantial changes in circumstances are related to this amendment:

- Policy and regulatory barriers were identified by the Kingston UVC Workgroup that have impeded the ability to achieve the existing vision for the development and planned growth applicable to downtown Kingston.
- Staff from the Kitsap County Departments of Community Development and Public Works identified administrative clarifications and improvements to the Kitsap County Code and the Kingston Subarea Plan as well as the need to integrate the Kingston Complete Streets Plan (adopted 8/22/2016) into the Design Standards for the Community of Kingston.
- Revisions of the regional growth strategy proposed in VISION 2050, which is the regional growth plan for Central Puget Sound. Specifically, the designation of the Kingston Unincorporated Urban Growth Area (UGA) as a High Capacity Transit Community assumes a high capacity transit station area will be designated around the Kingston ferry terminal.

2. *How the assumptions upon which the Comprehensive Plan is based are no longer valid, or there is new information available which was not considered during the adoption of, or during the last annual amendment to, the Comprehensive Plan or development regulations; and*

Staff Analysis: See 4.A.1 above.

3. *How the requested redesignation is in the public interest and the proposal is consistent with the Kitsap County Comprehensive Plan.*

Staff Analysis: Not applicable – this is not a redesignation request.

B. Additional Decision Criteria (KCC 21.08.070.B)

In addition to the findings and conclusions above, for each proposed text amendment, the Planning Commission in reaching its recommendation, and the Board of Commissioners in making its decision, shall develop findings and conclusions which consider:

1. *Whether the proposed amendment is consistent with and supports other plan elements and/or development regulations and, if not, what additional amendments to the plan and/or development regulations will be required to maintain consistency;*

Staff Analysis: The proposed amendment increases the consistency of Kitsap County Code with the existing vision for downtown Kingston in the Kingston Subarea Plan, implements Kingston Subarea Plan polices (e.g. Policies 36, 37, 41, 42, 50, & 51) to update various code provisions, and integrates the Kingston Complete Street Plan into code as required by Kingston Subarea Plan policies. The proposed amendment is consistent with and supports other plan elements.

Land capacity estimates based on the proposed amendments indicate the population and employment capacity of the Kingston UGA will not change significantly. The population capacity of the UGA is estimated to increase by 58 people to 2,831 (1.3% below the UGA target). The employment capacity of the UGA is estimated to decrease by 33 jobs to 718 (14.7% above the UGA target). Countywide capacities are estimated to remain within +/- 5% of adopted targets. Kitsap County considers planned capacities within the +/- 5% margin of error of adopted targets to be in balance. While employment capacity is technically outside of the County's margin of error, this amendment will decrease the capacity surplus from 20.3% to 14.7% and the UGA employment capacity is only 56 jobs above the County's margin of error. Therefore, due specifically to the small numbers associated with this surplus capacity, the Department believes that further adjustments to the UGA regarding employment capacity are not warranted at this time and should be reviewed during the next required update of the Comprehensive Plan due in 2024.

The updated land capacity estimates are largely consistent with the assumptions used for the Kitsap County Capital Facilities Plan and will not require new improvements beyond those already planned.

2. *Whether the proposed amendment to the plan and/or regulation(s) will more closely reflect the goals, objectives and policies of the Comprehensive Plan and reflect the local circumstances of the county;*

Staff Analysis: The proposed amendment more closely reflects the goals, objectives, and policies of the Comprehensive Plan, including the Kingston Subarea Plan, and the local circumstances of downtown Kingston. As noted above, many of the amendments implement actions spelled out in existing subarea plan policies and/or address regulatory barriers to achieving the existing vision and planned growth for a pedestrian-oriented downtown in Kingston. The proposed amendment is expected to increase the availability as well as the diversity in size and cost of market rate housing in downtown Kingston.

The proposed amendment also more closely aligns development regulations with the purpose statement of the Kingston UVC zone, which is:

- To foster a development pattern offering direct, convenient pedestrian, bicycle, and vehicular access between residences and businesses, in order to facilitate pedestrian and bicycle travel and reduce the number and length of automobile trips.
- To provide for a compatible mix of single-family, multifamily housing and neighborhood commercial businesses and services, with an emphasis on promoting multi-story structures with commercial uses generally located on the lower floors and residential housing generally located on upper floors.
- To promote a compact growth pattern to efficiently use developable land within UGAs, to enable the cost-effective extension of utilities, services and streets, to enable frequent and efficient transit service, and to help sustain neighborhood businesses.
- To foster the development of mixed use areas that are arranged, scaled and designed to be compatible with surrounding land.

3. *Whether the proposed amendment is consistent with the Kitsap County-wide Planning Policy;*

Staff Analysis: The proposed amendment is consistent with the [Kitsap County-wide Planning Policies](#) (CPPs; adopted on 5/11/2015 by Kitsap County Ordinance 522-2015). The proposed amendment is specifically consistent with the following relevant CPPs:

CW.1 (RE: Growth Patterns)

Roles of Cities and Urban Growth Areas/Urban Communities

- a. The primary role of Kitsap's urban communities is to encourage growth, through new development, re-development and in-fill. Population growth should be directed to Cities, urban growth areas and centers with a transportation system that connects people with jobs and housing.

- b. Each of Kitsap’s urban communities should foster its unique vision as a high quality place to live and work, through urban design, historic preservation, and arts that improve the natural and human-made environments; promote healthy lifestyles; contribute to a prosperous economy; and, increase the region’s resiliency in adapting to changes or adverse events.

...

CCOD.4 (RE: Contiguous, Compatible, and Orderly Development)

Community design and development: Strategies should promote orderly development that reflects the unique character of a community and encourages healthy lifestyles through building and site design and transportation connectivity. In addition, sustainable economic and environmental development techniques should be utilized to enhance the quality of life:

- a. Utilize design strategies to ensure that changes in the built environment provide continuous and orderly development.
- b. Encourage development that reflects unique local qualities and provides an economic benefit to the community.
- c. Design mixed use developments and local street patterns to improve the environment for overall mobility and accessibility to and within the development through multi-modal transportation options that serve all users.
- d. Design of transportation networks should fit within the context of the built and natural environment, enhancing the community, connectivity, and physical activity in the area community wide and specifically in designated growth centers and high transit areas.

...

T.1 (RE: Transportation)

Strategies to optimize and manage the safe use of transportation facilities and services:

...

- d. The County and the Cities should develop and implement access management regulations that provide standards for driveway spacing and delineation, and encourage the joint use of access points where practical.
- e. The County and the Cities shall actively seek opportunities to share facilities, expertise, and transportation resources, such as multiple use park & ride/parking lots or shared traffic signal maintenance responsibility.

T.4 (RE: Transportation)

Recognizing that the County and the Cities each encompass a range of development and density patterns, each jurisdiction shall designate its Centers consistent with the criteria set forth in Element C of the Countywide Planning Policies. The following policies relate to planning guidelines to support transit and pedestrian travel appropriate to each type of urban and rural development or re-development:

- a. The County and the Cities shall each prepare development strategies for their Designated Centers that encourage focused mixed use development and mixed type housing to achieve densities and development patterns that support multi-modal transportation.
- b. In Urban Growth Areas, comprehensive plans should promote pedestrian- and transit oriented development that includes access to alternative transportation and, in the interest of safety and convenience, includes features, such as lighting, pedestrian buffers, sidewalks, and access enhancements for physically challenged individuals.

...

AH.1 (RE: Affordable Housing)

...

d. The County and the Cities should each identify specific policies and implementation strategies in their Comprehensive Plans and should enact implementing regulations to provide a mix of housing types and costs to achieve identified goals for housing at all income levels, including easy access to employment centers.

e. The County and the Cities shall incorporate a regular review of public health, safety, and development regulations pertaining to housing implementation strategies to assure that:

...

ii. regulations are streamlined and flexible to minimize additional costs to housing.

AH.2 (RE: Affordable Housing)

Recognizing that the market place makes adequate provision for those in the upper economic brackets, each jurisdiction should develop some combination of appropriately zoned land, regulatory incentives, financial subsidies, and/or innovative planning techniques to make adequate provisions for the needs of middle and lower income persons.

AH.4.c (RE: Affordable Housing)

Provision of affordable housing for households below 120% countywide median income should include:

...

ii. provision for a range of housing types such as multi-family, single family, accessory dwelling units, cooperative housing, and manufactured housing on individual lots and in manufactured housing parks;

iii. housing design and siting compatible with surrounding neighborhoods;

...

ED.2 (RE: Economic Development)

The role of government agencies in assuring coordinated, consistent efforts to promote economic vitality and equity throughout Kitsap County:

...

b. The County and the Cities shall encourage the full utilization/development of designated industrial and commercial areas. The County and the Cities shall promote revitalization within existing developed industrial and commercial areas to take advantage of the significant investments in existing buildings and infrastructure.

4. *Whether the proposed amendment complies with the requirements of GMA, state and local laws and other applicable inter-jurisdictional policies or agreements; and*

Staff Analysis: The proposed amendment is consistent with the requirements of GMA, state and local laws and other applicable inter-jurisdictional policies and agreements, including specifically the following relevant requirements:

- The Kitsap County Comprehensive Plan must be consistent with the Multicounty Planning Policies (MPPs) adopted by the Puget Sound Regional Council, to which Kitsap County is a member. [see [WAC 365-196-305\(8\)](#)] To be consistent with the following MPP and the planning assumptions for the High Capacity Transit Community designation for the Kingston UGA, the County should establish a High Capacity Transit Station Area within the Kingston UGA around the Kingston Ferry Terminal.

MPP-RGS-1

Implement the Regional Growth Strategy through regional policies and programs, countywide planning policies and growth targets, and local plans.

5. *An explanation of why language should be added to the Comprehensive Plan or why existing language should be modified or deleted.*

Staff Analysis: The proposed amendment to the Comprehensive Plan and Kitsap County Code will:

- Implement the policies of the Kingston Subarea Plan;
- Better achieve the existing vision and planned growth for a pedestrian-oriented downtown in Kingston;
- Increase the availability as well as the diversity in size and cost of market rate housing in downtown Kingston;
- Improve consistency with regional and Countywide planning policies; and
- Improve the administration of the Code, including the Design Standards for the Community of Kingston.

C. State Environmental Policy Act (SEPA)

The Kitsap County SEPA official issued a SEPA threshold determination of non-significance (DNS; Attachment B1) for this amendment after having reviewed the SEPA environmental checklists prepared for this amendment (Attachment B2). The SEPA official's review found that this amendment is not related to or dependent on any of the other proposed Comprehensive Plan amendments and therefore an independent SEPA threshold determination was made regarding this amendment.

Notice of this SEPA threshold determination was:

- Filed with the Washington State Department of Ecology [SEPA Register](#);
- Published in the Kitsap Sun newspaper; and
- Will be integrated with other public announcements.

The SEPA threshold determination and environmental checklist was also distributed to agencies with jurisdiction, the Department of Ecology, affected tribes, and each local agency or political subdivision whose public services would be changed as a result of implementation of the proposal.

5. Public Involvement and Outreach

Kitsap County's public involvement and outreach in support of this amendment has exceeded the requirements of the Growth Management Act (RCW 36.70A) and Kitsap County Code (KCC 21.08).

A. Prior Public Involvement and Outreach

Prior public involvement and outreach regarding the 2019 docket, including this amendment, has included the following:

- An [Online Open House](#) with information about previous, current, and upcoming phases of the 2019 amendment process.
- A public comment period (11/1/2018 – 12/11/2018) and a public hearing by the Kitsap County Board of Commissioners (12/10/2019) while setting the initial docket of amendments. Notifications and announcements regarding this comment period and public hearing included the following:
 - Legal notice published in the Kitsap Sun newspaper (11/30/2018);
 - Broadcast announcements via email, text message, Facebook.com, Twitter.com, and Nextdoor.com; and
 - Formal letters to Tribes with usual and accustomed area in Kitsap County.
- Legal notice announcing the docket of amendments was published in the Kitsap Sun newspaper (1/5/2019).

Prior public involvement and outreach regarding the development of this amendment has included the following:

- The appointment of the Kingston UVC Workgroup in 2018 by Commission Gelder and Workgroup meetings in 2018 and 2019 (see Attachment C2).
- Several meetings with the Kingston stakeholders group, which is coordinated by the Kingston Chamber of Commerce.
- The formal consideration process associated with the Phase 1 amendment in 2018, which set some policy direction for this Phase 2 amendment.

B. Current Public Involvement and Outreach

This staff report provides, in full, the proposed amendment and analysis of the amendment for review by the public and the Kitsap County Planning Commission.

Visit the [Online Open House](http://tinyurl.com/kitsap2019cpa) (<http://tinyurl.com/kitsap2019cpa>) to learn more about the 2019 annual amendment process, important dates and deadlines, and how to participate in the process, including:

- Attending an open house in North Kitsap, Central Kitsap, or South Kitsap.
- Attending Planning Commission meetings.
- Testifying at the Planning Commission's public hearing.
- Submitting written comments.

A new comment period regarding the proposed amendment (Attachment A), this staff report, and the SEPA determination (Attachment B1) opened on the date this report was published. To be included in the official record, written comments must be submitted to the Department of Community Development before the deadline using one of the following methods:

- Entered [online via computer or mobile device](#) (preferred method).
- Emailed to CompPlan@co.kitsap.wa.us.
- Mailed to 614 Division St - MS36, Port Orchard, WA 98366.
- Dropped off at the Permit Center at 619 Division St, Port Orchard.
- Dropped off at one of the scheduled open houses.
- Submitted to the clerk at a scheduled public hearing.

Notifications and announcements regarding this comment period and the Planning Commission's public hearing will include:

- Legal notice published in the Kitsap Sun newspaper.
- Broadcast announcements via email, text message, Facebook.com, Twitter.com, and Nextdoor.com.
- Notice signs posted on site-specific amendment properties.
- Notices mailed to property owners near site-specific amendments and geographically specific amendments covering smaller areas.
- Formal letters to Tribes with usual and accustomed area in Kitsap County.

C. Future Public Involvement and Outreach

Additional public involvement and outreach are anticipated to occur during October through December when the Kitsap County Board of Commissioners will be considering the amendments for adoption.

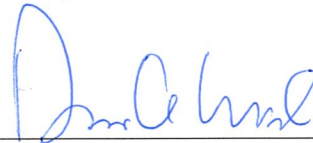
6. **Staff Contact**

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7. **Attachments**

A. Proposed Amendment

- Amendments to Kitsap County Code and the Comprehensive Plan
- Exhibit 1 – Amendments to the Design Standards for the Community of Kingston
- Exhibit 2 – Proposed High Capacity Transit Station Area Map

B. State Environmental Policy Act (SEPA)

1. SEPA Determination
2. SEPA Checklist

C. Supplemental Materials

1. Informational Maps

- Map 1 – Vicinity
- Map 2 – Aerial Photo (2017)
- Map 3A – Critical Areas
- Map 3B – Critical Aquifer Recharge Areas
- Map 4 – Current Land Use Designation Map
- Map 5 – Current Zoning Classification Map
- Legend for Maps 1-3
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- Map 6A – Non-Motorized Existing Facilities
 - Map 6B – Non-Motorized Network with Implementation of Complete Streets Projects
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2. Process Used for Developing the Amendment
 - Development Process Summary
 - Summary of Preliminary Market Assessment
 - Summary of Preliminary Infrastructure Assessment
 - Summary of Preliminary Policy & Regulatory Assessment
 - Workgroup’s List of Potential Barriers and Prioritization Exercise Results
 3. An Introduction: Principle Development Regulations Subject to the 2018 UVC Review and Summary of Existing Development Standards
 4. Summary Comparison of Allowed Uses by Community
 5. Summary Comparison of Parking Standards by Community
 6. Parking Information
 - Article: “People over Parking: Planners are Reevaluating Parking Requirements for Affordable Housing”, in *Planning* (October 2018)
 - Kingston Stakeholders Parking Committee – Historical Summary
 - Kingston Stakeholders – 2007 Downtown Parking Survey Data
 - Kingston Complete Streets – 2015 Parking Study Summary and Data
 7. Retail Projections (2013-2025)
 8. PSRC Planning for Whole Communities Toolkit Excerpts
 - Parking Management
 - Pedestrian-Oriented Design
 9. 2019 Kingston UGA Land Capacity Estimate
 10. Kingston Complete Streets Plan (Not attached due to size. The full report is available online here: <https://www.kitsapgov.com/pw/6APages/Transportation-Planning.aspx>)

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Attachment A

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Amendments

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Readers Guide

Each of the major sections of this document begin with an introduction to help provide context for the reader. Information notes are also provided to the right of certain proposals to provide additional context for the reader. These introductions and informational notes will not be codified.

In the amendments below, the existing language is provided, proposals to remove content are indicated with ~~strikeouts~~, and proposals to add new content are indicated with underlines.

Changes recommended by the Kingston UVC Workgroup, an appointed citizen advisory group, are indicated in blue text. Additional changes recommended by the Department of Community Development, including changes to the Workgroup’s recommendations, are indicated in red text. Where needed for clarity, tables are provided with alternative recommendations.

Kingston Design Standards

Introduction

Currently there are two highly redundant and somewhat conflicting design standards applicable to the UVC zone in KCC 17.480 and in a separate document (adopted by ordinance) titled the Design Standards for the Community of Kingston. In order to clarify the design standards, the following are proposed in this amendment:

- Multi-family design standards in KCC 17.480 that are currently adopted by reference in KCC 17.470 for use in the UCR, UM, and UH zones are preserved by moving them to KCC 17.470. (See Amendments #1.A – 1.D)
- Design standards in KCC 17.480 that are not redundant and do not conflict with the Design Standards for the Community of Kingston are incorporated into the Design Standards for the Community of Kingston. (See Amendment #2.B and Exhibit 1)
- Remove the redundant and conflicting design standards in KCC Chapter 17.480 by repealing the chapter. (See Amendment #1.E)

Additional revisions to the design standards are proposed in Exhibit 1 that:

- Clarify when the design standards are applicable to redevelopment.
- Clarify which standards are applicable to single-family residences, duplexes, accessory dwelling units (ADUs), and accessory living quarters (ALQs).
- Increase the maximum allowed building height to 45-feet and adds mitigation measures to maintain pedestrian scale.
- Incentivize alternatives to surface parking and add more flexibility to parking standards.
- Clarify standards regarding alley access.
- Clarify when some standards are required or suggested.
- Clarify headings and organization to improve readability.

To make the Design Standards for the Community of Kingston easier to access, they are proposed to be added to the Kitsap County Code as an appendix to Title 17 (see Amendment #2).

Amendment #1 – Remove Redundant UVC Design Standards (KCC)

A. Kitsap County Code Section 17.470.020 ‘Applicability – How to use the design criteria’, adopted by Ordinance 534 (2016), is amended as follows:

17.470.020 Applicability – How to use the design criteria.

A. Applicability.

1. The “requirements sections” in the following design criteria apply to each multifamily project requiring conditional use review under Chapter 17.540 or 17.550.

~~2. In addition to the requirements set forth in this chapter, the “requirements sections” set forth in Sections 17.480.160 and 17.480.180 to 17.480.240 shall apply to each multifamily project requiring review under subsection (A) of this section.~~

Commented [PB1]: Informational Note:
These sections are moved into this chapter below and will remain in effect.

1 B. How to Use the Design Criteria. The “requirements sections” state the design criteria
2 that each project shall meet. These design criteria are intended to supplement the
3 development standards of the UCR, UM and UH zones. Where the provisions of this
4 chapter conflict with the provisions of Chapters 17.210 (UCR), 17.220 (UM),
5 and 17.230 (UH), the provisions of the zoning district shall apply. The “guidelines”
6 which follow each requirement statement are suggested ways to achieve the design intent.
7 Each guideline is meant to indicate the preferred conditions, but other equal or better
8 design solutions will be considered acceptable by the director or hearing examiner, so
9 long as these solutions meet the intent of these sections. They are to be applied with an
10 attitude of flexibility, recognizing that each development site and project will have
11 particular characteristics that may suggest that some guidelines be emphasized and others
12 de-emphasized. However, while alternative solutions can be proposed, none of the
13 criteria in the requirement statements can be disregarded.
14

15
16 B. Kitsap County Code Section 17.480.160 ‘Multifamily – Site design – Parking location and
17 design’, adopted by Ordinance 534 (2016), is recodified as Kitsap County Code Section
18 17.470.090.
19

20
21 C. Kitsap County Code Section 17.480.180 ‘Multifamily – Site design – Screening’, adopted by
22 Ordinance 534 (2016), is recodified as Kitsap County Code Section 17.470.100.
23

24
25 D. Kitsap County Code Section 17.480.240 ‘Multifamily – Signs’, adopted by Ordinance 534
26 (2016), is recodified as Kitsap County Code Section 17.470.110.
27

28
29 E. Kitsap County Code Chapter 17.480 ‘Urban Village Center Design Criteria’, last amended by
30 Ordinance 550 (2018), is repealed.
31

32
33 **Amendment #2 – Revise and Publish Kingston Design Standards in Kitsap County Code**
34 **(KCC)**
35

36 A. The Design Standards for the Community of Kingston, adopted by Ordinance 250 (2000), are
37 amended as provided in Exhibit 1.
38

39
40 B. NEW SECTION. The Design Standards for the Community of Kingston, as amended in
41 Section A above, are added (as a linked PDF document in its original formatting) as
42 Appendix C1 to Kitsap County Code Chapter 17.700 ‘Appendices’ as follows.
43

44 Appendix C1 - Design Standards for the Community of Kingston
45
46

1 **Amendment #3 – Update Internal References to the Design Standards (KCC)**
2

3 A. Kitsap County Code Section 17.260.020 ‘Uses permitted and design standards’, last amended
4 by Ordinance 550 (2018), is revised as follows:
5

6 **17.260.020 Uses permitted and design standards.**

- 7 A. Uses Permitted: Section 17.410.044, Commercial, industrial, parks and public
8 facility zones use table.
9 B. Design Standards: Section 17.420.054, Commercial, industrial, and parks zones
10 density and dimensions table.
11 1. Density;
12 2. Lot dimensions;
13 3. Lot coverage standards;
14 4. Height regulations;
15 5. Setbacks.
16 C. Chapter 17.105, Interpretations and Exceptions.
17 D. Chapter 17.440, Master Planning.
18 E. Chapter 17.450, Performance Based Development.
19 ~~F. Chapter 17.480, Urban Village Center Design Criteria.~~
20 ~~F G.~~ Chapter 17.490, Off-Street Parking and Loading.
21 ~~G H.~~ Chapter 17.500, Landscaping.
22 ~~H I.~~ Chapter 17.510, Sign Code.
23 ~~I J.~~ Chapter 17.580, Transfer of Development Rights.
24
25

26 B. Kitsap County Code Section 17.410.050 ‘Footnotes for zoning use tables’, last amended by
27 Ordinance 550 (2018), is revised as follows:
28

29 **17.410.050 Footnotes for zoning use tables.**

- 30 A. Where noted on the preceding use tables, the following additional restrictions apply:
31 ...
32 30. The Design Standards for the Community of Kingston sets forth policies and
33 regulations for properties within the downtown area of Kingston. All development within
34 this area must be consistent with these standards in KCC 17.700.C1. ~~A copy of the~~
35 ~~Design Standards for the Community of Kingston may be referred to on the Kitsap~~
36 ~~County web page or at the department of community development front counter.~~
37

38 *[Note: This amendment is shown in an abbreviated form to keep this document concise.*
39 *The rest of the footnotes in this section will be inserted, without further amendment, into*
40 *the final ordinance prior to adoption.]*
41
42
43

- 1 C. Kitsap County Code Section 17.420.060 ‘Footnotes for tables’, last amended by Ordinance
2 559 (2018), is revised as follows:

3
4 **17.420.060 Footnotes for tables.**

- 5 A. Where noted on the preceding tables, the following additional provisions apply:

6 ...

7 5. The Design Standards for the Community of Kingston sets forth policies and
8 regulations for properties within the downtown area of Kingston. All development within
9 this area must be consistent with these standards [in KCC 17.700.C1](#). ~~A copy of the~~
10 ~~Design Standards for the Community of Kingston may be referred to on the Kitsap~~
11 ~~County web page or at the department of community development front counter.~~

12
13 *[Note: This amendment is shown in an abbreviated form to keep this document concise.*
14 *The rest of the footnotes in this section will be inserted, without further amendment, into*
15 *the final ordinance prior to adoption.]*

16
17
18 **Amendment #4 – Move Definitions from Design Standards to Title 17 - Zoning (KCC)**

- 19
20 A. Kitsap County Code Section 17.110.570 ‘Parking space, compact’, adopted by Ordinance
21 534 (2016), is renumbered as Section 17.110.567.

- 22
23
24 B. NEW SECTION. A new section is added to Kitsap County Code Chapter 17.110
25 ‘Definitions’, as follows:

26
27 **KCC 17.110.568 Pedestrian-oriented facade.**

28 “Pedestrian-oriented facade” means the ground floor frontage of a building design, which
29 offers an interesting appearance to attract pedestrian interest in the locality and
30 encourages pedestrian access.

- 31
32
33 C. NEW SECTION. A new section is added to Kitsap County Code Chapter 17.110
34 ‘Definitions’, as follows:

35
36 **KCC 17.110.569 Pedestrian-oriented space/plaza.**

37 “Pedestrian-oriented space/plaza” means the area between a building and a public street
38 or pedestrian path that promotes visual and pedestrian access onto the site and that
39 provides amenities and landscaping to enhance the public’s use of the space for passive
40 activities, such as resting, reading, picnicking, and window shopping. The area should be
41 visible from the public right-of-way and accessible to pedestrians, including those with
42 handicaps.

1 D. NEW SECTION. A new section is added to Kitsap County Code Chapter 17.110
2 'Definitions', as follows:

3
4 **KCC 17.110.570 Pedestrian-friendly street.**

5 "Pedestrian-friendly street" means any street designed for safe use by both pedestrians
6 and vehicles. A pedestrian-friendly street includes sidewalks or walkways, landscaping,
7 lighting, and other street amenities benefiting pedestrians.
8

9
10 E. NEW SECTION. A new section is added to Kitsap County Code Chapter 17.110
11 'Definitions', as follows:

12
13 **KCC 17.110.571 Pedestrian walkways.**

14 "Pedestrian walkways" means formal standardized public walkways and informal paths
15 worked into a site's landscape design that provide a means for pedestrians to travel
16 through the community along street sidewalks or other public routes.
17

18
19 F. NEW SECTION. A new section is added to Kitsap County Code Chapter 17.110
20 'Definitions', as follows:

21
22 **KCC 17.110.697 Streetscape.**

23 "Streetscape" means the visual and functional supporting elements of a roadway design
24 that provide aesthetic interest and comfort to the pedestrian. Street amenities serve to
25 define the public space of a sidewalk as well as the adjacent roadway corridor.
26 Pedestrian amenities include pedestrian-oriented plazas, furniture, lighting, and art.
27
28
29

Parking Standards

Introduction

“Pedestrian-oriented” is the central theme of the existing adopted vision for downtown Kingston. With regarding to parking, this means:

- Surface parking is minimized.
- Parking is largely located underground, in parking garages, or shared-use lots.
- A substantial amount of parking could be provided and managed through a future public parking management program, parking improvement district, or other public entity.
- A critical mass of residential and commercial uses are supported by high capacity transit and a high proportion of bicycle and pedestrian modes for short trips, thus requiring less parking.

The proposed amendments are intended to revise current parking standards, which were generally not crafted for a pedestrian-oriented downtown, so they better align with the existing vision for downtown Kingston.

Kingston will be designated a High Capacity Transit Community in the Central Puget Sound regional growth management plan, called VISION 2050, which is intended to coordinate growth management across Kitsap, Pierce, Snohomish, and King Counties. High capacity transit is a key element of the plan’s framework and most of downtown Kingston is within a high capacity transit station area, which is the area within a half-mile of the ferry terminal. The proposed amendments also intend to incorporate the high capacity transit framework into the County’s parking standards, which would be immediately applicable to Kingston and could be applicable to other specified areas of unincorporated Kitsap County as high capacity transit is planned and implemented in the future.

Amendment #5 – Define High Capacity Transit Station Areas (KCC)

A. NEW SECTION. A new section is added to Kitsap County Code Chapter 17.110 ‘Definitions’, as follows:

KCC 17.110.340 High Capacity Transit Station Area.

“High capacity transition station areas” include only those portions of urban growth areas within:

A. One-half mile of the following public ferry terminals:

1. Kingston – Washington State Ferry and Kitsap Transit Fast Ferry (see boundary in KCC 17.700.E1).

B. NEW SECTION. The Kingston High Capacity Transit Station Area Map, provided in Exhibit 2, is added (as a linked PDF document in its original formatting) as Appendix E1 to Kitsap County Code Chapter 17.700 ‘Appendices’ as follows:

Appendix E1 – High Capacity Transit Station Area - Kingston

1
2
3 **Amendment #6 – Location of Parking Facilities & On-Street Parking (KCC)**
4

5 A. Kitsap County Code Section 17.490.020 ‘General provisions’, adopted by Ordinance 540
6 (2016), is amended as follows:

7
8 **17.490.020 General provisions.**

9 A. Parking analyses shall be provided for all proposed uses as outlined on relevant
10 permit application checklists.

11
12 B. More Than One Use on One or More Parcels. In the event several uses occupy a
13 single structure or parcel of land, the total requirements for off-street parking shall be the
14 sum of the requirements of the several uses computed separately. If the director finds that
15 a portion of the floor area not less than a contiguous one hundred square feet in a retail
16 store will be used exclusively for storage of merchandise which is not being displayed for
17 sale, he may deduct such space in computing parking requirements, but the owners shall
18 not thereafter use the space for any other purpose without furnishing additional off-street
19 parking as required by Section 17.490.030.

20
21 C. Joint Use of Facilities. The off-street parking requirements of two or more uses,
22 structures, or parcels of land may be satisfied by the same parking or loading space used
23 jointly, if approved by the director, to the extent that it can be shown by the owners or
24 operators of the uses, structures, or parcels that their operations and parking needs do not
25 overlap in point of time. ~~If the uses, structures, or parcels are under separate ownership,~~
26 ~~the right to joint use of the parking space must be evidenced by a deed, lease, contract, or~~
27 ~~other appropriate written document to establish the joint use.~~ For joint use facilities (i.e.
28 shared-use parking and shared access facilities), a parking agreement shall be required
29 consistent with KCC 17.490.020(I).
30
31

1 D. Location of Parking Facilities.
2

Workgroup Recommendation	<p><u>1. In all zones (except the Urban Village Center), off-street parking spaces for dwellings shall be located on the same lot with the dwelling. Other required parking spaces shall be located on the same parcel or on another parcel not farther than three hundred feet from the building or use they are intended to serve, measured in a straight line from the building. Off-premise parking arrangements must be noticed in the title of the development site and off-premise parking site and obtaining such arrangement is the sole responsibility of the applicant.</u></p> <p><u>2. In the Urban Village Center zone, required parking spaces shall be located on the same parcel or on another parcel no farther than eight hundred feet from the building or use they are intended to serve, measured in a straight line from the building. Off-premise parking arrangements must be noticed in the title of the development site and off-premise parking site and obtaining such arrangement is the sole responsibility of the applicant.</u></p>
DCD Recommendation	<p><u>1. Within high capacity transit station areas, required parking spaces shall be located on the same parcel or on another parcel no farther than eight hundred feet from the building or use they are intended to serve, measured in a straight line from the main entrance of the building.</u></p> <p><u>2. In all other areas, off-street parking spaces for dwellings shall be located on the same lot with the dwelling. Other required parking spaces shall be located on the same parcel or on another parcel not farther than three hundred feet from the building or use they are intended to serve, measured in a straight line from the building the main entrance of the building.</u></p> <p><u>3. Off-site parking shall be connected to the building or use they are intended to serve by streets improved with sidewalks or by walkways.</u></p> <p><u>4. For off-site parking, a parking agreement shall be required consistent with KCC 17.490.020(I).</u></p>

Commented [PB2]: Informational Note:
In a high-capacity transit and pedestrian-oriented downtown, parking is intentionally limited and managed to reflect lower car ownership, less land area for parking, and more trips completed through walking and biking instead of moving cars short distances. Therefore, off-site parking can be further from its associated use. 800 feet is approximately 2.5 blocks in downtown Kingston.

3
4 E. Use of Parking Facilities. Required parking space shall be available for the parking
5 of operable passenger automobiles of residents, customers, patrons, and employees only,
6 and shall not be used for the storage of vehicles or materials, or for the parking of trucks
7 used in conducting the business or use.
8

9 F. Parking in Required Front, Side, Rear Yards or Setbacks. Unless otherwise provided,
10 required parking and loading spaces shall not be located in a required yard or setback,
11 except for development of single-family dwellings or duplexes. Automobile sales may be
12 allowed in no more than twenty-five percent of the front yard setback, as shown on an
13 approved site plan.
14

15 G. Off-site Employee Parking. Off-site employee parking may be used to reduce the
16 number of on-site parking spaces. ~~Off site parking for employees shall be evidenced by a
17 deed, lease, contract or other appropriate written document.~~
18
19

Commented [PB3]: Informational Note:
This sentence replaced by subsection D.4 in the DCD recommended alternative above.

1 H. Development of and Maintenance Standards for Off-Street Parking Areas. In
2 addition to requirements of Chapters 17.490 and 17.500 and the Kitsap Stormwater
3 Design Manual, every parcel of land hereafter used as a public or private parking area,
4 including commercial parking lots, shall be developed as follows:

- 5 1. An off-street parking area for more than five vehicles shall be effectively
6 screened by a sight-obscuring fence, hedge, or planting, on each side that adjoins
7 property situated in any residential zone, or the premises of any school or like
8 institution;
- 9
10 2. Lighting shall be directed away from adjoining properties. Not more than one
11 foot candle of illumination shall leave the property boundaries;
- 12
13 3. Except for single-family and duplex dwellings, groups of more than two parking
14 spaces shall be so located and served by a driveway that their use will require no
15 backing movements or other maneuvering within a street or right-of-way other than
16 an alley;
- 17
18 4. Areas used for standing and maneuvering of vehicles shall have durable and
19 dustless surfaces maintained adequately for all-weather use, and so drained as to
20 avoid flow of water across sidewalks;
- 21
22 5. Except for parking to serve residential uses, parking and loading areas adjacent
23 to or within residential zones or adjacent to residential uses shall be designed to
24 minimize disturbance of residents;
- 25
26 6. Service drives to off-street parking areas shall be designed and constructed to
27 facilitate the flow of traffic, to provide maximum safety of traffic ingress and egress,
28 and to provide maximum safety of pedestrians and vehicular traffic on the site. The
29 number of service drives shall be limited to the minimum that will allow the property
30 to accommodate and service the traffic to be anticipated. Service drives shall be
31 clearly and permanently marked and defined through the use of rails, fences, walls, or
32 other barriers or markers on frontage not occupied by service drives. Service drives
33 to drive-in establishments shall be designed to avoid backing movements or other
34 maneuvering within a street, other than an alley;
- 35
36 7. Service drives shall have a minimum vision clearance area formed by the
37 intersection of the driveway centerline, the street right-of-way line, and a straight line
38 joining said lines through points twenty feet from their intersection;
- 39
40 8. Parking spaces along the outer boundaries of a parking area shall be contained by
41 a curb or bumper rail so placed to prevent a motor vehicle from extending over an
42 adjacent property line, pedestrian walkway, or a street; and
- 43
44 9. When the parking standards require ten or more parking spaces, up to thirty
45 percent of these may be compact car spaces, as identified in Section 17.490.040.
46 Compact spaces shall be clearly labeled on the parking space.

1
2 10. Parking for bicycles should be provided at a ratio of one space per ten vehicle
3 spaces, and shall be required at a ratio of one space per twenty vehicle spaces.
4 Bicycle facilities shall be adjacent to buildings and protected from weather.
5

6 I. Parking Agreement.

7 1. For off-site parking, shared-use parking, or shared access to parking, a covenant,
8 easement or other contract approved by the director for shared parking and/or access
9 between the cooperating property owners shall be enacted and recorded by the
10 County with the County Auditor as a deed restriction on all associated properties
11 (i.e. the property with the use and the property providing the required parking) that
12 cannot be modified or revoked without the approval of the director. The parking
13 agreement shall:

14 a. Provide that the land comprising the required parking facilities shall not be
15 encroached upon, used, sold, leased, or conveyed for any purpose except in
16 conjunction with the building or use which the required parking serves;

17
18 b. For commercial uses, provide for directional signage to off-site public or
19 visitor parking.

20
21 c. Assign maintenance provisions for the parking facilities and landscaping;

22
23 d. If shared use is allowed, indicate prime hours of operation for shared uses;

24
25 e. If shared use is allowed, designate potential times of overflow, and a parking
26 plan which will be implemented in the event of overflow.

27
28 2. If any of the above requirements are violated, the affected property owners must
29 provide the full amount of required off-street parking for each use, in accordance
30 with conditions of approval, unless a satisfactory alternative remedy is approved by
31 the director.

Commented [PB4]: Informational Note:
This new provision is adapted from the King County Metro
Right Size Parking Model Code.

1

Workgroup Rec.	J. On-Street Parking Areas. In the Urban Village Center zone, on-street parking spaces within the right-of-way adjacent to the lot may be counted to satisfy the minimum off-street parking requirements for commercial development, as approved by the Department of Public Works.
DCD Rec.	[DCD recommends moving this proposed language, with minor edits, to Chapter III, Section B.2 (Parking) of the Design Standards for the Community of Kingston (see Exhibit 1) where it would be applicable to all three of the downtown Kingston design districts instead of just the UVC zone.]

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Amendment #7 – Number of Spaces Required (KCC)

Kitsap County Code Section 17.490.030 ‘Number of spaces required’, last amended by Ordinance 541 (2017), is amended as follows:

17.490.030 Number of spaces required.

Off-street parking spaces shall be provided as follows:

<i>Note: These 2 columns contain existing language (with proposed changes).</i>		<i>Note: These 2 columns provide alternatives for what would be 1 additional column in this table.</i>	
		Workgroup Rec.	DCD Rec.
Land Use	Minimum Parking Spaces Required in all Zones (Except as Modified to the Right)	Urban Village Center Zone Modifications	High Capacity Transit Station Area Modifications
Residential			
Single-Family (attached or detached)	During subdivision, 2 per unit + 0.5 per unit on street or set aside; for historical lots or lots with no standing requirement, 3 per unit. 1 additional space for accessory dwelling units or accessory living quarters. Garages are not calculated towards any parking requirement.	2 per unit. 1 additional space per guest house, accessory dwelling unit or accessory living quarter. Garages are calculated towards parking requirement.	2 per unit. 1 additional space per guest house, accessory dwelling unit or accessory living quarter. Garages are calculated towards parking requirement.

Commented [PB5]: Informational Note:
In a high-capacity transit and pedestrian-oriented downtown, parking is intentionally limited and managed to reflect lower car ownership and less land area for parking. Parking spaces in garages would be counted like they are for multi-family housing.

Multifamily (Condos/Townhouses/Apartments) and Cottage Housing	1.5 per unit + 0.5 per unit on street or set aside	<u>1 space per dwelling unit that is a studio or 1 bedroom unit, and 2 spaces for all other dwelling units.</u>	<u>Units with 1 or fewer bedrooms: 1 space per unit + 0.5 spaces per unit set aside;</u> <u>Units with 2 or more bedrooms: 1.5 spaces per unit + 0.5 spaces per unit set aside</u>
Senior Housing	0.5 per unit; 1 per on-duty employee		
Institutional/Educational/Other			
Bed and Breakfast	1 per sleeping unit		
Motels and Hotels	1 per bedroom; and spaces to meet the combined requirements of the uses being conducted such as hotels, restaurants, auditoriums, etc.		
Club/Lodges	Spaces to meet the combined requirements of the uses being conducted such as hotels, restaurants, auditoriums, etc.		
Hospitals and Institutions	1 per bed; 1 per 2 employees; 1 per 2 guests		
Places of Worship	1 per 4 seats or 8 feet of bench length in the main auditorium		
Library and Gallery	1 per 250 gross square feet		
Preschool-Kindergarten	1 per employee; 1 per 6 children		
Elementary/Middle or Junior High School	1 per employee; 2 per classroom		
High School	1 per employee and teacher; 1 per 10 students		
Colleges, Technical School	1 per 3 seats in classroom; 1 per employee and teacher		
Stadium, Arena, Theater	1 per 4 seats or 8 feet of bench length in the main auditorium		

Commented [PB6]: Informational Note:
Differentiates parking by unit size to reflect car ownership patterns and encourage a mix of unit sizes, diversify housing inventory, and make smaller units more affordable.

Commented [PB7]: Informational Note:
Specifies a portion of parking as set aside to encourage shared parking spaces and improve parking lot efficiency.

Bowling Alley	6 per alley		
Dance Hall, Skating Rink	1 per 200 gross square feet		
Self Storage	1 per 3,000 gross square feet		
Commercial/Retail/Office			
Restaurants/Bars/ Taverns	If under 5,000 square feet of gross floor area – 1 per 200 square feet of gross floor area; If 5,000 or more square feet of gross floor area – 20 plus 1 per each additional 200 square feet of gross floor area	1 per 400 square feet of gross floor area	1 per 400 square feet of gross floor area
Retail stores generating relatively little automobile traffic (e.g., appliance, furniture, hardware and repair stores)	1 per 400 square feet of gross floor area		1 per 800 square feet of gross floor area
Retail and personal service establishments generating heavy automobile traffic (e.g., department, drug, and auto parts stores, fitness centers, supermarkets, ice cream parlors, bakeries and beauty and barber shops)	1 per 200 square feet of gross floor area	1 per 400 square feet of gross floor area	1 per 400 square feet of gross floor area
Espresso Stands, Drive-In, and Fast Food Restaurants	1 per 80 square feet of gross floor area		
Professional Office	1 per 300 square feet of gross floor area	1 per 400 square feet of gross floor area	1 per 400 square feet of gross floor area
Shops and stores for sales, service	1 per 600 square feet of gross floor area		

or repair of automobile, machinery and plumbing, heating, electrical and building supplies			
Mortuaries, Funeral Homes, Crematories	1 per 75 square feet of assembly area	<u>1 per 400 square feet of gross floor area</u>	<i>[Note: DCD recommends keeping as is]</i>
Medical and Dental Office or Clinic	1 per 200 square feet of gross floor area	<u>1 per 300 square feet of gross floor area</u>	<u>1 per 300 square feet of gross floor area</u>
Bank, Financial Institutions	1 per 400 square feet of gross floor area		
Industrial			
Marinas and Moorage Facilities	1 per 4 moorage slips		
Warehouse, Storage, and Wholesale Facilities	1 per 2 employees; 1 per company vehicle parked on site at night (if applicable); 1 per 300 square feet of office space	<u>1 per 2 employees; 1 per company vehicle parked on site at night (if applicable); 1 per 400 square feet of gross floor area</u>	<u>1 per 2 employees; 1 per company vehicle parked on site at night (if applicable); 1 per 400 square feet of office space</u>
Manufacturing, Research, Testing, Processing and Assembly Facilities	1 per 1,000 square feet		
Winery/Brewery	1 per 800 square feet of gross floor area		

Commented [PB8]: Informational Note:
Changes the Working Group's proposal from "gross floor area" to "office space".

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- 1 A. ~~Relaxation of Deviation from~~ Required Spaces.
 2 1. The director may authorize a reduction up to twenty-five percent to the amount of
 3 required ~~off-street~~ parking if a project proponent demonstrates that, due to the unusual
 4 nature of the proposed use, it is reasonable that the ~~off-street~~ parking required by this
 5 section exceeds any likely need, or that trip demand reduction programs or public transit
 6 availability serves to further reduce parking demand.
 7

Workgroup Rec.	<p>2. <u>In all zones (except the Urban Village Center) an increase over ten percent or a reduction greater than twenty-five percent from the minimum parking ratio shall be processed pursuant to Chapter 17.560.</u></p> <p>3. <u>In the Urban Village Center zone, a reduction greater than twenty-five percent from the minimum parking ratio may be granted by the director if the reduction is supported by a parking and traffic impact statement and the development:</u></p> <p><u>a. Is within a seven-minute walk to a regional transit or ferry terminal and transit-supportive elements are provided (for example, participation in Kingston Ride or other Kitsap Transit program, covered bus stop or rideshare waiting area, covered bike parking, or car share program offered to residents); or</u></p> <p><u>b. Includes construction of a structured parking facility (e.g. an under building or multi-level parking garage); or</u></p> <p><u>c. Utilizes spaces in a joint-use structured parking facility; or</u></p> <p><u>d. Utilizes underground parking to achieve the off-street parking requirement.</u></p>
DCD Rec.	<p>2. An increase over ten percent or a reduction greater than twenty-five percent from the minimum required parking ratio shall be processed pursuant to Chapter 17.560- except in high capacity transit station areas a reduction greater than twenty-five percent may be granted by the director if the reduction is supported by a parking and traffic impact analysis and the development:</p> <p><u>a. Provides a car share program, shuttle program, or regional transit pass/subsidy program to all residents that is adequate to offset the parking reduction;</u></p> <p><u>b. Implements recorded lease/deed restrictions that limit the combined total number of vehicles owned by tenants to the number of parking spaces available for tenants;</u></p> <p><u>c. Participates in a public parking management program or a parking improvement district with adequate capacity to offset the parking reduction;</u></p> <p><u>d. Implements an alternative Transportation Management Plan with measures adequate to offset the parking reduction that has been approved by the director and recorded on the title of the affected properties; or</u></p> <p><u>c. Substantially replaces on-site surface parking with parking underground and/or in a structured parking facility (e.g. an under building or multi-level parking garage) located on-site and/or, if otherwise allowed, off-site.</u></p>

Commented [PB9]: Informational Note:
 This new provision is adapted from the Workgroup recommendation and the King County Metro Right Size Parking Model Code.

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9

1 B. Other Uses.

2 1. Other uses not specifically listed above shall furnish parking as required by the
3 director. The director shall use the above list as a guide for determining requirements for
4 said other uses.

5 2. Storage of junk motor vehicles is subject to the provisions of Section 17.105.090(I).

6

Use Table

Introduction

The following proposed amendments to the types of uses allowed within the UVC zone are intended to support the existing adopted vision of a diverse and vibrant pedestrian-oriented downtown and support the incremental transition of downtown over time consistent with that vision.

Amendment #8 – Allowed Uses in the UVC Zone (KCC)

[Note: It may be difficult to visually identify changes in this table. To assist the reader find all of the changes, this proposal affects only the following uses in the UVC column: 100, 102, 124, 126, 208, 210, 220, 238, 262, 274, 282, 294, 308, 508, 514, and 536.]

A. Kitsap County Code Section 17.410.044 ‘Commercial, industrial, parks, and public facility zones use table’, last amended by Ordinance 574 (2019), is amended as follows:

17.410.044 Commercial, industrial, parks, and public facility zones use table.

Comprehensive Plan Land Use Designation →	Urban High Intensity Commercial		Urban Low Intensity Commercial			Rural Commercial	Urban Industrial			Rural Industrial	Public Facilities	
	C (19)(30) (48)(57) (101)	RC (19)(48) (57)(88) (101)	UVC (30) (48)(57) (101)	NC (19)(30) (48)(57) (101)	LIC (48)(57) (101)	RCO (12)(64) (101)	BC (31)(42) (101)	BP (101)	IND (32)(42) (101)	RI (12)(42) (101)	P (101)	(Reserved)
RESIDENTIAL USES												
100	Accessory dwelling units (1)	--	--	--P	--	--	--	--	--	--	--	--
102	Accessory living quarters (1)	--	--	--P	--	--	--	--	--	--	--	--
104	Accessory use or structure (1)(51)	P	P(84)	P	P	P	P	P	P	P	P	--
106	Adult family home	ACUP P	ACUP P	ACUP P	-	ACUP P	ACUP P	ACUP P	ACUP P	ACUP P	ACUP P	--

Comprehensive Plan Land Use Designation →		Urban High Intensity Commercial		Urban Low Intensity Commercial			Rural Commercial	Urban Industrial			Rural Industrial	Public Facilities	
Zoning Classification →	Categorical Use	C (19)(30) (48)(57) (101)	RC (19)(48) (57)(88) (101)	UVC (30) (48)(57) (101)	NC (19)(30) (48)(57) (101)	LIC (48)(57) (101)	RCO (12)(64) (101)	BC (31)(42) (101)	BP (101)	IND (32)(42) (101)	RI (12)(42) (101)	P (101)	(Reserved)
		(41)	(41)(84)	(41)		(41)(79)	(41)	(41)	(41)	(41)	(41)		
108	Bed and breakfast house or vacation rental	--	--	ACUP C (34)	ACUP C (34)	ACUP (79)	ACUP C (34)	--	--	--	--	--	
109	Boarding house (102)	P (99)	P (99)	P (99)	P (99)	P (99)	P (99)	--	--	--	--	ACUP (99)	
110	Caretaker's dwelling	ACUP	ACUP (84)	ACUP	ACUP	ACUP	P	P	P	P	P	P	
112	Convalescent home or congregate care facility (97)	ACUP	ACUP (84)	ACUP	C	ACUP -- (79)	--	--	--	--	--	--	
114	Cottage housing developments	--	--	ACUP	--	--	--	--	--	--	--	--	
116	Dwelling, duplex	--	--	ACUP P	P	--	--	--	--	--	--	--	
118	Dwelling, existing	P	P	P	P	P	P	P	P	P	P	--	
120	Dwelling, multifamily	ACUP	ACUP C (85)	ACUP	P	P -- (79)	--	--	--	--	--	--	
122	Dwelling, single-family attached	ACUP	ACUP (84)	P	P	P -- (79)	--	--	--	--	--	--	
124	Dwelling, single-family detached (includes manufactured homes)	--	--	-- P	P	--	--	--	--	--	--	--	
126	Guest house (1)	--	--	-- P	--	--	--	--	--	--	--	--	
127	High-Risk Secured Facility (1)	C	C					C	C	C			
128	Home business (1)(53)	--	--	P	ACUP	--	ACUP	--	--	--	--	--	
130	Hotel/motel	P	P (84)	ACUP	C	ACUP -- (79)	--	--	--	--	--	--	

Comprehensive Plan Land Use Designation →		Urban High Intensity Commercial		Urban Low Intensity Commercial			Rural Commercial	Urban Industrial			Rural Industrial	Public Facilities	
Zoning Classification →	Categorical Use	C (19)(30) (48) (57) (101)	RC (19)(48) (57)(88) (101)	UVC (30) (48)(57) (101)	NC (19)(30) (48)(57) (101)	LIC (48)(57) (101)	RCO (12)(64) (101)	BC (31)(42) (101)	BP (101)	IND (32)(42) (101)	RI (12)(42) (101)	P (101)	(Reserved)
132	Mobile homes	--	--	--(43)	--	--	--	--	--	--	--	--	
134	Residential care facility	ACUP	ACUP (84)	ACUP	--	ACUP -- (79)	--	--	--	--	--	--	
COMMERCIAL/BUSINESS USES													
200	Accessory use or structure (1)(51)	P	P	P	P	P	P	P	P	P	P	P	
202	Adult entertainment (1)	C	C (84)	--	--	--	--	C	--	C	--	--	
204	Ambulance service	P	P (84)	C	C	P	--	P	ACUP	ACUP	--	--	
206	Auction house (55)	P	P (84)	ACUP	--	P	C	ACUP	ACUP	P	C	--	
208	Auto parts and accessory stores (65)	P	P (84)	P	P	P (83)	C	--	--	--	--	--	
210	Automobile rentals	P	P (61)(84)	P (56) ACUP	P (56)	P (83)	--	--	--	--	--	--	
				[Note: DCD recommends keeping as is with footnote clarification below]									
212	Automobile repair and car washes (65)	P	P (84)	--	ACUP (54)	P (83)	C	P (61)	ACUP	P (33)	C	--	
214	Automobile service station (6)	P	P (61)(84)	--	ACUP	P (79)(83)	C	C (33)	C (33)	P (33)	C	--	
216	Automobile, recreational vehicle or boat sales	ACUP	ACUP (84)	--	--	P (83)	--	ACUP (35)	--	ACUP (35)	--	--	
218	Nonmotorized recreation rentals (95)	P	P	P	P	P	P	--	--	--	--	P	
220	Boat/marine supply stores	P	P (84)	P	--	P (83)	C	--	--	--	--	--	

Comprehensive Plan Land Use Designation →		Urban High Intensity Commercial		Urban Low Intensity Commercial			Rural Commercial	Urban Industrial			Rural Industrial	Public Facilities	
Zoning Classification →	Categorical Use	C (19)(30) (48)(57) (101)	RC (19)(48) (57)(88) (101)	UVC (30) (48)(57) (101)	NC (19)(30) (48)(57) (101)	LIC (48)(57) (101)	RCO (12)(64) (101)	BC (31)(42) (101)	BP (101)	IND (32)(42) (101)	RI (12)(42) (101)	P (101)	(Reserved)
222	Brew pubs	P	P C (85)(87)	ACUP	ACUP	P	--	ACUP (33)	ACUP (33)	ACUP	--	--	
224	Clinic, medical	P	P (87)	ACUP	ACUP	P	--	P	ACUP	C	--	--	
226	Conference center	P	P C (85)	P	--	P	--	--	--	--	--	ACUP	
228	Custom art and craft stores	P	P C (85)(87)	P (54)	P (54)	P	C	--	--	--	--	--	
230	Day-care center (14)	P	P C (85)	P (54)	P (54)	P -- (79)	ACUP	P (33)	P (33)	P (33)	--	ACUP -- (79)	
232	Day-care center, family (14)	P	P (61)(84)	ACUP (54)	ACUP (54)	P -- (79)	--	P (33)(61)	P (33)	--	--	--	
234	Drinking establishments	C	C (87)	ACUP	C	P	C	P (33)	C (33)	--	--	--	
236	Engineering and construction offices	P	P (84)	P (54)	P (54)	P	ACUP	P	P (33)	P (33)	ACUP (72)	--	
238	Espresso stands (58)(72)	P	P (61)(84)	P	P	P	ACUP	P (33)(61)	P (33)	P (33)	ACUP	--	
240	Equipment rentals	P	P (61)(84)	ACUP	--	--	ACUP	P	P	P	ACUP (73)	--	
242	Farm and garden equipment and sales	P	P (61)(84)	--	--	P	ACUP	--	--	--	C	--	
244	Financial, banking, mortgage and title institutions	P	P C (85)(87)	P (54)	P (54)	P	--	P	P (33)	ACUP (33)	--	--	
245	Fitness center	P	P	C	--	--	P (100)	P (100)	--	P (100)	P (100)	--	
246	General office and management services – less than 4,000 s.f.	P	P	P	P	P	ACUP	P	P	P (33)	--	--	
248	General office and management	P	P (84)	ACUP	ACUP	P	C	P	P	--	--	--	

Comprehensive Plan Land Use Designation →		Urban High Intensity Commercial		Urban Low Intensity Commercial			Rural Commercial	Urban Industrial			Rural Industrial	Public Facilities	
Zoning Classification →	Categorical Use	C (19)(30) (48) (57) (101)	RC (19)(48) (57)(88) (101)	UVC (30) (48)(57) (101)	NC (19)(30) (48)(57) (101)	LIC (48)(57) (101)	RCO (12)(64) (101)	BC (31)(42) (101)	BP (101)	IND (32)(42) (101)	RI (12)(42) (101)	P (101)	(Reserved)
	services – 4,000 to 9,999 s.f.												
250	General office and management services – 10,000 s.f. or greater	P	P (84)	ACUP	--	P	--	P	P	--	--	--	
252	General retail merchandise stores – less than 4,000 s.f.	P	P	P	P	P	ACUP	P (33)	P (33)	ACUP (33)	--	--	
254	General retail merchandise stores – 4,000 to 9,999 s.f.	P	P (84)	ACUP	ACUP	P	C	--	--	--	--	--	
256	General retail merchandise stores – 10,000 to 15,000 s.f.	P	P (84)	C	--	--	--	--	--	--	--	--	
258	General retail merchandise stores – 15,001 to 24,999 s.f.	P	P (84)	C	--	--	--	--	--	--	--	--	
260	General retail merchandise stores – 25,000 s.f. or greater	ACUP (62)	ACUP (62)(84)	--	--	ACUP	--	--	--	--	--	--	
262	Kennels or pet day cares (1)	C	C (61)(84)	C	C	C	C	P	ACUP	ACUP	C	--	
264	Kennels, hobby	--	--	P	P	--	--	--	--	--	--	--	
266	Laundromats and laundry services	P	P (84)	P (54)	P (54)	P	--	P (33)	P	ACUP	--	--	
268	Lumber and bulky building material sales	ACUP (42)	ACUP (42)(61)(84)	--	--	ACUP (42)	C	P (61)	--	P	ACUP	--	
270	Mobile home sales	ACUP	ACUP (61)(84)	--	--	--	--	--	--	--	--	--	
272	Nursery, retail	P	P (84)	ACUP	ACUP	P	ACUP	--	--	--	--	--	
274	Nursery, wholesale	P	P (61)(84)	ACUP --	ACUP	P	P	--	--	--	P	--	
276	Off-street private parking facilities	P	P C (85)	ACUP	ACUP	--	--	--	--	--	--	--	
278	Personal services – skin care, massage, manicures, hairdresser/barber (66)	P	P (87)	P (54)	P (54)	P	ACUP (54)	--	--	--	--	--	

Comprehensive Plan Land Use Designation →		Urban High Intensity Commercial		Urban Low Intensity Commercial			Rural Commercial	Urban Industrial			Rural Industrial	Public Facilities	
Zoning Classification →		C (19)(30) (48)(57) (101)	RC (19)(48) (57)(88) (101)	UVC (30) (48)(57) (101)	NC (19)(30) (48)(57) (101)	LIC (48)(57) (101)	RCO (12)(64) (101)	BC (31)(42) (101)	BP (101)	IND (32)(42) (101)	RI (12)(42) (101)	P (101)	(Reserved)
Categorical Use													
280	Pet shop – retail and grooming	P	P (84)	ACUP	ACUP	P	ACUP (54)	--	--	--	--	--	
282	Research laboratory	--	--	-- ACUP	--	--	--	P	P	P	C	--	
284	Restaurants	P	P ACUP (85)	P (54)	P (54)	P	C	P (33)	C (33)	ACUP (33)	--	--	
286	Restaurants, high-turnover (33)	P	P (63) (84)	ACUP	C	P	--	P	P	P	--	--	
288	Recreational vehicle rental	ACUP	ACUP (61)(84)	--	--	--	--	ACUP (61)	ACUP	ACUP	--	--	
290	Temporary offices and model homes (27)	--	--	--	--	--	--	--	--	--	--	--	
292	Tourism facilities, including outfitter and guide facilities	P	P	P	P	P	ACUP	P	P	ACUP	--	--	
294	Tourism facilities, including seaplane and tour boat terminals	ACUP	ACUP (84)	-- C	--	--	C	--	--	--	--	--	
296	Transportation terminals	ACUP	ACUP C (85)	C	C	C	--	P	--	ACUP	--	--	
298	Veterinary clinics/animal hospitals	P	P (84)	ACUP	ACUP	P	ACUP	P	ACUP	ACUP	--	--	
RECREATIONAL/CULTURAL USES													
300	Accessory use or structure (1)(51)	P	P	P	P	P	P	P	P	P	P	P	
302	Amusement centers	ACUP (11)	ACUP (11)	C (11)	C	ACUP (11) -- (79)	--	--	--	C (11)	--	ACUP	
304	Carnival or circus	ACUP (11)	ACUP (11)(61) (84)	ACUP (11)	C	ACUP (11) -- (79)	--	--	--	ACUP (11)	--	ACUP	
306	Club, civic or social (12)	P	P	ACUP	ACUP	P	C	ACUP	--	ACUP	--	ACUP	

Comprehensive Plan Land Use Designation →		Urban High Intensity Commercial		Urban Low Intensity Commercial			Rural Commercial	Urban Industrial			Rural Industrial	Public Facilities	
Zoning Classification →	Categorical Use	C (19)(30) (48)(57) (101)	RC (19)(48) (57)(88) (101)	UVC (30) (48)(57) (101)	NC (19)(30) (48)(57) (101)	LIC (48)(57) (101)	RCO (12)(64) (101)	BC (31)(42) (101)	BP (101)	IND (32)(42) (101)	RI (12)(42) (101)	P (101)	(Reserved)
			ACUP (85)										
308	Golf courses	ACUP	ACUP (61)(84)	ACUP	ACUP	--	--	--	--	--	--	ACUP	
310	Marinas	ACUP	ACUP (61)(84)	C	ACUP	--	C	--	--	C	C	ACUP	
312	Movie/performance theaters, indoor	P	P ACUP (85)	P	ACUP	P	--	--	--	--	--	--	
314	Movie/performance theaters, outdoor	C	ACUP	ACUP	--	C	C	C	ACUP	--	--	C	
316	Museum, galleries, aquarium, historic or cultural exhibits (67)	P	P C (85)	P	ACUP	P	C	P	ACUP	--	--	ACUP	
318	Parks and open space	P	P	P	P	P	P	P	P	P	P	P	
392	Race track, major	C	C (61)(84)	--	--	--	--	C (61)	C	C	--	C (12)	
322	Race track, minor	--	--	--	--	--	--	--	--	C	--	C (12)	
324	Recreational facilities, private	ACUP	ACUP	ACUP	ACUP	ACUP	C	P	C	C	--	ACUP	
326	Recreational facilities, public	ACUP	ACUP	ACUP	ACUP	P	ACUP	P	C	C	--	ACUP	
328	Recreational vehicle camping parks	C	--	--	C	--	--	--	--	--	--	ACUP	
330	Zoo	C	C (61)(84)	--	--	C	--	--	--	--	--	--	
INSTITUTIONAL USES													
400	Accessory use or structure (1)(51)	P	P	P	P	P	P	P	P	P	P	P	
402	Government/public structures	ACUP	ACUP	ACUP	ACUP	ACUP	ACUP	P	P	P	C	P	
404	Hospital	ACUP	ACUP (84)	C	--	ACUP	--	C	C	C	--	--	

Comprehensive Plan Land Use Designation →		Urban High Intensity Commercial		Urban Low Intensity Commercial			Rural Commercial	Urban Industrial			Rural Industrial	Public Facilities	
Zoning Classification →	Categorical Use	C (19)(30) (48)(57) (101)	RC (19)(48) (57)(88) (101)	UVC (30) (48)(57) (101)	NC (19)(30) (48)(57) (101)	LIC (48)(57) (101)	RCO (12)(64) (101)	BC (31)(42) (101)	BP (101)	IND (32)(42) (101)	RI (12)(42) (101)	P (101)	(Reserved)
406	Places of worship (12)	ACUP	ACUP (84)	C	C	ACUP	C	C	--	C	--	--	
408	Private or public schools (20)	ACUP	ACUP	C	C	ACUP	C	P	ACUP	ACUP	C	--	
410	Public facilities and electric power and natural gas utility facilities, substations, ferry terminals, and commuter park-and-ride lots (16)	ACUP	ACUP	ACUP	ACUP	ACUP	C	ACUP	ACUP	ACUP	C	P	
INDUSTRIAL USES													
500	Accessory use or structure (1)(51)	P	P (84)	P	P	P	P	P	P	P	P	--	
502	Air pilot training schools	P	P (84)	P	--	--	--	P	P	P	--	--	
504	Assembly and packaging operations	C	C (61)(84)	C	--	--	--	P	--	ACUP	C	--	
506	Boat yard	ACUP	ACUP (61)(84)	--	--	--	--	P (61)	ACUP	ACUP	C	--	
508	Cemeteries, mortuaries, and crematoriums (10)	ACUP	ACUP (61)(84)	C	C	--	C	ACUP (61)	--	ACUP	C	--	
510	Cold storage facilities (69)	--	--	--	--	--	C	--	ACUP	P	C	--	
512	Contractor's storage yard (21)	--	--	--	--	--	--	P (61)	--	P	ACUP	--	
514	Food production, brewery or distillery	C	C (61)(84)	C	--	--	C	ACUP	ACUP	C	C	--	
516	Fuel distributors	C	C (61)(84)	--	--	--	--	C (61)	--	C	C	--	
518	Helicopter pads (13)	C	C (84)	C	--	C	--	ACUP	--	ACUP	ACUP	--	
520	Manufacturing and fabrication, light	C	C (61)(84)	C	--	--	--	P	P	P	C	--	
522	Manufacturing and fabrication, medium	--	--	--	--	--	--	C (52)(61)	ACUP	P	C	--	

Comprehensive Plan Land Use Designation →		Urban High Intensity Commercial		Urban Low Intensity Commercial			Rural Commercial	Urban Industrial			Rural Industrial	Public Facilities	
Zoning Classification →		C (19)(30) (48)(57) (101)	RC (19)(48) (57)(88) (101)	UVC (30) (48)(57) (101)	NC (19)(30) (48)(57) (101)	LIC (48)(57) (101)	RCO (12)(64) (101)	BC (31)(42) (101)	BP (101)	IND (32)(42) (101)	RI (12)(42) (101)	P (101)	(Reserved)
Categorical Use													
524	Manufacturing and fabrication, heavy	--	--	--	--	--	--	--	--	ACUP	--	--	
526	Manufacturing and fabrication, hazardous	--	--	--	--	--	--	--	--	C	--	--	
528	Recycling centers	--	--	--	--	--	C	--	--	ACUP	C	--	
530	Rock crushing	--	--	--	--	--	--	--	--	C	C	--	
532	Slaughterhouse or animal processing	--	--	--	--	--	C (70)	--	--	C	C (70)	--	
534	Storage, hazardous materials	--	--	--	--	--	C (75)	--	--	C	C (75)	--	
536	Storage, indoor	C	C (61)(84)	-- P	--	--	C (75)	P (61)	P	P	ACUP	--	
538	Storage, outdoor	--	--	--	--	--	C (75)	ACUP (61)	--	P	P (75)	--	
540	Storage, self-service	ACUP	ACUP (61)(84)	--	C	ACUP -- (79)	C (75)	ACUP (61)	--	P	P (75)	--	
542	Storage, vehicle and equipment (1)	ACUP	--	--	--	--	C	ACUP (61)	--	P	C (75)	--	
544	Top soil production, stump grinding	--	--	--	--	--	C	--	--	ACUP	ACUP	--	
546	Transshipment facilities, including docks, wharves, marine rails, cranes, and barge facilities	--	--	--	--	--	--	P (61)	C	C	C	--	
548	Uses necessary for airport operation such as runways, hangars, fuel storage facilities, control towers, etc. (13)	--	--	--	--	--	--	--	--	C	C (74)	--	
550	Warehousing and distribution (68)	--	--	--	--	--	--	P (61)	P	P	ACUP	--	
552	Wrecking yards and junk yards (1)	--	--	--	--	--	--	--	--	C	C	--	

Comprehensive Plan Land Use Designation →		Urban High Intensity Commercial		Urban Low Intensity Commercial			Rural Commercial	Urban Industrial			Rural Industrial	Public Facilities	
Zoning Classification →	C (19)(30) (48)(57) (101)	RC (19)(48) (57)(88) (101)	UVC (30) (48)(57) (101)	NC (19)(30) (48)(57) (101)	LIC (48)(57) (101)	RCO (12)(64) (101)	BC (31)(42) (101)	BP (101)	IND (32)(42) (101)	RI (12)(42) (101)	P (101)	(Reserved)	
Categorical Use													
RESOURCE LAND USES													
600	Accessory use or structure (1)(51)	P	P (84)	P	P	P	P	P	P	P	P	P	
602	Aggregate extractions sites	--	--	--	--	--	C	P	--	C	C	--	
606	Aquaculture practices	C	C (84)	C	C	C	C	P	--	C	C	P	
608	Forestry	P	P (84)	--	P	P -- (79)	P	P	P	P	P	P -- (79)	
610	Shellfish/fish hatcheries and processing facilities	--	--	--	--	--	--	--	--	C	C	--	

1
2
3

1 B. Kitsap County Code Section 17.410.050 ‘Footnotes for zoning use tables’, last amended by
2 Ordinance 550 (2018), is revised as follows:

3
4 **17.410.050 Footnotes for zoning use tables.**

5 A. Where noted on the preceding use tables, the following additional restrictions apply:

6 ...
7 56. There shall be no more than six rental vehicles kept on site. Additional rental car
8 inventory shall be kept at an off-site location consistent with the Kitsap County Code.

9
10 *[Note: This amendment is shown in an abbreviated form to keep this document concise. The rest*
11 *of the footnotes in this section will be inserted, without further amendment, into the final*
12 *ordinance prior to adoption.]*
13
14
15

Commented [PB10]: Informational Note:
This addition is intended to clarify current implementation of this footnote and address the concern of the Workgroup of large rental car inventories in a pedestrian-oriented downtown.

Kingston Subarea Plan

Introduction

The following proposed amendments to the Kingston Subarea Plan are intended to update the Kingston goals and policies current relative to the above amendments.

Amendment #9: Kingston Subarea Plan Goals and Policies (Comp Plan)

The Kitsap County Comprehensive Plan, Chapter 8 (Subarea Plans), Kingston Subarea Plan, pages 8-91 through 8-96, last amended by Ordinance 565 (2018), is amended as follows:

Kingston Goals and Policies

Economic Development

Kingston Economic Development Goal 1. Within the Kingston Urban Growth Area, support the establishment of locally-owned businesses, cottage industries and home businesses.

Kingston Policy 1. Encourage the development of state-of-the-art telecommunication infrastructure to serve the Kingston Urban Growth Area.

Kingston Policy 2. Continue to allow home office businesses within the Urban Village Center zone.

Kingston Policy 3. Collaborate with the Kingston Chamber of Commerce, the Kitsap Economic Development Alliance (KEDA), Public Utilities District, and the Port of Kingston and other organizations to foster and promote an information system infrastructure and promote a business atmosphere that encourages and supports technology-based industry.

Kingston Economic Development Goal 2. Support the maintenance of local businesses.

Kingston Policy 4. Investigate feasibility of using a rolling type "multi-year" permit process for event venues for appropriate locations.

Kingston Policy 5. Work to streamline regulations to encourage agritourism.

Kingston Policy 6. Reserved.

Kingston Policy 7. ~~Reserved. Encourage development that build projects compatible with Kingston's current design.~~

Commented [PB11]: Informational Note:
Kingston Policy 7 has been moved down to become Kingston Land Use Goal 16.

1 Kingston Policy 8. Encourage small business development and business incubators.
2

3 **Kingston Economic Development Goal 3. Support tourism to enhance the local**
4 **economy.**
5

6 Kingston Policy 9. Support the development of bed and breakfasts and small lodging
7 venues in the Kingston area.
8

9 Kingston Policy 10. Facilitate the development entry/exit signage consistent with area
10 identity.
11

12 Kingston Policy 11. Facilitate the development of a downtown way-finding system
13 consistent with area identity.
14

15 Kingston Policy 12. Foster partnerships with the North Kitsap Tourism Coalition, Visit
16 Kitsap, the Port of Kingston, the Kingston Chamber of Commerce, the Greater Kingston
17 Economic Development Committee and other organizations to promote tourism and
18 business development.
19

20 **Environment**
21

22 **Kingston Environment Goal 4. Protect and work to restore wildlife habitat, marine**
23 **shorelines, and other natural areas around Kingston.**
24

25 Kingston Policy 13. Support community work with non-profit groups to acquire land for
26 conservation and preservation of wildlife habitat.
27

28 Kingston Policy 14. Participate in the West Sound
29 Watersheds Council and support Puget Sound Salmon Recovery programs.
30

31 Kingston Policy 15. As feasible, acquire shoreline property and investigate easements in
32 Kingston for public stewardship and habitat protection.
33

34 **Kingston Environment Goal 5. Coordinate an integrated network of spaces that**
35 **could expand recreational opportunities for both residents and visitors, and takes**
36 **advantage of Kingston’s visual amenities and natural environment.**
37

38 Kingston Policy 16. As feasible, work with non-profit groups to acquire land for open
39 space conservation and trails network.
40

41 **Parks, Trails and Open Space**
42

43 **Kingston Parks, Trails and Open Space Goal 6. Encourage participation of**
44 **community organizations and residents in the planning, development and**
45 **authorized use of parks, community facilities, libraries and senior centers.**
46

1 Kingston Policy 17. Consult with the community organizations and residents concerning
2 the disposition or repurposing of public land and facilities that have parks, trails and
3 recreation potential.
4

5 Kingston Policy 18. Coordinate with other local jurisdictions, community organizations
6 and residents in developing long range plans, budgets and usage regulations for public
7 facilities, parks and open space.
8

9 **Kingston Parks, Trails and Open Space Goal 7. Pursue the creation of a more**
10 **walkable community by supporting development of pedestrian pathways, sidewalks**
11 **and trails that connect people to places.**
12

13 Kingston Policy 19. As feasible, support recommendations identified in the Kitsap
14 County Non-Motorized Facility Plan.
15

16 Kingston Policy 20. Coordinate with state, Tribal, and non-governmental groups and
17 Kingston residents to acquire land for trails, community connectors and open space
18 corridors.
19

20 Kingston Policy 21. Coordinate maintenance and operation support for parks, trails and
21 open space with other jurisdictions and supporting community groups.
22

23 Kingston Policy 22. Encourage Kingston residents to review budgets for Parks,
24 Recreation and Open space for the Kingston area.
25

26 Kingston Policy 23. Coordinate with the local
27 jurisdictions, state agencies, and community groups to create and enhance water related
28 recreation, facilities and public access.
29

30 Kingston Policy 24. Safety and security shall be considered when reviewing plans for
31 trails, pathways, and greenways to connect parks, shoreline and recreational resources
32 throughout the Kingston area.
33

34 Kingston Policy 25. Encourage the establishment of a trails system sign program that
35 identifies access points and destinations.
36

37 **Kingston Parks, Trails and Open Space Goal 8. Create bicycle routes, multiuse**
38 **pathways, and bike storage facilities to provide safe, secure and efficient bicycle**
39 **connections for commuter, visitor, fitness and recreational riders.**
40

41 Kingston Policy 26. Consult and coordinate with Kingston area community groups to
42 define, and prioritize Kingston area bike routes.
43

44 Kingston Policy 27. Ensure that local bike routes connect with regional bike routes.
45

46 Kingston Policy 28. Support and promote the Kitsap Peninsula Water Trail.

1
2 Kingston Policy 29. Coordinate with state agencies and community organizations to
3 ensure that public use of tidelands and public waterfront property protects water quality
4 and sensitive areas while also allowing for public recreation.
5

6 **Kingston Parks, Trails and Open Space Goal 9. Provide and maintain Parks and**
7 **Fields and community facilities to support sports, recreational, educational, and**
8 **social activities for the community.**
9

10 Kingston Policy 30. Consult with state agencies and local jurisdictions to provide parks,
11 open space, fields, and facilities that support active and passive recreation.
12

13 Kingston Policy 31. Encourage public participation in development of plans for
14 maintenance and operation for parks, open space, fields, and facilities in the Kingston
15 area including volunteer efforts.
16

17 Kingston Policy 32. Encourage public participation of community organizations and
18 residents in the planning, development, operation, authorized use and maintenance of
19 parks, trails, community facilities, libraries, sports fields and senior centers.
20

21 **Transportation**
22

23 **Kingston Transportation Goal 10. Work with WSDOT to attempt to reduce traffic**
24 **issues.**
25

26 Kingston Policy 33. Encourage WSDOT to reduce the back up of queued ferry traffic in
27 downtown Kingston and optimize the use of the ferry holding lot.
28

29 Kingston Policy 34. Encourage WSF to provide a third overflow ferry boat during
30 periods of high ferry demand and ferry wait times.
31

32 Kingston Policy 35. Encourage WSDOT to pursue the rerouting of ferry traffic to the
33 present outbound ferry traffic lane and investigate the feasibility of using the current
34 WSDOT parking lot as auxiliary holding area.
35

36 **Kingston Transportation Goal 11. Enhance the aesthetic values of the streetscape in**
37 **Kingston ~~as identified in the Kingston Complete Streets~~.**
38

39 Kingston Policy 36. ~~Within the Kingston Urban Growth Area, ensure~~ **Ensure the**
40 **streetscapes envisioned in the Kingston** design standards **and the Kingston Complete**
41 **Streets Plan** are **implemented followed.**
42

43 Kingston Policy 37. ~~Reserved. Consider updating the Kingston Design Standards with~~
44 ~~focus on aesthetics of streets, sidewalks, and associated amenities.~~
45

Commented [PB12]: Informational Note:
Struck-out language is addressed more specifically in Policy 36 than in this broad goal statement.

Commented [PB13]: Informational Note:
This change reflects how the proposed amendments would work by integrating the design standards and complete streets plan.

Commented [PB14]: Informational Note:
Amendment #2 completes this policy and therefore it can be retired.

1 **Kingston Transportation Goal 12. Work to improve safety for pedestrians, bicycles,**
2 **and vehicles within the Kingston Urban Growth Area.**

3
4 Kingston Policy 38. Encourage connectivity between developments.

5
6 Kingston Policy 39. Coordinate with WSF to improve pedestrian and bicycle access to
7 the ferry dock.

8
9 **Kingston Transportation Goal 13. Work to ensure that an appropriate balance of**
10 **long-term and short-term parking is available in the downtown area.**

11
12 Kingston Policy 40. Through public-private and public-public partnerships, review the
13 existing public parking availability, and accessibility within Kingston. Consider creating
14 a public parking management program and/or parking improvement district.

15
16 Kingston Policy 41. ~~Reserved. After public parking availability and accessibility study~~
17 ~~is complete, consider updates or changes to parking requirements relating to occupancy~~
18 ~~in the Kingston Urban Growth Area.~~

19
20 Kingston Policy 42. As feasible, implement incentive-based parking programs within the
21 Urban Village Center Zone, such as transit-oriented development, off-site parking,
22 shared-use parking and on-street parking. On-street parking incentives should be limited
23 to short-term customer parking close to or adjacent to the commercial development.

24
25 **Community**

26
27 **Kingston Community Goal 14. Formalize Kingston community identity.**

28
29 Kingston Policy 43. Locate community-oriented public facilities within the Urban
30 Growth Area.

31
32 Kingston Policy 44. Work with the community to consider preservation and highlighting
33 of the historic features and characteristics of Kingston in community planning and
34 development, and design standards.

35
36 Kingston Policy 45. Preserve the small town character of the Kingston Urban Growth
37 Area in community planning and development.

38
39 Kingston Policy 46. Work with Kingston Community Advisory Council to support
40 ongoing efforts to provide severe weather shelter services and/or warming station in
41 public buildings.
42
43

Commented [PB15]: Informational Note:
This addition is consistent with the design standards and suggest next steps for public parking, which is the focus of this policy.

Commented [PB16]: Informational Note:
Amendments #4-6 complete this policy and therefore it can be retired.

1 **Land Use**

2
3 **Kingston Land Use Goal 15. Involve the Kingston community input when proposing**
4 **development regulations.**

5
6 Kingston Policy 47. Reserved.

7
8 Kingston Policy 48. Reserved.

9
10 Kingston Policy 49. Support the community to the extent feasible in their aspiration to
11 identify view corridors while balancing the rights of private property owners.

12
13 **Kingston Land Use Goal 16. ~~Apply Kingston Design Standards in the permitting~~**
14 **~~process.~~ Encourage development that is compatible with Kingston's current design.**

15
16 Kingston Policy 50. ~~Explore new code provisions to support~~ Periodically monitor
17 redevelopment activity to ensure code provisions and the design standards are achieving
18 the vision for downtown Kingston and public feedback while also supporting
19 redevelopment.

20
21 Kingston Policy 51. ~~Update Kingston Downtown Design Standards to address ongoing~~
22 ~~public input.~~ Apply Kingston Design Standards in the permitting process.

23
24 Kingston Policy 52. Prioritize the required infrastructure to encourage urban medium and
25 high density residential uses near commercial areas and public spaces.

Commented [PB17]: Informational Note:
Underlined language was previously Kingston Policy 7. It was moved here because it provides an appropriate goal statement related to the following policies.

Struck-out language was moved to Policy 51.

Commented [PB18]: Informational Note:
These amendments are intended to satisfy the existing policy. Therefore, this policy has been updated to require periodic review and to continue the intent in Policy 51 of addressing ongoing public input.

Commented [PB19]: Informational Note:
Amendment #2 completes the update portion of the existing policy and therefore it can be retired. The intent to address ongoing public input has been incorporated into the proposed changes to policy 50.

Commented [PB20]: Informational Note:
This underlined language was previously Kingston Land Use Goal 16. It was moved to a policy because it implements the broader goal language proposed above.

Public Review Draft 11/25/2019

Design Standards for the Community of Kingston

~~Prepared for the Kingston Citizens Advisory Committee
By the Kingston Revitalization Association~~

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Commented [PB1]: Informational Note:

Information notes, like this one, are used to provide supplemental information regarding the change to the reader during the review and adoption process and will not be included in the final adopted document.

How do I know what changes are proposed?

Amendments proposing to remove content are indicated with ~~strikeouts~~ whereas amendments proposing to add new content are indicated with underlines.

Changes recommended by the Kingston UVC Workgroup, an appointed citizen advisory group, are indicated in blue text. Additional changes recommended by the Department of Community Development, including changes to the Workgroup's recommendations, are indicated in red text. Where needed for clarity, tables with alternative recommendations are provided.

Changes to page numbering are not shown. Scrivener edits to list/bullet numbering will be included in the final adopted document.

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History

History

These design standards were originally prepared for the Kingston Citizens Advisory Committee by the Kingston Revitalization Association to help implement the 1993 Kingston Community Design Study. These design standards were first adopted in 2000 by Kitsap County Ordinance No. 250-2000 and have been amended as follows:

<u>Ordinance</u>	<u>Adoption Date</u>
TBD-2019	TBD

Introduction

A. Purpose

The purpose of the following Design Standards is to help implement the physical aspects of the Kingston community vision for downtown ~~areas that evolved out of the Kingston Community Design Study developed in 1993 in the Kingston Subarea Plan~~. These standards are intended to promote Kingston’s small town character and support local economic vitality while accommodating the impact of existing regional transportation and tourism issues. The intent is not only to provide some assurance to the community of basic conformity to the vision statement but also to encourage creativity.

B. Applicability

These standards apply ~~to projects~~ within the ~~identified following zones~~ design districts of the Kingston Urban Growth Area of Kingston (see the map on pg. ii) as mapped in Appendix A. ~~They are defined and named as follows:~~

1. Old Town/Waterfront Design District.
2. Village Green Design District.
3. Lindvog Commercial Design District.

~~Any existing structure or facility that does not conform to these standards is exempt from compliance unless the use or the structure is modified, upgraded, remodeled, or otherwise improved. All single-family residential structures are exempt from these design standards.~~

The design standards apply to (1) all new development in the design districts and (2) the redevelopment of existing structures in the design districts as follows:

Workgroup Rec.	<ul style="list-style-type: none"> • <u>For all existing structures in the design districts with exterior improvements or additions or both valued at less than 50 percent of the assessed value of the structure, only the new portion(s) of the structure and associated site improvements are subject to the design standards to the greatest extent practical.</u> • <u>For all existing structures in the design districts with exterior improvements or additions or both valued at equal to or greater than 50 percent of the assessed value of the structure in any three-year period, then the design standards apply to the entire structure and associated site improvements to the greatest extent practical.</u>
DCD Rec.	<ul style="list-style-type: none"> • <u>For exterior improvements and additions valued at less than 50 percent of the parcel’s assessed building value, only the new portion(s) of the structure(s) and associated site improvements are subject to the design standards.</u> • <u>For exterior improvements and additions over any three consecutive calendar years cumulatively valued at equal to or greater than 50 percent of the parcel’s assessed building value in the beginning of the same three-year period, the design standards apply to the entire structure(s) and associated site improvements.</u>

Commented [PB2]: Informational Note:
The Workgroup and Department believe this provision needs clarification and a reasonable threshold. The 50% threshold proposed below is consistent with the Silverdale Design Standards.

Commented [PB3]: Informational Note:
Amendment #8 proposes to change detached single-family residences and accessory dwelling units from prohibited uses to allowed uses and would need to meet minimum densities of 10 DU/Ac. The sections of the design standards applicable to these and similar uses are listed below for clarity.

Additionally, only the following sections of the design standards shall apply to single-

Design Study Review

family residences, duplexes, accessory dwelling units (ADUs), and accessory living quarters (ALOs) in order to maintain a basic level of consistency with the community character:

- [Introduction, Section D – Variances](#)
- [Introduction, Section E – Improvement Districts](#)
- [Chapter II, Section A.1.a and A.1.b – Relationship to Street Front](#)
- [Chapter III, Section A.2 – Sidewalk Construction](#)
- [Chapter III, Section B.2 – Parking](#)
- [Chapter III, Section B.5.b – Street Furniture](#)
- [Chapter IV, Section A.2 – Building Height](#)
- [Chapter IV, Section D.1 – Building Materials](#)
- [Chapter IV, Section F – Accessory Dwelling Units \(ADUs\) and Accessory Living Quarters \(ALOs\)](#)
- [Chapter V, Section C – Retention of Significant Trees](#)
- [Chapter V, Section D – Approved Plant List](#)
- [Chapter VII – Lighting](#)

Where the provisions of these design standards conflict with other provisions in Kitsap County Code, the provisions in these design standards shall apply.

Workgroup Rec.	<p>C. Design Concept Statement</p> <p>As part of the design review application, a written design concept statement shall be submitted that will identify the significant site features, support the reasoning behind the architectural design and site plan proposal, explain how and why the existing site features are incorporated into the project design, and demonstrate how the proposal is consistent with the general goals of the Kingston Community Design Study. Development proposals are subject to Site Plan Review procedures set forth in Section 410 of the Kitsap County Zoning Ordinance.</p>
DCD Rec.	<p>C. Design Narrative</p> <p>As part of the design review application, a <u>Δ</u> written design concept statement narrative shall be submitted that will identify the <u>identifies</u> significant site features, <u>supports</u> the reasoning behind the architectural design and site plan proposal, <u>explains</u> how and why the existing site features are incorporated into the project design, and <u>demonstrates</u> how the proposal is consistent with <u>each required element of these design standards</u> the general goals of the Kingston Community Design Study. Development proposals are subject to Site Plan Review procedures set forth in Section 410 of the Kitsap County Zoning Ordinance.</p>

Commented [PB4]: Informational Note:
The referenced requirements are out of date.

Commented [PB5]: Informational Note:
The referenced requirements are out of date.

D. Variances

All variance requests from these standards must demonstrate conformance with the general goals, policies, and objectives of the Kingston Subarea Plan Community Design Study, the Kingston Design Standards, and the criteria for variance approval outlined in

Design Study Review

1 the Kitsap County Zoning Ordinance (KCC 17.560). Financial hardship shall not be
2 considered for variances from these design standards.

Commented [PB6]: Informational Note:
This language is consistent with similar variance provisions elsewhere in Kitsap County Code.

3
4 **E. Improvement Districts**

5 ~~All new development shall participate in future improvement districts for~~
6 ~~business/community aesthetic enhancement that may include a proportionate share in~~
7 ~~construction of pedestrian walkways, street lighting, street trees, and road improvements.~~
8 ~~A covenant of agreement shall be recorded with the Kitsap County Auditor and filed with~~
9 ~~the Department of Community Development and Public Works as part of the~~
10 ~~development approval process.~~

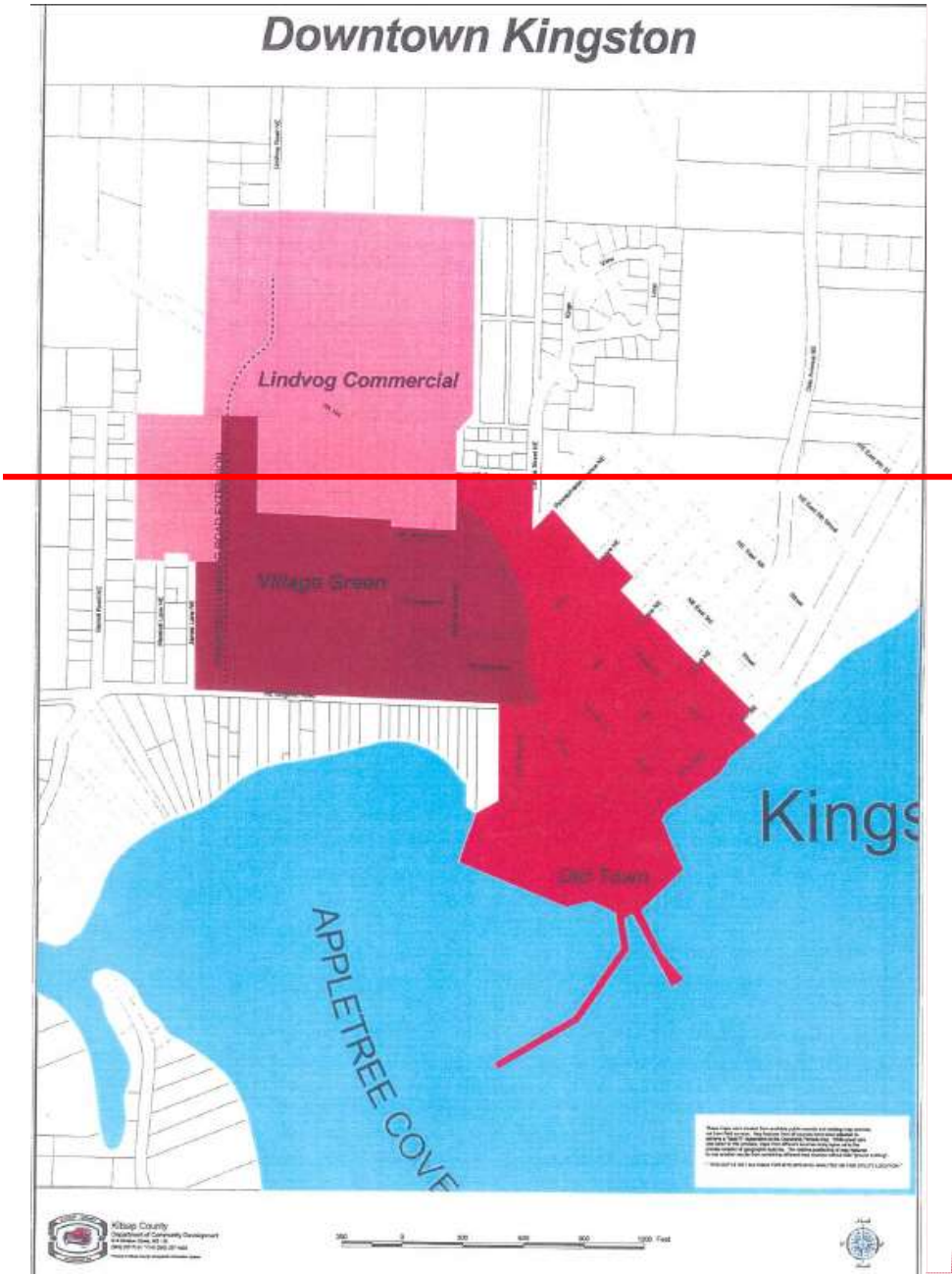
Commented [PB7]: Informational Note:
Revisions in this section are intended to update the language consistent with best practices.

11
12 Any new development or redevelopment that would be required to construct sidewalks,
13 pedestrian walkways, street lighting, street furniture, street trees, on-street parking, or
14 other pedestrian or road improvements as a condition of approval may be allowed, at the
15 discretion of the Kitsap County Department of Public Works, to defer those
16 improvements to allow for a coordinated design and development effort if the property
17 owner executes a legally binding agreement, in a form acceptable to the County, in
18 which:

- 19 1. The property owner agrees to participate without protest in any local
20 improvement district, local utility improvement district, road improvement
21 district, transportation benefit district, or other similar structure for the
22 construction of the deferred improvements;
- 23 2. The property owner agrees to sign any petition for the formation thereof and
24 payment of subsequent fees or charges;
- 25 3. The agreement is signed by the director on behalf of Kitsap County; and
- 26 4. The agreement is recorded by the County with the Kitsap County Auditor and is
27 binding on all heirs, assigns, transferees, donees, and successors in interest.

28
29 Nothing in this section shall be construed to limit the ability of the property owner to
30 challenge the amount of any assessment.
31

Design Study Review



1

Commented [PB8]: Informational Note:
The maps is being moved to Appendix A and updated for clarity, there are no boundary changes.

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Definitions

~~**Downtown area of Kingston:** The area of Kingston that includes the Old Town/Waterfront District, the Village Green District, and the Lindvog Commercial District (for the specific boundary see the map in the Introduction, pg. ii).~~

~~**Old Town/Waterfront District:** The properties located north of the Port of Kingston, east of Central Street, south of Georgia Ave. and west of the northeastern properties abutting E. 2nd St. (not a legal description, for the specific boundary see the map in the Introduction, pg. ii).~~

~~**Village Green:** The properties located north of West Kingston Rd., east of the western properties abutting the proposed Lindvog Rd. extension, south of California Ave., and west of SR 104 (not a legal description, for the specific boundary see the map in the Introduction, pg. ii).~~

~~**Lindvog Commercial:** The properties located north of California Ave., east of the western properties abutting Lindvog Rd. and its proposed extension, south of the northern property line of the Kingston Tree Development and west of 1st Ave. (not a legal description, for the specific boundary see the map in the Introduction, pg. ii).~~

~~**Pedestrian-oriented space:** The area between a building and a public street that promotes visual and pedestrian access onto the site and that provides amenities and landscaping to enhance the public's use of the space for passive activities, such as resting, reading, picnicking, and window shopping. The area should be visible from the public right-of-way and accessible to pedestrians, including those with handicaps.~~

~~**Pedestrian-oriented facade:** The ground floor frontage of a building design, which offers an interesting appearance to attract pedestrian interest in the locality and encourages pedestrian access.~~

~~**Pedestrian-friendly street:** Any street designed for safe use by both pedestrians and vehicles. A pedestrian-friendly street will include sidewalks or walkways, landscaping, lighting, and other street amenities benefiting pedestrians.~~

Commented [PB9]: Informational Note:
Relevant definitions from here and elsewhere in the design standards have been moved to KCC 17.110. Design district descriptions have been removed in lieu of a clearer map.

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Chapter I

Design Study Review

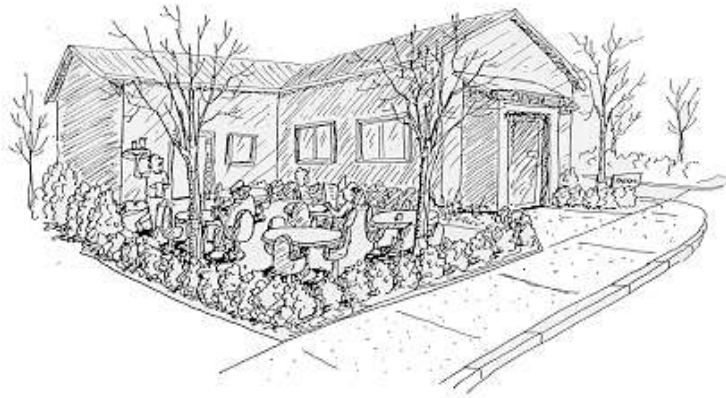


Illustration courtesy of Bob Hines

The Kingston Community Design Study (Final Report of 1993, page 1-1) established the following vision, goals, and objectives:

Vision for Downtown Kingston: The purpose of the Kingston Community Design Study, as defined by the steering committee, is to develop a community vision to guide the future growth of Kingston and an urban design plan to implement that vision. The vision statement is to represent the desired outcome of meeting the needs of a growing community and to provide the foundation for the development of the plan. The elements of the steering committee’s vision are:

- That the town will be oriented toward the residents.
- That the community will retain its small town atmosphere, its feeling of friendliness and gentleness, even as it is affected by growth.
- That it will be a friendly, interesting, and safe place for children.
- That its downtown will be of a comfortable scale, one in which people can safely walk to various destinations.
- That it will have a pedestrian orientation.
- That views and open space will be preserved.
- That there will be adequate recreational opportunities.
- That economic vitality will be encouraged and that businesses that serve residents will also be encouraged.

Design Study Review

1 **Community Goals and Objectives:** The following goals and objectives were copied from the
2 Kingston Community Design Study, Final Report of 1993, pages IV-1 thru IV-5.

3
4 A. Protect Kingston's environmental quality.

- 5 • Use shoreline resources to tie community to water and provide better shoreline access.
- 6 • Protect sensitive/critical areas and natural systems.
- 7 • Protect aquifers and water quality and quantity.
- 8 • Protect wildlife habitat.
- 9 • Protect trees, especially in highly visible areas and along ridge lines.

10
11 B. Build a sense of community.

- 12 • Increase local empowerment regarding Kingston's future, especially with respect to land
13 use development, public facilities and Washington State Department of Transportation
14 project impacts.
- 15 • Improve communication between Kitsap County and the Kingston community and its
16 citizens.
- 17 • Provide more community gathering places for social interaction.
- 18 • Create a central community focal point for gathering, socializing, and celebrations.
- 19 • Design public facilities to serve more than one use.
- 20 • Upgrade the Library.
- 21 • Build a consensus among Kingston's citizens for cooperative action.

22
23 C. Enhance Kingston's visual identity as a community.

- 24 • Preserve the small town character.
- 25 • Preserve natural/visual resources.
- 26 • Emphasize the town's waterfront orientation.
- 27 • Enhance arrival points, or gateways at the entries to the town.
- 28 • Create pedestrian-oriented retail cross streets in the downtown core.
- 29 • Enhance historic features.

30
31 D. Establish predictable and defined land use patterns that support Kingston's community vision.

- 32 • Define Kingston's boundary.
- 33 • Recognize and enhance existing neighborhoods.
- 34 • Prevent sprawl and utilize land efficiently.
- 35 • Integrate land use pattern and circulation systems so that they are mutually supporting.

36
37 E. Improve and encourage economic development opportunities.

- 38 • Encourage locally owned businesses and cottage industries.

Design Study Review

- 1 • Provide basic consumer needs and convenience shopping in town.
- 2 • Provide for “appropriate” expansion of business core for growth expected within the
- 3 urban growth area.
- 4 • Respect property rights while pursuing a cooperative plan that emphasizes community –
- 5 wide public benefits.
- 6 • Provide urban design and public improvement projects to provide a focal point for
- 7 businesses and community activities in the town of Kingston.
- 8
- 9 F. Improve traffic conditions for local community.
- 10 • Improve safety for pedestrians/bicycles/vehicles.
- 11 • Provide a range of transportation options including walking, cycling, transit as well as
- 12 private automobile.
- 13 • Upgrade local access streets.
- 14 • Improve ferry traffic management. Reduce impact of speeding cars, parking and holding.
- 15 • Manage downtown parking.
- 16 • Improve public transit
- 17
- 18 G. Create integrated system of parks, trails and open space.
- 19 • Provide multi-use, non-motorized trails for both commuter and recreational use.
- 20 • Provide safe and secure pedestrian access for residents and visitors.
- 21 • Encourage expanded use of existing facilities.
- 22 • Create a common social and recreational focus:
- 23 1. Community center/Library/“City” hall.
- 24 2. A village green.
- 25 3. A town square.
- 26
- 27 H. Improve Port facilities and waterfront
- 28 • Provide more waterfront/beach access with managed, well defined public access points.
- 29 • Preserve visual access to water.
- 30 • Protect view corridors.
- 31 • Provide and maintain good pedestrian access from town to the Marina with sidewalks and
- 32 other pedestrian amenities on downtown “core” streets.
- 33

1 Chapter II
2 Site Planning
3



4
5 *Illustration courtesy of Bob Hines.*

6
7 **Definition:** ~~The determination of the location, orientation, and relationships of buildings,~~
8 ~~parking, landscaping, open spaces, access, and other features on the subject property and their~~
9 ~~relationship to the surrounding neighborhood.~~

10
11 **Goal:** To promote development that is functional, visually coherent, and visually compatible
12 among existing and proposed development and that ~~will~~ achieve a high-quality appearance,
13 enhance the Kingston community, and achieve the plan objectives of the Kingston Community
14 Design Study.

15
16 **Objectives:**

- 17
- 18 • To create an active and safe pedestrian environment.
 - 19 • To upgrade the downtown and enhance the visual identity of Kingston.
 - 20 • To improve pedestrian and auto circulation.
 - 21 • To organize activities and structures into a unified village setting.
 - 22 • To create attractive and compatible streetscapes.
 - 23 • To increase compatibility between uses and support an efficient land use pattern.

24 All development in the downtown area of Kingston shall include site planning measures to define
25 the street edge, encourage pedestrian access, provide amenities, and support downtown design
26 objectives.

Site Planning

1 **A. Building Location and Orientation**

2 Buildings ~~in the downtown commercial districts~~ should be sited to provide functional
3 outdoor spaces and public spaces that ~~will~~ enhance the use of the ~~design~~ district, that ~~will~~
4 maintain continuity between developments by relating the building and use to the street
5 frontage, and that ~~will~~ encourage and accommodate pedestrians. The following elements
6 shall be addressed:

7
8 **1. Relationship to Street Front**

9 Site structures in a manner that ~~will~~ complement ~~the~~ adjacent structures. Sites should
10 be developed in a coordinated manner to provide order and diversity to avoid a
11 jumbled, confused development.
12

Workgroup Rec.	<p>(a) Set back new construction no more than 15 feet from the roadside edge <u>front lot line</u>, except:</p> <ul style="list-style-type: none"> • Where <u>where</u> public spaces are incorporated into the front yard setback; or • When <u>when</u> new construction can be demonstrated to be consistent with a larger master plan of multiple properties and uses and found to be consistent with the goals and policies of the Kingston <u>Subarea Plan Community Design Study</u>; or • <u>To accommodate the minimum off-street parking for single-family dwellings and duplexes.</u> 								
DCD Rec.	<p>(a) Set back new construction no more than 15 feet <u>Maximum building setbacks</u> from the roadside <u>right-of-way</u> edge, except shall be as follows:</p> <table border="1" style="margin-left: 40px;"> <thead> <tr> <th style="text-align: center;"><u>Street Frontage Type</u></th> <th style="text-align: center;"><u>Maximum Building Setback</u></th> </tr> </thead> <tbody> <tr> <td><u>Designated pedestrian-oriented commercial streets (Appendix B)</u></td> <td style="text-align: center;"><u>3 feet</u></td> </tr> <tr> <td><u>Alleys (Appendix C) and shared driveways</u></td> <td style="text-align: center;"><u>N/A</u></td> </tr> <tr> <td><u>All other streets</u></td> <td style="text-align: center;"><u>10 feet</u></td> </tr> </tbody> </table> <p><u>The maximum building setback shall be increased by the director:</u></p> <ul style="list-style-type: none"> • <u>Where greater setbacks are required to be consistent with the Kingston Complete Streets Plan or for public safety as determined by the Kitsap County Public Works Department (e.g., clear zones for turning radii);</u> • <u>Where</u> where public spaces are incorporated into the front yard setback; or • <u>When</u> when new construction can be demonstrated to be consistent with a larger master plan of multiple properties and uses and found to be consistent with the goals, and <u>policies, and objectives</u> of the Kingston <u>Subarea Plan Community Design Study and the Kingston Design Standards, in which case the setback shall be determined accordingly; or</u> • <u>For single-family dwellings and duplexes where parking in the rear or side of the site is not feasible, the garage or carport portion of the development shall be setback a minimum of 20 feet.</u> 	<u>Street Frontage Type</u>	<u>Maximum Building Setback</u>	<u>Designated pedestrian-oriented commercial streets (Appendix B)</u>	<u>3 feet</u>	<u>Alleys (Appendix C) and shared driveways</u>	<u>N/A</u>	<u>All other streets</u>	<u>10 feet</u>
<u>Street Frontage Type</u>	<u>Maximum Building Setback</u>								
<u>Designated pedestrian-oriented commercial streets (Appendix B)</u>	<u>3 feet</u>								
<u>Alleys (Appendix C) and shared driveways</u>	<u>N/A</u>								
<u>All other streets</u>	<u>10 feet</u>								

- Commented [PB10]: Informational Note:**
The baseline for measuring setback is shifted from the "roadside edge" (i.e. the curb) to the "right-of-way edge" because it is much easier to define and administer and will result in a more consistent relationship to the streetscape than using the front lot line, which could be separated from the right-of-way by common area tracts.
- Commented [PB11]: Informational Note:**
The 3-foot maximum setback was derived by subtracting the preferred 12-foot sidewalk width from the prior maximum 15-foot setback from the "roadside edge".
- Commented [PB12]: Informational Note:**
The 10-foot maximum setback is proposed to be responsive to other amendments that:
- Removed the mixed-use requirement (adopted in 2018), which previously had required commercial uses in all developments within the UVC zone.
 - Will allow single-family dwellings and apply the design standards to single-family dwellings and duplexes.

Site Planning

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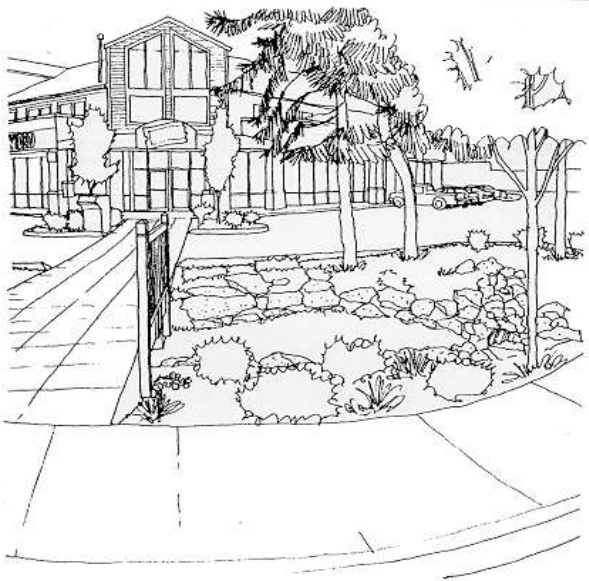
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Figure II.A.1.(a). Street frontages built to the sidewalk give Kingston much of its attraction.

- ~~(b) The setback of new buildings on pedestrian-oriented streets to allow a 12-foot sidewalk is encouraged.~~
- (c) Treat building setbacks as pedestrian-oriented spaces or landscape them in accordance with Chapter V. Minimize paved surfaces except for pedestrian walkways and pedestrian-oriented spaces.
- (d) Provide a clear, identifiable pedestrian route from the public walkway to the building entrance. Preferably, walkways should be separated from the parking lot. If walkways transverse parking lots, the walkways shall be raised or treated with marking and textures. (See Chapter IV, Section B.) Street-facing pedestrian entrances are preferred, but entrances may be on the side of buildings, provided they are visible from the street and connected by a continuous pedestrian walkway.

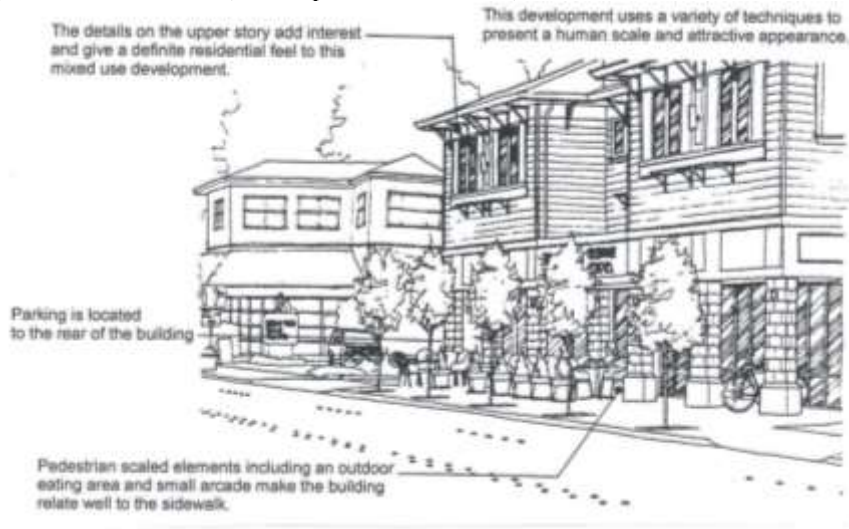
Commented [PB13]: Informational Note:
This provision has been incorporated into the sidewalk table in Section III.A.2.b.

Site Planning



1
2 *Figure II.A.1.(d). This mixed-use development has a strong pedestrian connection to the street*
3 *because of its direct pedestrian connection.*

- 4
5
6 (e) Site buildings so that a pedestrian-oriented facade (see [definitions KCC 17.110.568](#)) is
7 presented to the street. (See Chapter IV, Section B.1.)

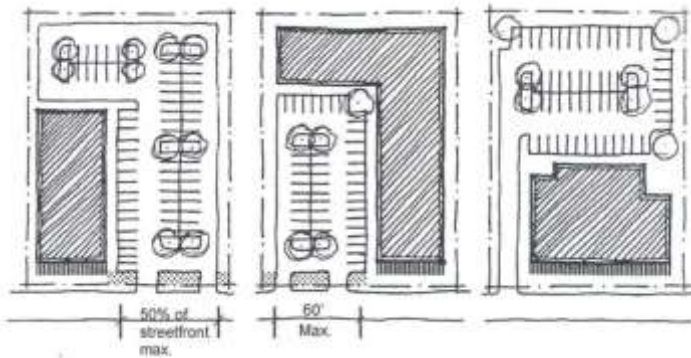


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Site Planning

1 *Figure II.A.1.(e). The quality of downtown Kingston's identity depends on the view from the*
2 *street. New development should be oriented and designed to provide pleasing, pedestrian-*
3 *oriented streetfront. The example above shows a few ways this can be achieved.*

4
5
6 (f) Limit parking and vehicle access (driveways) to no more than 60 feet, or 50 percent
7 (whichever is greater), of the street frontage measured parallel to the curb.



8
9 *Figure II.A.1.(f). Alternate Site plans illustrating different building configurations limiting*
10 *parking lots in the front yard to give the building more prominence and utilize the site more*
11 *efficiently. ~~Street facades must conform to visual design guidelines.~~*

12
13
14 (g) Limit driveways to one entry lane and one exit lane per 300 feet, or to one lane each way per
15 lot if the site frontage is less than 300 feet. (See Chapter III, Section B.I. ~~Guideline~~ (c).

16
17 (h) Do not site outdoor storage areas or outdoor sales areas over 200 square feet in the front of
18 commercial establishments or visible from the street. Exception: Outdoor sales areas are
19 permitted if the merchandise and supporting appurtenances are moved in each day at the
20 close of business hours. The County may allow outdoor sales in front yards if the sales are
21 done in areas that conform to pedestrian-oriented space standards and the merchandise is
22 attractively displayed. Examples that may be permitted under this provision include garden
23 shop or nursery displays of plant materials, outdoor art galleries, and examples of completed
24 home construction projects.

25
26 Open storage of bulk materials, such as top soil or peat, shall not be visible from
27 the street or adjacent properties. Temporary signs, banners, flags, or other visual
28 distractions are not allowed in open storage or sales areas.
29

Site Planning



Figure II.A.1. (h). ~~Rare~~ Occasionally communities need businesses with outdoor sales areas, such as nurseries, lumber yards, hardware stores, and feed stores. Outdoor sales and storage areas can harm a business's and community's identity if not handled in an attractive manner. The two positive examples in this figure solve this problem in different ways. Sack's Feed and Garden Store provides an attractive landscape screen and display, while Kingston Lumber incorporates a covered sales area. In both cases, the parking is located at the side of the building. Note how both businesses are inviting assets to the community's character. Also note how Sack's sign is modest but attractive and ideally located for maximum visibility.

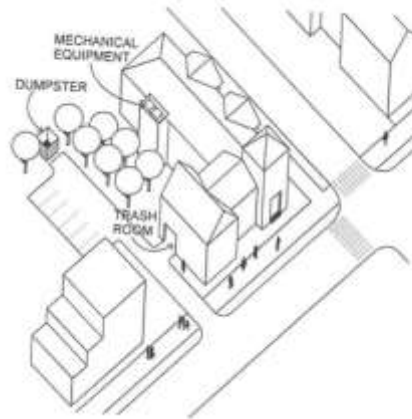
2. Relationship to Adjacent Properties

- (a) Locate service areas, outdoor storage areas, and other intrusive site features away from neighboring properties to reduce conflicts with adjacent uses. Where the County deems necessary, landscape screening ~~must~~ shall be planted along property lines adjacent to "incompatible uses." Incompatible uses include: outdoor storage areas adjacent to any other use, service areas adjacent to any other use, commercial development adjacent to a residentially zoned property. The buffer ~~must~~ shall conform with the requirements of Chapter V, Landscape Design.

If changes in topography between the residential and adjacent property are sufficiently great, then modifications to some of the above buffer options may be allowed with the County's approval. The County may waive screening requirements if a combined multi-lot parking advantage or landscaping is approved.

Site Planning

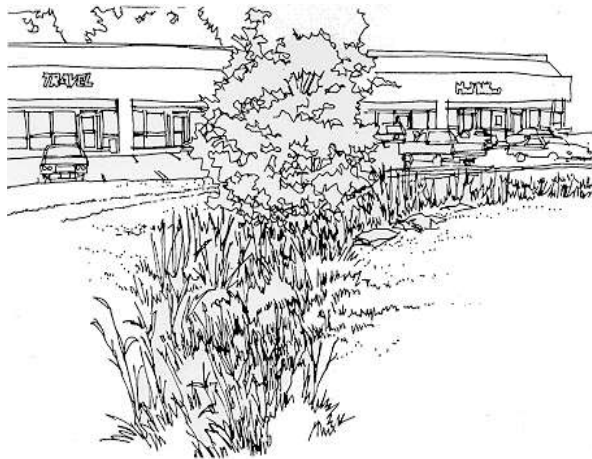
- 1
2 (b) Integrate outdoor storage areas and loading facilities into the site design to
3 minimize their size, reduce visual impact, and allow for pedestrian and vehicular
4 (where appropriate) movement between sites.



5
6 *Figure 11.A.2.(b). Locate trash and service areas to minimize visibility*
7 *and other adverse impacts.*

- 8
9
10 (c) Arrange artificial outdoor lighting during site construction so that the light is
11 directed away from adjoining properties. Lighting shall be directed down to the
12 intended area to be illuminated.
13
14 (d) Ensure that site development meets the requirements of the Kitsap County Noise
15 Ordinance and consider ways to minimize impact upon neighboring properties.
16
17 (e) Incorporate dust, soil erosion, and storm water control measures as required by the
18 Kitsap County Storm Water Management Ordinance.
19

Site Planning



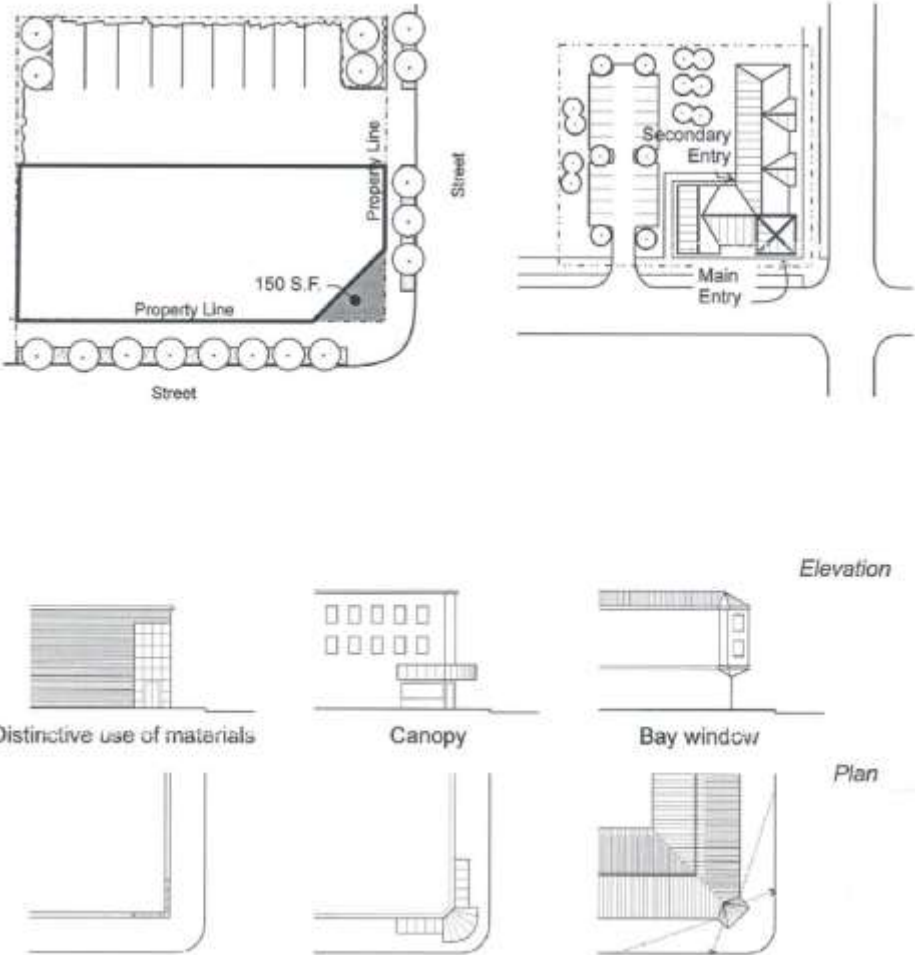
1
2 *Figure II.A.2.(e). Biofiltration swales can incorporate storm water management in a soft,*
3 *attractive, naturalistic way. At this shopping center, a drainage swale in a*
4 *central open space serves as a visual amenity.*
5
6

7 **3. Corner Lot Requirements**

8 Incorporate outdoor spaces or other features at or near the intersection corner of the
9 site when a development is located at a corner intersection. All new buildings or
10 major remodels located on properties at the intersection of two public streets are
11 encouraged to employ one or more of the following design elements or treatments to
12 the building corner facing the intersection:

- 13
- 14 • At least 25 square feet of sidewalk area or pedestrian-oriented open space in
15 addition to the otherwise required setback.
 - 16 • A building entrance, lobby, atrium, or pedestrian pathway at the corner.
 - 17 • A corner architectural element such as a bay window or turret; roof deck,
18 balconies or upper stories; building core setback “notch” or curved façade
19 surfaces; or sculpture or artwork.

Site Planning



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Figure II.A.3. Three ways to satisfy requirements for corner lot development

4. Open Space

(See also Chapter V, Section A.1.1.Guideline (a).)

(a) Site buildings so that the open space is usable, except where it is prudent or necessary to allow for future building expansion. The intention is not that every open space must have a use, but rather that buildings should be oriented to make effective use of the site.

(b) Incorporate significant views, where possible, and solar angles into the design of outdoor spaces.

~~(b)~~(c) Coordinate with adjacent and nearby outdoor spaces and streetscape. To provide larger and more functional open spaces and pedestrian-oriented pocket parks in the downtown and to help facilitate the development of highly constrained lots, the director may allow required open spaces and landscaping, except for parking landscaping, to be located off-site and integrated with other open space and landscaping areas, provided:

1. The proposed development:

- Is otherwise consistent with the goals, policies, and objectives of the Kingston Design Standards;
- Is compatible with adjacent uses and buildings; and
- It can be demonstrated the offsite location will provide a better public amenity than if the open space and landscaping were provided on the development site;

2. It is within the same block as the proposed development or anywhere along the opposite frontage of a street on which the proposed development is located; and

3. A covenant, easement or other contract approved by the director between the cooperating property owners shall be enacted and recorded by the County with the County Auditor as a deed restriction on all associated properties (i.e. the property being developed and the property providing the required open space and landscaping) that cannot be modified or revoked without the approval of the director. The agreement shall:

- Provide that the land comprising the required open space and landscaping shall not be encroached upon, used, sold, leased, or conveyed for any other purpose; and
- Assign maintenance provisions.

If any of the above requirements are violated, the affected property owners must provide the full amount of required off-street parking for each use, in accordance with conditions of approval, unless a satisfactory alternative remedy is approved by the director.

Commented [PB14]: Informational Note:
Due to small lot sizes, this new provision is intended to produce larger and more useful open space and pocket park pedestrian-oriented areas within downtown, which will better achieve the vision for downtown.

Site Planning

1 (e)(d) Address pedestrian needs in site and building design and develop creative
2 approaches to improving pedestrian interest, access, and enjoyment for visitors
3 and those living in the Kingston community.

4
5 (d)(e) Provide, if possible, pedestrian open spaces, such as covered walkways,
6 courtyards, and plazas, as well as open and attractive passageways between
7 buildings and blocks.

8
9 (e)(f) Provide, if possible, outdoor seating and dining areas that face the street.
10



11
12 *Figure 11.A.4. Creative use of open space can add quality to a development and an amenity*
13 *for its occupants and the community.*
14
15

16 5. Multiple Building/Large Lot Developments

17 (a) Take advantage of special opportunities and mitigate impacts of large
18 developments. The site planning for all developments over five acres ~~must~~ shall
19 feature a demonstrable unifying organization that accomplishes the following
20 goals:

- 21 • Mitigates transportation impacts and conforms to the Kingston Complete Streets
22 Plan ~~Downtown Road Improvement Plan's~~ objectives for better pedestrian and
23 traffic circulation by connecting through streets where appropriate.
- 24 • Provides convenient pedestrian circulation connecting all on-site activities to
25 adjacent pedestrian routes and street rights-of-way. (See Chapter III.)

Site Planning

- 1 • Encourages buildings to complement adjacent activities and visual character
- 2 and creates comfortable human environments.
- 3 • Incorporates open space and landscaping as a unifying element.
- 4 • Upgrades the quality of pedestrian-oriented streets if adjacent to applicable
- 5 street fronts. (See Chapters III and V.)
- 6 • Where possible, incorporates screening, environmental mitigation, utilities,
- 7 and drainage as positive elements (e.g., creates a “natural” open space or wet
- 8 pond as a site feature to accommodate surface water runoff).
- 9
- 10 (b) Coordinate guideline design standard requirements to produce innovative
- 11 organizational schemes. While the referenced guidelines design standards apply to
- 12 smaller lots as well, the intent of this guidelines design standard is to encourage
- 13 project designers to coordinate guideline design standard requirements into
- 14 innovative organizational schemes, such as a “village green,” “small town grid,”
- 15 “pedestrian square,” or “perimeter walk,” that integrate the new development into
- 16 the downtown’s existing structure and create a pedestrian- oriented focus.
- 17

18 **6. Related Guidelines**

- 19 ~~(a) See Chapter III for circulation and parking elements.~~
- 20
- 21 ~~(b) See Chapter IV for building elements related to site planning.~~
- 22
- 23 ~~(c) See Chapter V for landscape design elements.~~
- 24
- 25 ~~(d) See Chapter VI for site lighting.~~
- 26
- 27 ~~(e) See Chapter VII for utilities and service areas.~~
- 28
- 29

Commented [PB15]: Informational Note:
Removed because it is redundant with section below.

Site Planning

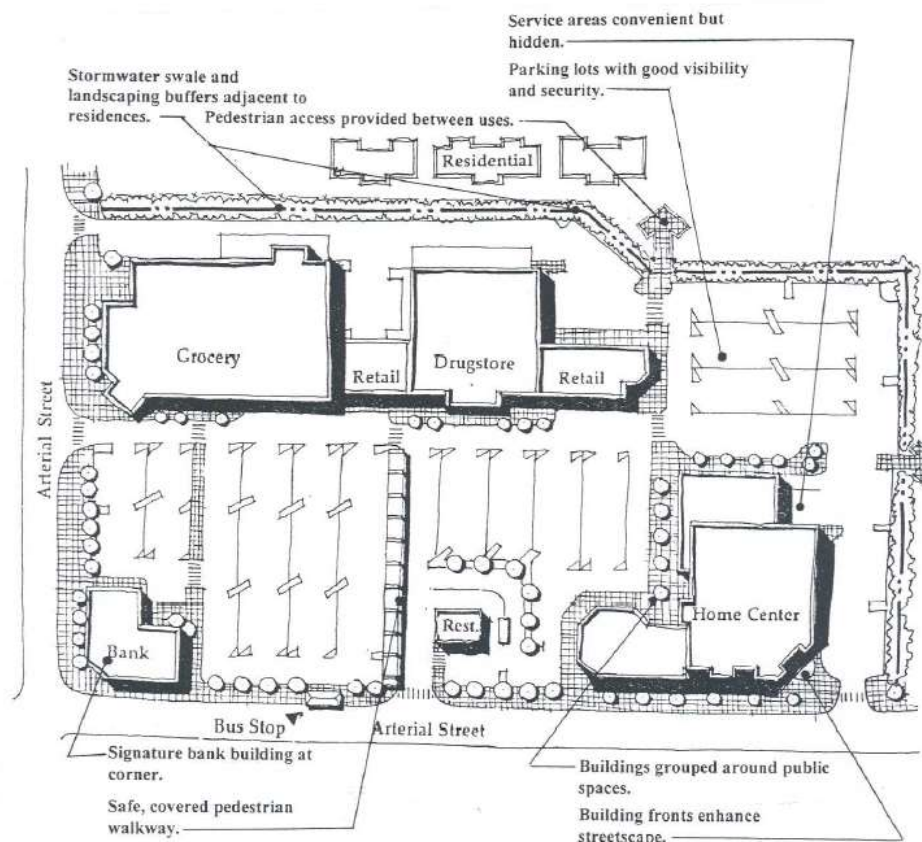


Figure II.A.5.(a). Some considerations for large lot development.

6. Related [Design Standards Guidelines](#)

- (a) See Chapter III for circulation and parking elements.
- (b) See Chapter IV for building elements related to site planning.
- (c) See Chapter V for landscape design elements.
- (d) See Chapter VI for site lighting.
- (e) See Chapter VII for utilities and service areas.

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Chapter III

Roads, Parking, Walkways and Amenities



Illustration courtesy of Bob Hines.

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Overview: This chapter concerns the relationship of the components of a roadway, including the traveled road section, on-street parking, storm water runoff collection system, pedestrian walkways, access points, and utilities, along with street amenities, such as landscaping and street trees, pocket parks, street lighting, and street furniture, that occurs within the public right of way. The overall goal is to ensure a coordinated system of roads, walkways, and amenities that protect and accommodate the needs of the pedestrian while allowing efficient vehicular circulation.

Roadway design is composed of the following elements: roads and sidewalks in the public right-of-way, including streets, sidewalks, and pedestrian walkways, and on-site circulation and parking, including vehicle circulation, parking, bicycle and pedestrian circulation, and coordination with transit stops.

A. Roads and Sidewalks in the Public Right-of-Way

Goals:

- To provide for connectivity between components of the community with design standards that ~~will~~ integrate neighborhoods and commercial ~~areas~~ ~~districts~~, thus providing for efficient vehicular circulation and pedestrian safety.

Roads, Parking, Walkways and Amenities

- 1 • To require all major road improvement projects to conform to Kitsap County road
2 standards and the Kingston Complete Streets Plan ~~Downtown Road Improvement Plan~~
3 and these design standards.
- 4 • To require all new development to mitigate direct traffic impacts to roads in Kingston
5 through construction of necessary roadway improvements, as warranted through traffic
6 impact analysis.
- 7 • To encourage existing development to participate in the formation of area-wide
8 improvement districts to fund desired roadway improvements.
- 9 • To encourage street designs and development patterns that accommodate pedestrians,
10 vehicles, transit users, and bicyclists in a balanced way.
- 11 • To minimize visual impacts of utilities, such as poles, wires, signal controller boxes,
12 and transformers.
- 13 • To move ferry terminal traffic efficiently through town while supporting Kingston
14 community objectives for a pedestrian-friendly and livable environment.
- 15 • To encourage existing development and require new development to participate in
16 areawide streetscape improvements as a means of building community, attracting
17 tourism, and ensuring economic prosperity.
- 18 • To set standards for road construction design that accommodates street tree plantings,
19 street furniture, and pedestrian lighting within the public right-of-way to provide for
20 an aesthetic route for both pedestrians and drivers.
- 21

22 1. Roadway Design

- 23 (a) Clearly mark and locate bicycle routes to form a network of routes that is
24 convenient and safe.
- 25
- 26 (b) Where feasible, design roads to control traffic through the use of traffic-calming
27 devices, such as the following:
 - 28 • Curb bulbs or neck downs at intersections or pedestrian crosswalks.
 - 29 • Reduced turning radius in high-pedestrian areas; to be applied to non-arterial
30 streets and not to exceed 25 feet radii in the Old Town/Waterfront and Village
31 Green Design Districts.
 - 32 • Large street trees, green planting strips, and pocket parks. (See Chapter V,
33 Landscaping.)
 - 34 • Offset parking along the length of both sides of a roadway where appropriate.
 - 35 • Changes in road surface, including changes in paving materials, texture, speed
36 tables, or color.
- 37
- 38 (c) Design roadways to meet the level of traffic volume projected as well as the land
39 use design objectives of the local area. ~~Street design for local access roads in the~~
40 ~~Kingston Urban Growth Area falls into the following five categories:~~
 - 41 • ~~Neighborhood Boulevard.~~
 - 42 • ~~Neighborhood Commercial Street.~~

Commented [PB16]: Informational Note:
These categories do not align with current functional road classifications in the Kitsap County Street Standards and are not needed here.

Roads, Parking, Walkways and Amenities

- 1 ~~• Residential Parking Street.~~
- 2 ~~• Residential Lane.~~
- 3 • ~~Alley.~~
- 4
- 5 (d) ~~When designing street~~ Street improvements, ~~consult shall be consistent with the~~
6 ~~Kitsap County road standards, the Kingston Complete Streets Plan, and these~~
7 ~~design standards~~ Design Standards for the Community of Kingston.
- 8
- 9 (e) Keep curb cuts narrow and far apart to minimize sidewalk interruptions. Turning
10 radii should be minimized in high-pedestrian areas and, in the Old
11 Town/Waterfront and Village Green Design Districts, to no more than 25 feet or
12 whatever is deemed necessary to meet County Emergency Access Requirements.
- 13
- 14 (f) Locate parking on-street where feasible within the constraints of right-of-way and
15 pedestrian needs to buffer pedestrians and slow traffic. Consult the Kingston
16 Complete Streets Plan.
- 17
- 18 (g) Plant street trees along all project frontages where feasible. Street tree selection
19 shall be coordinated between properties along road corridors according to the
20 Kingston Complete Streets Plan ~~Downtown Road Improvement Plan.~~ (See Chapter
21 V, Landscaping, for recommended species.) Trees shall be contained within a
22 planting strip or tree grate within the sidewalk. Select species that will not
23 obstruct desirable views at maturity. Select appropriate species and locate trees to
24 provide visibility of storefronts and signs under 12 feet in height.
- 25

26
27 **2. Sidewalk Construction**

- 28 ~~(a) Developer may sign a covenant of participation (in lieu of sidewalk construction~~
29 ~~when deemed appropriate by the County Engineer) on Local Road Improvement~~
30 ~~Districts for sidewalk and other improvements.~~
- 31
- 32 (b) Install sidewalks in widths determined on a basis of the road right-of-way width
33 available and objectives of the design standards Design Guidelines and the
34 Kingston Complete Streets Plan. Sidewalk width shall be as follows:
- 35

<u>Design District</u>	<u>Preferred Width</u>	<u>Min. Width</u>
<u>Old Town/Waterfront</u>	<u>12'</u>	<u>8'</u>
<u>Village Green</u>	<u>12'</u>	<u>8'</u>
<u>Lindvog Commercial</u>	<u>12'</u>	<u>6'</u>

- 36
- 37 ~~• Minimum sidewalk width shall be 8 feet, with a 12-foot minimum width at the~~
38 ~~building entry and a 4-foot minimum pedestrian clear zone provided.~~
- 39 • Where right-of-way width is constrained, additional land on the development
40 parcel ~~should~~ shall be allotted to accommodate sidewalk construction
41 requirements and implementation of the Kingston Complete Streets Plan
42 Downtown Road Improvement Plan, with a minimum of a 2-foot building

Commented [PB17]: Informational Note:
This section removed to avoid redundancy/conflict with Section E in the Introduction.

Commented [PB18]: Converted into a table at Jeff's request.

Commented [PB19]: Informational Note:
This 4-foot entry setback requirement is provided in Section IV.B.1.b.

Roads, Parking, Walkways and Amenities

- 1 ~~setback~~, provided that in the Old Town/Waterfront and Village Green Design
2 Districts, upper stories may extend an additional 2 feet into the public right-
3 of-way upon the approval of Kitsap County.
- 4 • Sidewalk width may be reduced to 5 feet for existing structures, and for new
5 development with a minimum 4-foot front yard setback, when constrained by
6 the right-of-way and when demonstrated to be consistent with the Kingston
7 Complete Streets Plan, Downtown Road Improvement Plan's Streetscaping
8 the streetscaping requirements in these design standards, and with existing
9 improvements.
 - 10 • ~~In the Lindvog Commercial Design District, minimum sidewalk width shall~~
11 ~~be 6 feet.~~
- 12
- 13 (c) For sidewalk surfaces, use concrete or an approved brick-paver inlay consistent
14 with the Kingston Complete Streets Plan Downtown Road Improvement Plan.
15 Vehicular crossings of pedestrian walkways shall be clearly marked and
16 identifiable, with surface treatments such as color changes, surface material or
17 texture changes, or slight grade changes.
- 18
- 19 (d) Locate public sidewalks primarily within the right-of-way area. In special
20 instances sidewalks may need to extend into private property to meet the
21 objectives of the Kingston Complete Streets Plan Downtown Road Improvement
22 Plan.
- 23
- 24 (e) Where feasible, plant street trees along all project frontages. Street tree selection
25 ~~should shall~~ be coordinated between the County and properties along road
26 corridors ~~according to~~ consistent with the Kitsap County road standards and the
27 Kingston Complete Streets Plan Downtown Road Improvement Plan. (See Chapter
28 V, Landscaping, for recommended species. Trees shall be contained within
29 planting strips or tree grates within the sidewalk.
- 30
- 31 (f) Provide lighting fixtures as described in Chapter VII, Lighting.
- 32
- 33 (g) For café zones or outdoor dining in the public right-of-way, ensure that outdoor
34 dining areas or sidewalk areas conform to all of the following:
- 35 • ~~Requirements of a minor Site Plan Review (Kitsap County Zoning Ordinance,~~
36 ~~Section 410).~~
 - 37 • Are lighted with low-intensity lighting that is directed down toward the seating
38 area and does not spill out onto adjacent properties.
 - 39 • Are contained within the property boundary to the rear or side area of a site,
40 within a screened fence or hedge. Such areas may be allowed along a building
41 frontage subject to the following criteria:
 - 42 • ~~Are subject to Administrative Site Plan Review (Kitsap County Zoning~~
43 ~~Ordinance, Section 410).~~
 - 44 • Provide clear separation between pedestrians and vehicles through use
45 of planters, decorative fencing, and low walls.

Commented [PB20]: Informational Note:
The referenced requirements are out of date.

Commented [PB21]: Informational Note:
The referenced requirements are out of date.

Roads, Parking, Walkways and Amenities

- Are able to provide 4 feet minimum of sidewalk clearance within all points of the designated area.



Figure III.A.1.(g). Attractive, appropriately sized trees add much to Kingston's identity.

B. On-Site Circulation and Parking

1. Vehicle Circulation

- (a) Where pedestrian circulation crosses vehicular routes, provide a change in grade, materials, textures, or colors to emphasize the conflict point and improve visibility.
- (b) Minimize the width of curb cuts while ensuring that the requirements of emergency service vehicles are met. Circulation routes shall focus upon main entries and exits and also identify secondary access points.
- (c) Site new driveways away from or immediately opposite street or alley intersections. Limit the number of driveways or alleys to no more than one driveway per every 300 feet for arterial roads and one per 150 feet for secondary and local access roads. Parcels without alley access and less than 150 feet in width may shall be required to share access and coordinate internal circulation with adjacent parcels whenever possible.

Access shall be provided in the following prioritized order:

- From the rear of the parcel or from alleyways alleys. Alleys or shared driveways built to alley standards shall be provided where shown in Appendix C. Alleys or shared driveways built to alley standards may be required in other locations.
- From the lesser traveled roadway.
- Shared with adjacent parcels.

Driveway and alley location and design are subject to approval by Kitsap County.

Roads, Parking, Walkways and Amenities

- 1 (d) Accommodate access requirements of emergency vehicles and services, per the
2 Kitsap County Fire Code, on all elements of the site design.
3
4 (e) Integrate service functions into the circulation pattern in a manner that ~~will~~
5 minimize conflicts between vehicles and pedestrians.
6
7 (f) Orient drive-through facilities in a way that ensures minimal disruption on the
8 street edge. Minimize curb cuts and the disruption of a sidewalk by:
9 • Making the width of the lane approaching the window no less than 10 feet
10 wide; and
11 • Using landscaping and planters or other approved feature to provide a street
12 edge adjacent to the sidewalk.
13

Commented [PB22]: Informational Note:
This new section integrates unique language from KCC
17.480.095 Building design – Commercial use with ancillary
drive-through component

14 2. Parking

15 Parking shall be provided as required by KCC 17.490 (Off-Street Parking and
16 Loading) and these design standards.
17
18

19 **Parking Objectives Goals:** ~~Typically, the largest percentage of land use in a~~
20 ~~commercial district area is parking.~~

- 21 • ~~Parking areas should be designed so that the use is not visually intrusive and does~~
22 ~~not detract from the goal of providing a “pedestrian-friendly” downtown.~~
23 • ~~In the future, the pedestrian only ferry system will~~ Ferry and transit systems should
24 ~~encourage the~~ encourage commuters and tourists traveling through the downtown to do so on
25 foot or by transit shuttle from commuter lots. This will help maintain the majority
26 of the downtown streets for local access and pedestrians interested in strolling the
27 sidewalks of downtown. ~~To fulfill this goal, relaxation of the current required~~
28 ~~parking standards may be necessary in the Old Town/Waterfront and Village~~
29 ~~Green Districts, with demonstration that parking is available within close~~
30 ~~proximity.~~
31 • ~~Shared parking or public lots should be encouraged and could be financed by~~
32 ~~private partnerships, an improvement district, or public entities.~~

33 ~~This may be shared parking or public lots that could be financed directly by a~~
34 ~~partnership between development proposals and the general community. Project~~
35 ~~proposals that exceed on-site parking capacity may contribute payment in lieu to~~
36 ~~structured parking funds held in an escrow account to support construction of a parking~~
37 ~~garage.~~
38

39 **Design Standards:**

- 40
41 (a) Locate parking to the side or rear of a site, except in these instances where there is
42 an existing structure with no feasible means of accommodating parking to the side
43 or rear. (See Chapter II, Site Planning.)
44

Roads, Parking, Walkways and Amenities

- 1 (b) Create public parking areas behind buildings or in structured parking facilities
2 (e.g. an under building or multi-level parking garage), using under-utilized public
3 land where possible and good directional signs.
- 4
- 5 ~~(c) Provide incentives for developments to locate parking to the rear of buildings and~~
6 ~~underneath structures.~~
- 7
- 8 ~~(d) Connect parking areas on adjacent properties or create service alleys behind~~
9 ~~buildings.~~
- 10
- 11 (e) Soften the visual impact of parking areas with landscaping, trees and shrubs
12 where feasible. In site design of parking areas, incorporate the use of screening,
13 such as hedge material or planters, to screen parking from pedestrian routes or
14 adjacent properties. (See Chapter V, Landscaping.)
- 15
- 16 ~~(f) Limit parking and vehicle access to no more than 65 feet or 50 percent, whichever~~
17 ~~is greater, of the street frontage measured parallel to the curb.~~
- 18
- 19 ~~(g) In parking lot design, coordinate access with adjacent properties, if possible. This~~
20 ~~is especially critical in the Old Town/Waterfront and Village Green Districts. (See~~
21 ~~Chapter III, Section B.1. Guideline (e).)~~
- 22
- 23 (h) Line structured parking garages with perimeter tenant commercial spaces or
24 mixed uses. Structured parking shall meet minimum site and building design
25 requirements as prescribed herein.
- 26
- 27 (i) On-street parking spaces within the right-of-way adjacent to the lot may be
28 counted to satisfy the minimum off-street parking requirements for commercial
29 development, as provided the on-street parking spaces are approved by the
30 Department of Public Works and consistent with the Kingston Complete Streets
31 Plan. In no instance shall on-street parking be indicated by a sign or considered
32 dedicated to any parcel or business.
- 33

Commented [PB23]: Informational Note:
Incentives have been added to Chapter IV, Section C.1.(a)
and KCC 17.490.020.

Commented [PB24]: Informational Note:
Redundant to standard in Chapter III, Section B.1.(c).

Commented [PB25]: Informational Note:
Redundant (and slightly less restrictive) to standard in
Chapter II, Section A.1.(f).

Commented [PB26]: Informational Note:
Redundant to standard in Chapter III, Section B.1.(c).

34 **3. Bicycle and Pedestrian Circulation**

35 ~~**Definition:** Pedestrian walkways are both formal standardized public walkways and~~
36 ~~informal paths worked into a site's landscape design that provide a means for~~
37 ~~pedestrians to travel through the community along street sidewalks or other public~~
38 ~~routes.~~

39

40 **Goal:** To improve the pedestrian environment by making it easier, safer, and more
41 comfortable to walk between businesses, to the street sidewalk, to transit stops, and
42 through parking lots.

43 **Objectives:**

- 44
- 45 • To provide pedestrian facilities such as sidewalks, crosswalks and bus shelters that
46 ~~will~~ connect all modes of transportation, including auto, bus, ferry, van pools, and

Roads, Parking, Walkways and Amenities

- 1 bicycles.
- 2 • To provide attractive, safe, continuous pedestrian access routes in the downtown
- 3 area of Kingston that connect uses and public spaces in a system of public
- 4 sidewalks and private walkways.
- 5 • To provide safe pedestrian routes across busy streets, including Highway 104, by a
- 6 variety of means, including signalized intersections and distinctively marked
- 7 crosswalks.
- 8 • To provide pedestrian-/bicycle-oriented routes from the ferry terminal to
- 9 commuter parking lots.
- 10 • To make pedestrian circulation routes as obvious and simple as possible,
- 11 illustrating a clear, identifiable path between vehicles and access into buildings or
- 12 sites.

13 **Design Standards Guidelines:**

- 14 (a) Provide a clear unobstructed route for pedestrians along all public roadways.
- 15
- 16 (b) Provide pedestrian paths or walkways connecting businesses and the entries of
- 17 multiple buildings on the same site. All likely pedestrian routes should be
- 18 considered in the design phase to eliminate “short cuts” which damage landscape
- 19 areas.
- 20
- 21 (c) Where pedestrian circulation crosses vehicular routes, provide a change in grade,
- 22 materials, textures or colors to emphasize the conflict point and improve its
- 23 visibility and safety.
- 24
- 25 (d) Focus circulation routes upon main entries and exits and identify secondary access
- 26 points.
- 27
- 28 (e) Minimize redundant pavement that reduces the amount of a site available for
- 29 landscaping and storm water absorption.
- 30
- 31 (f) ~~For the downtown commercial districts, provide~~ Provide the following:
- 32
- 33 • ~~Streetscaping that addresses the requirements of the Site Plan Review (Kitsap~~
- 34 ~~County Zoning Ordinance, Section 410).~~
- 35 • A paved pedestrian path of at least 5 feet wide from the public street sidewalk
- 36 to the building main entry.
- 37 • A clear pedestrian path into parking lots over 150 feet in depth or 2 parking
- 38 bays (approx. 65 feet) wide. Path shall be a minimum of 4 feet wide and
- 39 delineated through striping, texturing, or paving to mark the route.
- 40 • A concrete public sidewalk along the property road frontage that is consistent
- 41 with ~~the Kitsap County road standards and~~ the Kingston Complete Streets Plan
- 42 Downtown Road Improvement Plan.
- 43 • Adequate lighting at the building entries and along all walkways and paths
- 44 through parking lots. (See Chapter VII, Lighting.)

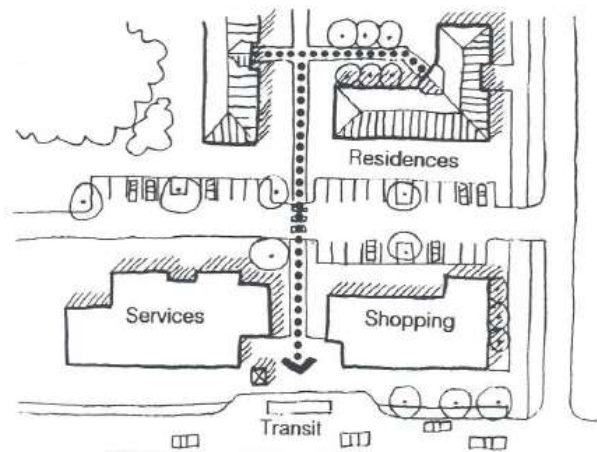
Commented [PB27]: Informational Note:
The referenced requirements are out of date.

Roads, Parking, Walkways and Amenities

- 1
2 (g) Make all site facilities and amenities accessible to people with disabilities in
3 accordance with the Americans with Disabilities Act. Accessibility requirements
4 include the provision of special parking spaces, ramps, and signage and shall be
5 noted on the approved site plan.
6
7 (h) Design circulation systems to avoid conflicts between vehicular, bicycle, and
8 pedestrian traffic. Pedestrian circulation shall take precedence over vehicular
9 circulation in safety considerations.
10
11 (i) Provide bicycle storage spaces with commercial development at the rate of 5
12 percent, minimum, of the number of auto parking spaces.
13
14 (j) Construct internal site walkways a minimum of 5 feet in width. A lesser walkway
15 width is supportable in low pedestrian traffic areas, unless required for handicap
16 access, where the minimum width shall be 4 feet constructed to A.D.A. standards.
17

18 **4. Coordination with Transit Stops**

- 19 (a) Encourage pedestrian paths from all transit stops, including those specified in the
20 Kingston Complete Streets Plan, through commercial areas to residential areas
21 within 1,200 feet by the creation of easements for coordinated pedestrian routes
22 across projects and properties as projects are developed.
23



24 *Figure III.B.4.(a). In this large-scale commercial site, wide landscaping walkways provide*
25 *pedestrian connections.*
26
27
28

- 29 (b) Integrate nearby transit stops, including those specified in the Kingston Complete
30 Streets Plan, into the planning of adjacent site improvements. This may be

1 achieved by:

- 2 • Providing bus pull-outs or transit stops into the site circulation scheme.
3 • Providing a walkway directly from the transit stop into the project's entrance.
4 • Pavement provided for transit-waiting areas may account for landscape area
5 requirements.

6
7 (c) Provide pedestrian circulation routes to adjacent uses and transit stops, including
8 those specified in the Kingston Complete Streets Plan, by providing access from a
9 building/business entry to connect with:

- 10 • Building entries of businesses and services within the development.
11 • Building entries of nearby residential complexes.
12 • Sidewalks along all developer-provided roads.
13 • Marked crosswalks.
14

15 5. On-Site Pedestrian Amenities and Spaces

16 ~~**Definition:** The streetscape, the visual and functional supporting elements of a
17 roadway design, provides aesthetic interest and comfort to the pedestrian. Street
18 amenities serve to define the public space of a sidewalk as well as the adjacent
19 roadway corridor. Pedestrian amenities include private spaces such as pedestrian-
20 friendly plazas, furniture, lighting, and art. Pedestrian-oriented spaces and plazas are
21 defined as an area between a building and a public street or pedestrian path which
22 promotes visual and pedestrian access onto the site and which provides amenities and
23 landscaping that enhance the public's use of passive activities, such as resting,
24 reading, and picnicking.~~

25
26 **Goal:** To ensure a coordinated system of street corridor improvements that protect and
27 accommodate the needs of the pedestrian while allowing efficient vehicular circulation.
28

29 **Design Objective:** To provide functional amenities for the pedestrian that are safe,
30 comforting, and aesthetically pleasing.
31

32 Design Standards Guidelines:

33 (a) *Front Yards.* Where the front building façade is not directly adjacent to the
34 sidewalk, develop the space between the sidewalk pavement and the building (the
35 front yard) as a garden, lawn, and/or pedestrian-oriented space.
36

37 A pedestrian-oriented space is encouraged to have:

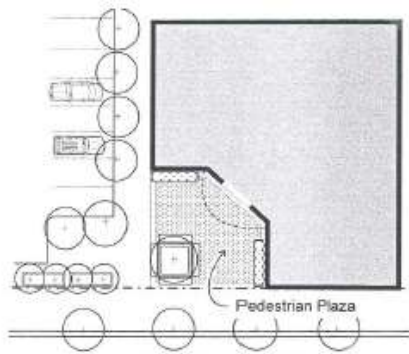
- 38 • Landscaping that does not act as a visual barrier, such as planter beds.
39 • Site furniture, artwork, or amenities such as fountains, kiosks, furniture.
40 • Pedestrian weather protection.
41

Roads, Parking, Walkways and Amenities

- 1 • Pedestrian-oriented landscaping along walkway; e.g., flower boxes or planter.
2 ~~All uses shall provide street trees and street lighting as appropriate. All new~~
3 ~~uses shall participate in future improvement districts for business/community~~
4 ~~aesthetic enhancement.~~
- 5 • Space for a transit stop with seating.
- 6 • Window displays over the majority of the front façade.
- 7 • Decorative screen wall, trellis, or other building or site feature.
- 8 • Pedestrian lighting.
- 9 • Street trees.

10 A pedestrian-oriented space shall not have:

- 11 • Asphalt or gravel pavement.
- 12 • Adjacent unscreened parking lots.
- 13 • Adjacent chain-link fences.
- 14 • Adjacent blank wall without treatment.



17
18 *Figure III.B.5.(a)-1. The orientation of this pedestrian plaza is well suited to provide a pleasant*
19 *outdoor space, convenient pedestrian access, and landscaping enhancement of the building.*
20

Commented [PB28]: Informational Note:
Removed from this bullet because street trees and street lighting are listed separately below.

Improvement Districts are addressed in Section E of the Introduction.

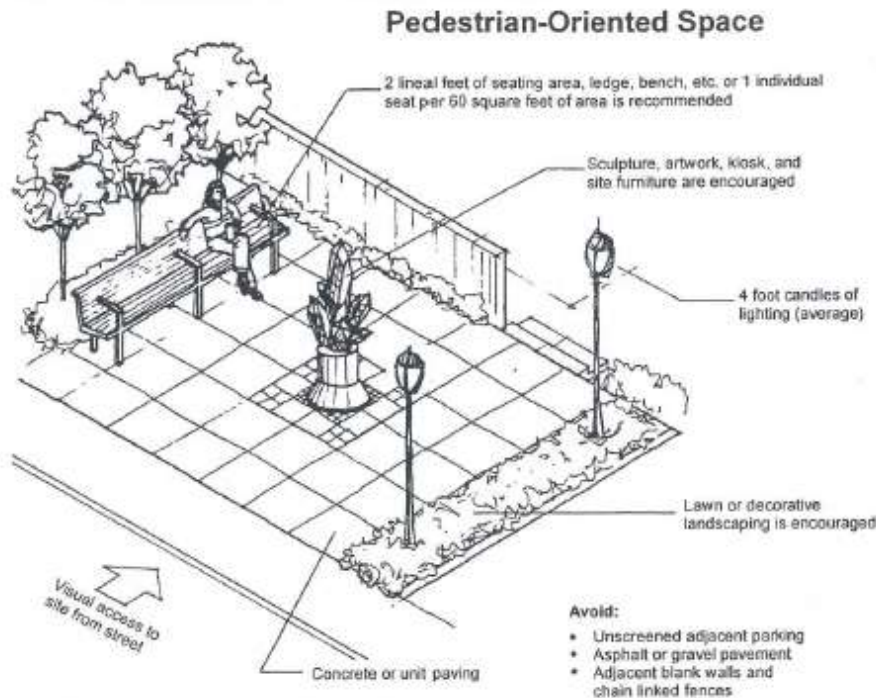


Figure III.B.5.(a)-2. Characteristics of a pedestrian-oriented space.

(b) *Street Furniture.* Provide two pieces of approved pedestrian furniture or other site amenities for every 100 feet of property frontage. Street furniture shall be as approved by Kitsap County. Components of street furniture include:

- Pedestrian seating.
- Artwork.
- Drinking fountains.
- Kiosks or public information boards.
- Refuse receptacles.
- Tables and chairs.
- Guard railings.
- Decorative lighting.
- Bicycle racks.
- Space for transit stop seating.
- Other elements as approved by the County.

Roads, Parking, Walkways and Amenities

1
2 (c) *Site Lighting*. Provide lighting at all building entrances, exit points, and
3 pedestrian-oriented space. ~~Provide a minimum of two foot candles for pathways~~
4 ~~and four foot candles for entry ways. On building lighting shall be down spot~~
5 ~~lighting or front lighted.~~ Specific lighting detail standards are addressed in
6 Chapter VII, Lighting.
7

Commented [PB29]: Informational Note:
These requirements are provided in Chapter VII.

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Chapter IV

Architectural/Building Design

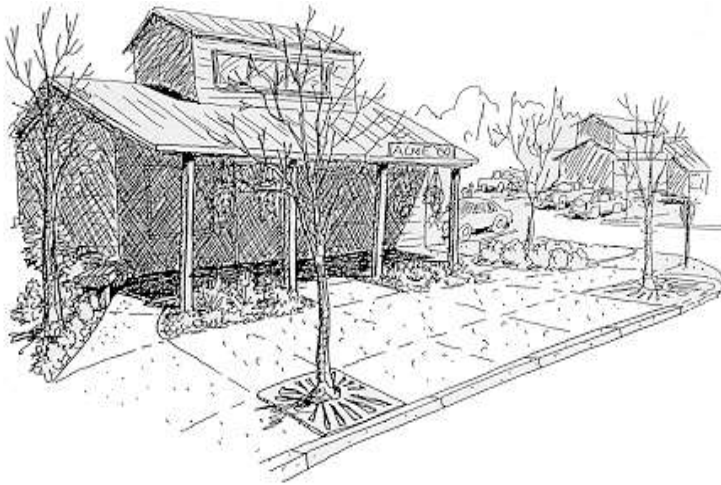


Illustration courtesy of Bob Hines.

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Definition: ~~The building scale, mass, form, size, color, and materials as they relate to the site and adjacent structures and properties.~~

10 **Goal:** To maintain the “old town” character of the Kingston maritime community through
11 incorporation of architectural design features that ~~will~~ promote architectural compatibility as well
12 as diversity.

13
14 **Objectives:**

- 15 • To encourage and promote development that features excellence and comfortable
16 amenities by incorporating human-scale elements in building design.
- 17 • To encourage creative approaches to the use of land and related physical improvements.
- 18 • To encourage the enhancement and preservation of land or buildings of unique or
19 outstanding scenic or historical significance.
- 20 • To strengthen the architectural identity of the community. Examples of buildings with
21 desirable architectural characteristics are presented in Figures IV-1 through IV-2.

Architectural/Building Design



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Figure IV-1. The Kingston Hotel is perhaps the community's best example of the Old Town character. Its simple form, traditional details and materials, and understated Victorian ornamentation make it "classic Old Town Kingston." The covered porch, view deck, and modest front yard landscaping present a welcoming feeling.



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Figure IV-2. This contemporary interpretation of traditional architecture would be appropriate in all three downtown design districts. The simple gable form, traditional siding, appropriate sign, welcoming porch, and transparent front façade make it pedestrian friendly and consistent with the town's identity. Note how the multi-paned windows, porch railing, and porch column details all add a simple grace and human scale without being overly ornamental. The colors, light gray with white trim, are appropriate. The landscaping also enhances the development's qualities.

1 A. Compatibility

2 **Goals:** To encourage compatibility in scale, mass, form, and design character between
3 ~~commercial buildings and the~~ structures and the pattern of development within the
4 ~~downtown design~~ districts.

5
6 Exterior building design and detail on all elevations visible from adjacent properties or a
7 public right-of-way should be coordinated with regard to color, types of materials, number
8 of materials, architectural form, and detailing to achieve harmony and continuity of design.
9

10 1. Design Character

11 (a) Be prepared to demonstrate ways in which the proposed structure ~~responds to its~~
12 ~~district context~~ conforms to these design standards through its architectural scale,
13 roof form, building details, windows, materials, and signs. The design
14 characteristics are described below and in the guidelines design standards for the
15 characteristics mentioned. Building design should invoke the “old town” character
16 of the maritime community through incorporation of neo-traditional architectural
17 design standards on the side of building facades that face a public street or park,
18 such as:

- 19 • Steepened roof lines or appearance of steepened roof lines (greater than 6:12
20 pitch).
- 21 • Multiple gables.
- 22 • Dormers.
- 23 • False front.
- 24 • Multiple-paned windows with clerestories.
- 25 • Front porches.
- 26 • Corner and window trim details.

27
28 (b) Carefully design rear and side facades visible from public streets or neighboring
29 properties with similar detailing as the principle facades of the building to
30 maintain compatibility.

31
32 (c) The ground floor of buildings along designated pedestrian-oriented commercial
33 street frontages (see Appendix B) shall be built for commercial use or eventual
34 conversion to commercial occupancy along the street frontage. These commercial
35 spaces may initially be used for residential use.

36
37 (d) Orient multifamily buildings to provide privacy, to the extent practical, both
38 within the multifamily project and for the neighborhood.

- 39 • Locate windows, to the greatest extent practical, so that residents from one
40 unit cannot look directly into another unit;

Commented [PB30]: Informational Note:

This new section added to ensure the long-term viability of a pedestrian-oriented commercial core to downtown Kingston while providing flexibility for property owners to maximize occupancy of their buildings.

Commented [PB31]: Informational Note:

This new section integrates unique language from KCC Section 17.480.190 Multifamily – Building design – Privacy

Architectural/Building Design

- 1 • [Locate parking lots so that they do not impose on the ground floor units' privacy. If this is not feasible, locate buildings so that adequate \(8-foot](#)
 2 • [minimum\) landscaping can be planted to provide privacy.](#)
 3
 4
 5 (e) [Drive-throughs.](#)
 6 • [Locate the main pedestrian entry on a pedestrian-oriented street for a bank, dry](#)
 7 • [cleaner, coffeehouse, or other commercial use with an ancillary drive-through](#)
 8 • [component.](#)
 9 • [Design the drive-through window so that it is clearly subordinate to the main](#)
 10 • [building.](#)
 11 • [Where the drive-through is a separate structure, use architectural details that](#)
 12 • [conform to those used on the main building.](#)
 13
 14 (f) Submit all elevations of the building ~~to~~ [for](#) building permit/design review
 15 evaluation.
 16

Commented [PB32]: Informational Note:
This new section integrates unique language from KCC 17.480.095 Building design – Commercial use with ancillary drive-through component

17 2. Building Height
 18

Workgroup Rec.	<p>In the Kingston Commercial Districts design districts, do not exceed the following building height limits:</p> <ul style="list-style-type: none"> • Old Town/Waterfront: 3 stories, not to exceed 35 45 feet. Minimum two-story construction for commercial, mixed-use, and multi-family buildings. An upper story setback of at least 10 feet along all facades abutting road frontages (not including alleys) is required for buildings more than 35 feet in height (see also Chapter IV, Section C.1). • Village Green: 4 stories, not to exceed 45 feet. Minimum two-story construction for commercial, mixed-use, and multi-family buildings. • Lindvog Commercial: 3 stories, not to exceed 35 feet. No minimum. As allowed by the Kitsap County Zoning Ordinance.
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Architectural/Building Design

DCD Rec.	<p>In the Kingston Commercial Districts, do not exceed the following building height limits:</p> <ul style="list-style-type: none"> • Old Town/Waterfront: 3 stories, not to exceed 35 feet. Minimum two-story construction. • Village Green: 4 stories, not to exceed 45 feet. Minimum two-story construction. • Lindvog Commercial: 3 stories, not to exceed 35 feet. No minimum. <p><u>The following building height requirements shall apply:</u></p>			
	<u>Design District</u> →	<u>Old Town/ Waterfront</u>	<u>Village Green</u>	<u>Lindvog Commercial</u>
	<u>Maximum building height</u>	<u>45'</u>	<u>45'</u>	<u>45'</u>
	<u>Minimum number of stories for commercial buildings</u>	<u>2</u>	<u>2</u>	<u>1</u>
	<u>Minimum number of stories for mixed-use and multi-family buildings</u>	<u>2</u>	<u>2</u>	<u>2</u>
<p><u>Upper story setbacks for buildings over 35' in height are required (see Chapter IV, Section C.1).</u></p>				

Commented [PB33]: Informational Note:
Old Town/Waterfront & Lindvog Commercial districts increased from 35' to 45' to be consistent with surrounding districts and zones, which are mostly 45'. Limited surrounding areas have 55' and 35' height limits.

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“Building height” is defined in the Kitsap County Zoning Ordinance.

~~Maximum~~ The number of stories is determined from the sidewalk abutting the main entrance, ~~or from the sidewalk along the abutting road frontage.~~

B. Human/Pedestrian Scale

The incorporation of defined outdoor spaces into the building and site designs of all new development ~~in the commercial district~~ is encouraged. Encouraged outdoor spaces include courtyards, patios, plazas, covered walkways, passages, gardens, and trellised areas. Buildings should incorporate design elements that ~~will~~ accommodate the needs of the pedestrian and provide pedestrian interest. Buildings should incorporate design elements that reduce the apparent scale of buildings. The following design elements and techniques ~~should~~ shall be incorporated into building and site design:

1. Pedestrian-Oriented Façades

- (a) For all development, include at least three of the following design elements or techniques:
 - Sculptural, mosaic, or other architectural details.
 - Transparent window area or window displays at or below eye level along at least 50 percent of the length of the ground floor façade for retail.

Architectural/Building Design

- 1 • Pedestrian weather protection.
- 2 • Decorative light fixtures.
- 3 • Landscape trellises.
- 4 • Decorative building materials, including decorative masonry, shingles, brick,
- 5 or stone.
- 6 • Individualized patterns or continuous woods details such as fancy butt shingles
- 7 in a geometric pattern, decorative moldings, brackets, wave trim or lattice
- 8 work, ceramic tile, stone, glass block, or similar materials.
- 9 • Other materials with decorative or textural qualities as approved.
- 10 • Gable or hipped roof, provided the hipped or gable roof covers at least one
- 11 half of the building's footprint and has a slope greater or equal to 6 feet vertical
- 12 in 12 feet horizontal.
- 13 • Building articulation, with the upper story set back from the face of the
- 14 building at least ~~6~~ 10 feet or projecting out (forward) at least 2 feet.
- 15 • Decorative artwork.
- 16

Commented [PB34]: Informational Note:
Modified for consistency with changes to Chapter IV,
Section C.1.



17
18 *Figure IV.B.1.(a). The front of this tiny coffee shop features many pedestrian-oriented elements,*
19 *including weather protection, decorative lights, modest but effective ornamentation, traditional*
20 *siding, recessed entry, and large display windows. Note the outdoor table and chairs. Add the*
21 *smell of fresh brewed coffee and you have a customer magnet, not only for the coffee shop, but*
22 *neighboring businesses as well.*
23
24

- 25 (b) Provide a porch, covered entry, or other building element that defines an outdoor
- 26 space, such as a trellis, overhang, or canopy. Entries should include weather
- 27 protection, planters, and building façade artwork. Entries ~~should~~ shall be inset into

Architectural/Building Design

1 the building façade; accordingly, provide a minimum 4-foot setback from the
2 internal edge of the sidewalk. Dark, hidden corridors or stairways and long entry
3 balconies are discouraged. Avoid the use of exterior stairways when porches and
4 front doors can be used as a primary building entry. If exterior stairways are used,
5 they should shall fit with the architectural massing and form of the structure.
6

Commented [PB35]: Informational Note:
This integrates unique language from KCC 17.480.220
Multifamily – Building Design – Entries



7



8
9 *Figure IV V.B.1.(b)-1. Two examples of porches. Though simple in the extreme, the top porch*
10 *adds a bit of comfort and protection to an otherwise plain façade. The bottom example*
11 *illustrates how a covered entryway can be effectively integrated into building design.*
12

Architectural/Building Design

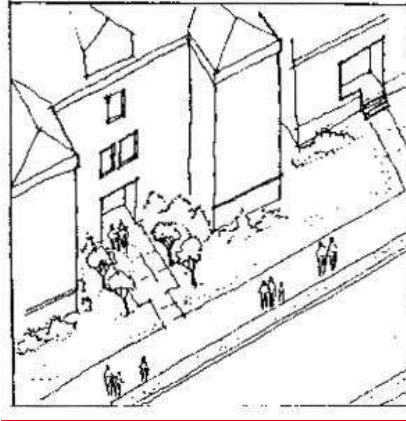


Figure IV.B.1.(b)-2. Clear entries to the sidewalk encourage pedestrian circulation.

Commented [PB36]: Informational Note:
This incorporates a relevant figure from KCC 17.480.220
Multifamily – Building Design – Entries

- (c) Treat code-required elements, such as parapet walls and screen walls, as an integral part of the architecture.
- (d) Minimize the visibility of a flat roof through the use of a false front and gables. Direct a flat roof away from, or screen it from, public spaces and street fronts.



Figure IV.B.1.(d). The Ben Franklin store façade incorporates metal roof arcades and a gabled entryway to add interest and human scale to a concrete masonry unit building.

Architectural/Building Design

- 1 (e) For all buildings located along designated ~~streets~~ pedestrian-oriented commercial
2 street frontages (see Appendix B) in the Old Town/Waterfront Design District,
3 provide pedestrian weather protection at least 3 feet wide for a minimum of 80
4 percent of the building’s façade. The weather protection may be in the form of
5 awnings, marquees, canopies, or building overhangs. Awnings may extend to
6 within 4 feet of the roadway curb, subject to Kitsap County building permit
7 review.
8

9 **C. Architectural Scale**

10 The following set of guidelines design standards are intended to address building design
11 details as might be seen at the scale of a pedestrian. Buildings that are stylized in an
12 attempt to use the building itself as advertising shall be discouraged, particularly where
13 the proposed architecture is the result of a “corporate” or franchise style.
14

15 **1. Architectural Scale**

- 16 (a) For all new buildings and additions over two stories and more than 5,000 square
17 feet in gross building footprint, provide at least three of the following features
18 along the facades visible from the public right-of-way and pedestrian routes.
19

20 For all new buildings and additions more than exceeding 35 feet in height
21 (regardless of gross building footprint area), provide an upper story setback of at
22 least 10 feet along all facades abutting ~~road street~~ frontages (not including alleys).
23 In addition, provide at least three of the ~~other~~ following features along the facades
24 visible from the public right-of-way and pedestrian routes.
25

26 ~~• Upper story setback.~~

- 27 • Horizontal building modulation. The maximum width without building
28 modulation shall be 75 feet, measured horizontally. The minimum depth of
29 modulation shall be 6 feet. Roof decks or balconies may be used as all or part
30 of the building modulation so long as each individual balcony has a floor area
31 of at least 100 square feet.
- 32 • Modulated roof line, as seen from a public right-of-way or designated public
33 space. No individual segment of a roof ~~may shall~~ exceed 50 feet in length
34 without a change in slope of 22.5 degrees or greater.
- 35 • Building articulation with design elements with a 50 foot-maximum spacing,
36 such as the following:
- 37 • Repeating window patterns.
 - 38 • Including a porch, patio, deck, or covered entry for each interval.
 - 39 • Including a balcony or bay window for each interval.
 - 40 • Changing the roofline by alternating dormers, stepped roofs, gables, or
41 other roof elements to reinforce the modulation or articulation interval.
 - 42 • Changing materials.

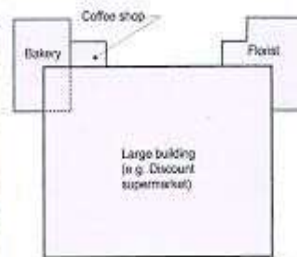
Commented [PB37]: Informational Note:
Upper story setbacks are mandatory for buildings over 35’ in
the new paragraph above. For buildings under 35’, upper
story setbacks remain a design feature listed above in
Chapter IV, Section B.1.(a).

Architectural/Building Design

- 1 • Providing a lighting fixture, trellis, trees or other landscape feature within
- 2 each interval.
- 3 • Substantial landscaping and/or pedestrian-oriented open spaces along the
- 4 building façade.

Workgroup Rec.	<ul style="list-style-type: none"> • Underground parking which will substantially replace(s) surface parking on-site.
DCD Rec.	<ul style="list-style-type: none"> • Substantial replacement of on-site surface parking with parking underground and/or in a structured parking facility (e.g. an under building or multi-level parking garage) located on-site and/or, if otherwise allowed, off-site. Replacements shall occur in the following priority order: <ul style="list-style-type: none"> • Off-street surface parking along street frontages (not including alleys). • Off-street surface parking fronting residences. • Other off-street surface parking.

- 6
- 7 • Other proposed methods that satisfy the intent of the design principles of this
- 8 document.

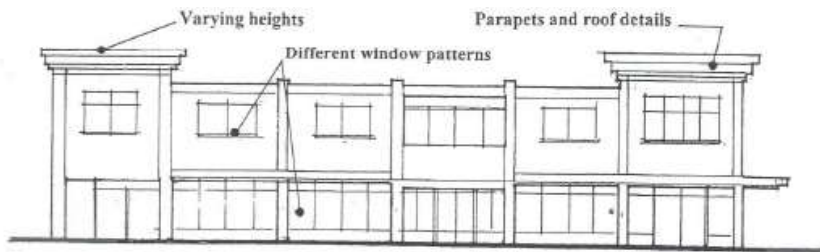


10 *Figure IV.C.1.(a). Clustering smaller activities around the entrance to a large building adds*
 11 *human scale and minimizes the large building bulk. The Thriftway Market effectively*
 12 *incorporates this concept, adding human scale and visual interest to its entrance.*
 13
 14
 15
 16

Architectural/Building Design

- 1 (b) Avoid long or continuous wall planes, ~~particularly in the pedestrian districts,~~
2 ~~where buildings~~ Buildings should exhibit more detail and elements appropriate for
3 close-range pedestrian view.
4
5 (c) ~~Outside the Old Town/Waterfront and Village Green Districts,~~ relieve building
6 surfaces over two stories high or 50 feet in length with articulation or changes in
7 wall plane that provide shadow or visual interest.
8

Commented [PB38]: Informational Note:
Removed reference to select districts since complementary standards (a) and (b) apply to all districts.



9
10 Figure IV.C.1.(c). The roofline, windows, and details of these examples are coordinated to provide a
11 rhythmic breakup of an otherwise horizontal building. Note how the awnings and lights provide
12 human scale while their repetition relates back to the building massing.
13
14

Architectural/Building Design

Workgroup Recommendation	<p>(d) <u>Architectural scale of those portions of a multifamily building facing a neighborhood with a different scale shall use design techniques that minimize the contrast in scale. Use house-size building elements when locating a multifamily project adjacent to a single-family neighborhood by employing any of the following techniques:</u></p> <ul style="list-style-type: none"> • <u>Place one- and two-story units adjacent to existing one-story houses, and two- and three-story units adjacent to existing two-story houses.</u> • <u>Use wall plane articulation/modulation to break a multifamily building into house size building elements, especially where there is a building height transition.</u> • <u>Design the exterior of multifamily buildings to appear as a single building, such as a large single-family detached dwelling.</u>
DCD Rec.	<p>(d) <u>Where a multi-family, commercial, and mixed-use building shares a common lot line with a residential lot in a single-family subdivision, use design techniques that minimize the contrast in architectural scale, such as:</u></p> <ul style="list-style-type: none"> • <u>Place one- or two-story buildings adjacent to existing one-story houses, and two- or three-story buildings adjacent to existing two-story houses.</u> • <u>Use wall plane articulation/modulation to break a building into house size building elements, especially where there is a building height transition.</u>

Commented [PB39]: Informational Note:
This integrates unique language from KCC 17.480.190 Multifamily – Building Design – Neighborhood scale

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D. Building Materials

1. Exterior Materials

(a) Use durable and high-quality exterior materials. Highly reflective materials are not allowed. Materials ~~should~~ shall be those of typical use in the Northwest, including:

- Bevel or lap siding.
- Rock, stone, and brick material.
- Architectural shake-style roofing.
- Metal roofs.

(b) If sheet materials, such as composite fiber products or metal siding, are used as a siding material over more than 25 percent of a building’s façade, use material with a matted finish in a color ~~as specified in Section IV.D.2.~~ Include the following elements:

- Visible window and door trim painted or finished in a complementary color.
- Corner and edge trim that covers exposed edges of the siding material.

Commented [PB40]: Informational Note:
If alternative Appendix D is adopted, then this should reference Appendix D.

See note regarding colors in Section IV.D.2. below.

Architectural/Building Design

- 1 (c) If concrete blocks (concrete masonry units or “cinder blocks”) are used for walls
2 that are visible from a public street or park, use one or more of the following
3 architectural treatments:
- 4 • Use of textured blocks with surfaces such as split-face or grooved.
 - 5 • Use of colored mortar.
 - 6 • Use of other masonry types, such as brick, glass block, or tile, in conjunction
7 with concrete blocks.
- 8
- 9 (d) Do not use the following materials in visible locations unless an exception is
10 granted by Kitsap County.
- 11 • Mirrored glass.
 - 12 • Corrugated fiberglass.
 - 13 • Chain-link fences in front yards.
 - 14 • Synthetic materials with reflective surfaces, including galvanized steel and
15 glossy vinyl siding.
- 16
- 17 (e) Select all vents, gutters, downspouts, flashing, electrical conduits, etc., to match or
18 complement the color of the adjacent surface unless they are being used expressly
19 as a trim or accent element.
- 20
- 21 (f) Provide approved address numbers so that they are legible to the public from the
22 street fronting the property.
- 23
- 24 (g) Minimize the visibility of rooftop equipment by grouping all plumbing, vents,
25 ducts, and rooftop mechanical equipment away from the public view through use
26 of architectural screening details, such as parapets, walls, and rooflines.
- 27
- 28 (h) Where possible, retain original facades that conform to the above guidelines
29 design standards on older buildings. Do not use metal siding, metal screening,
30 plastic, plywood, sheet wood products, or fiberglass to cover existing facades. The
31 County may allow synthetic siding on existing buildings, provided the material
32 and details are consistent with the original architecture.
- 33



2. Colors

~~Kitsap County will keep a sampling of acceptable color ranges from the historical paint colors from major paint companies or colors of traditional materials, such as wood, brick, or slate.~~

Commented [PB41]: Informational Note:
DCD recommends removing color requirements from the design standards because they are very difficult to administer and enforce and what is 'acceptable' is subjective.

Alternatively, a specific color palette can be provided in Appendix D. The alternative Appendix D is the only color palette staff has record of from when the design standards were originally adopted.

E. Building Equipment and Service Areas

1. Mechanical Equipment

- (a) Locate, design, and/or screen building mechanical equipment to minimize visual impact on public streets, large surface parking fields, and neighboring properties.
- (b) Screen plumbing vents, HVAC equipment, and other building equipment from public view as feasible.

2. Service Areas

- (a) See Chapter VIII, Section A.1.

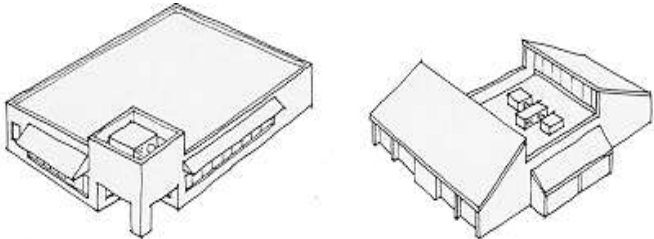


Figure IV.E.1.(b). Mechanical equipment and service areas ~~should be~~ are screened from view.

F. Accessory Dwelling Units (ADUs) and Accessory Living Quarters (ALQs)

Goals: To encourage compatibility of ADUs and ALQs with the structures and pattern of development within the ~~downtown design~~ districts and achieve harmony and continuity of design with the primary residence.

In addition to 17.410.060.B, ADUs and ALQs ~~must shall~~ also comply with all design standards applicable to single family residences as well as standards in this section.

1. Design Character

- (a) Use a roof ~~form style~~ and roof pitch, ~~and as well as~~ window and door ~~form style~~ and arrangement that looks like the primary residence. ~~In general~~Where possible, the roof ridge of the primary residence ~~shall~~ be higher than the ADU. An obvious exception is when the accessory unit is built onto the second story of an existing ~~unit structure or garage~~, or topography of the land causes the base elevation of the ADU to be higher than the base elevation of the primary residence.
- (b) To the extent practical, maintain privacy of adjoining ~~residences dwellings~~, and the primary ~~residences dwelling~~. Use a combination of landscape screening, fencing and window and door placement so that residents cannot look directly into the windows, porches and decks of adjoining ~~residences dwellings~~.

2. Entry

- (a) Provide a clearly defined building entry, which is well lighted, easily accessible and integral to the building structure.
- Entries should be plainly visible from the fronting street sidewalk. If the entry cannot be seen from the fronting street sidewalk, a well-defined walkway (e.g., constructed of contrasting materials or lined with a pattern of shrubbery) ~~should shall~~ be used to “lead” the visitor to the entry.
 - An identifying entry feature, such as a portico, porch, stoop and/or eave overhang or a similar entry structure shall be constructed that is designed to be integral to the structure.
 - Entry may be off an existing foyer for an ALQ that is added within an existing primary residence.
 - Walkways, entry porches, or stairways that are dark or hidden ~~are to shall be~~ avoided.
 - Where an exterior stairway to the entry is needed or a porch, portico, or eave overhang constructed, it ~~should shall be constructed of wood, or the most common material used in the construction consistent with the design of the primary residence.~~

Commented [PB42]: Informational Note:

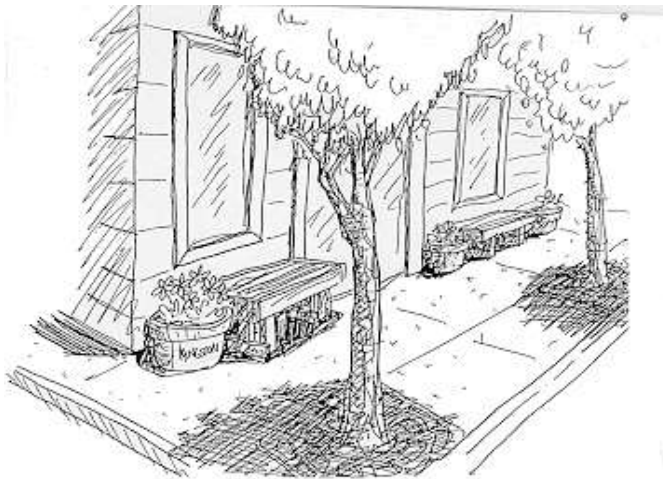
This new section integrates unique language from:

- KCC 17.480.300 Accessory dwelling units – Site and building design – Privacy
- KCC 17.480.310 Accessory dwelling units – Building design – Entry features
- KCC 17.480.320 Accessory dwelling units – Building design – Materials and colors

Architectural/Building Design

- 1 **3. Exterior Materials and Colors**
- 2 (a) The exterior materials (e.g. roofing, siding, trim, windows, etc) and colors of the
- 3 structure ADU/ALQ shall be consistent with ~~designed to maintain~~ the appearance
- 4 of the primary residence.
- 5

1 Chapter V
2 Landscape Design
3



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8 *Illustration courtesy of Bob Hines.*

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A. Landscape Plan Concept

Goal: The intent of these standards is to preserve and improve the aesthetic character of Kingston, to enhance the esthetic quality of new construction, to promote retention and protection of existing vegetation and to reduce the impact of development on drainage systems and natural habitats. Landscaping should be suitable and fitting with the character of Kingston as a community bordering marine and semi-rural areas. Existing substantive vegetation and native materials in informal plantings and arrangements should be considered in the concept.

Objectives:

- To provide visual screens and barriers as a transition between differing land uses.
- To define plant species which are of low maintenance, resistant to drought and otherwise appropriate for local conditions ~~within the business district~~ and which are safe for pedestrians.
- To provide visual relief from large expanses of parking areas and integrate new construction into the natural environment.
- To provide some physical separation between vehicular and pedestrian traffic.
- To retain existing vegetation and significant trees by incorporating them into the site

Landscape Design

1 design.

- 2 • To provide decorative landscape as a focal setting for signs, special site elements and/
3 or pedestrian areas.
- 4 • To provide increased areas of permeable surfaces to allow for infiltration of surface
5 water into groundwater resources, reduce the quantity of storm water discharge and
6 improve the quality of storm water discharge.

7 8 1. Landscape Plan Requirements

9 (a) Submit a landscape design plan and be prepared to demonstrate that the plan
10 addresses the following considerations:

- 11 • A unified pedestrian circulation system with amenities and plantings.
- 12 • A coordinated system of open spaces and/or planted areas that provide the
13 required pedestrian areas. The plan should indicate how the various spaces
14 and plantings relate to the project's site design objectives of continuity, variety,
15 activity, etc.
- 16 • Screening of service or unsightly areas.
- 17 • Plantings and/or site features that enhance the building's architectural qualities.
- 18 • Protection of existing significant trees and natural site amenities.

19 In addition, the design should consider the following landscape design objectives
20 where appropriate:

- 21 • Where feasible, coordinate the selection of plant material to provide a
22 succession of blooms, seasonal color, and a variety of textures.
- 23 • Provide a transition in landscaping design between adjacent sites, within a site,
24 and from native vegetation areas in order to achieve greater continuity.
- 25 • Design landscaping- to create definition between public and private spaces.
- 26 • Design landscaping to provide a transition between built structures (vertical
27 planes) and the site (horizontal planes).
- 28 • Use plantings to highlight significant site features and to define the function of
29 the site, including parking, circulation, entries, and open space.
- 30
- 31

Landscape Design

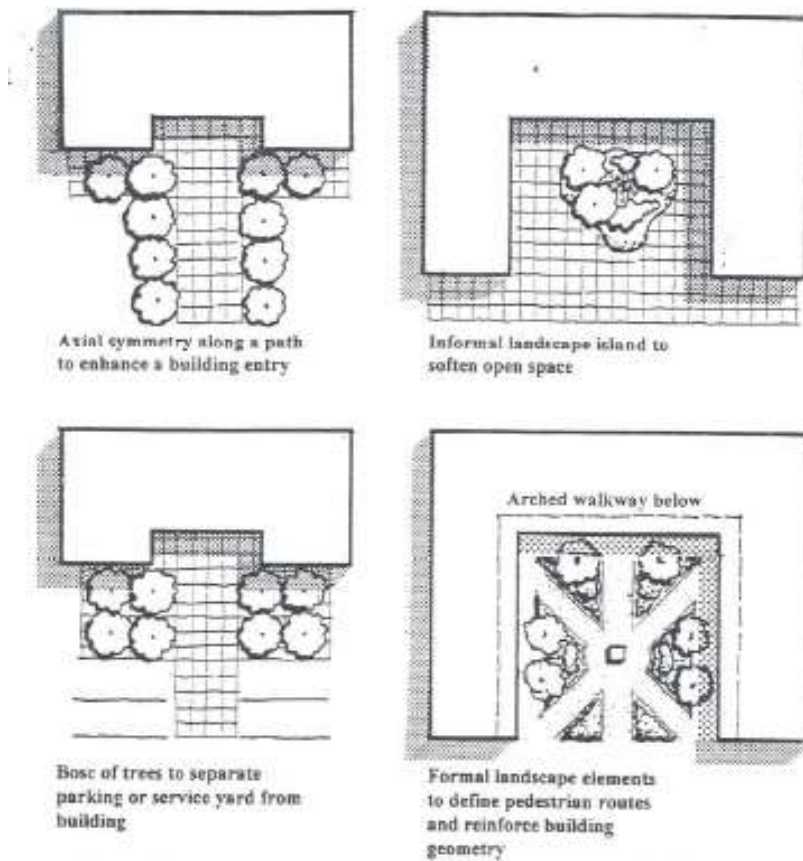


Figure V.A.1.(a). Examples of landscape designs associated with buildings.

B. Landscape Types

1. Planting Strips and Barriers

- (a) Use planting strips or areas as barriers and/or screens to separate land uses or specific activities and provide visual relief from large expanses of parking areas or buildings. Planting areas should be a mix of evergreen and deciduous shrubs whose height and width will be proportionate to the area being planted. Trees, shrubs, ground covers, and/or grasses that are native to the Puget Sound basin and are appropriate to the conditions of the site are preferred. Care must be taken not to visually block lines of sight for vehicles or pedestrians or obscure businesses with landscape material that will be too large for the site at maturity. Plants should be arranged asymmetrically unless the landscape plan emphasizes a formal design concept or space is too restricted to allow a mix of plant materials. Ground

Landscape Design

1 cover material ~~should~~ shall cover 90 percent of the soil in one growing season or
2 90 percent of the soil in three years if mulch is applied until the ground cover fills
3 the designated area.

4
5 Planting strips in parking areas and along roadways where traffic visibility is
6 required ~~will be~~ are limited to items on the approved plant list.
7

8 2. Decorative Landscaping and Special Areas

9 (a) Use decorative landscaping as a focal setting for signs, special site elements, and
10 pedestrian areas. Shrubs ~~should~~ shall cover at least 50 percent of the landscaped
11 area. At least 50 percent of the shrubs should exhibit decorative flowers or foliage.
12 They ~~should~~ shall be planted in a manner that will cover the ground in the area
13 allotted to them within 3 years. The remainder of the area ~~may~~ shall be planted
14 with trees, shrubs, ground cover, grasses, or cultivated flowerbeds.
15

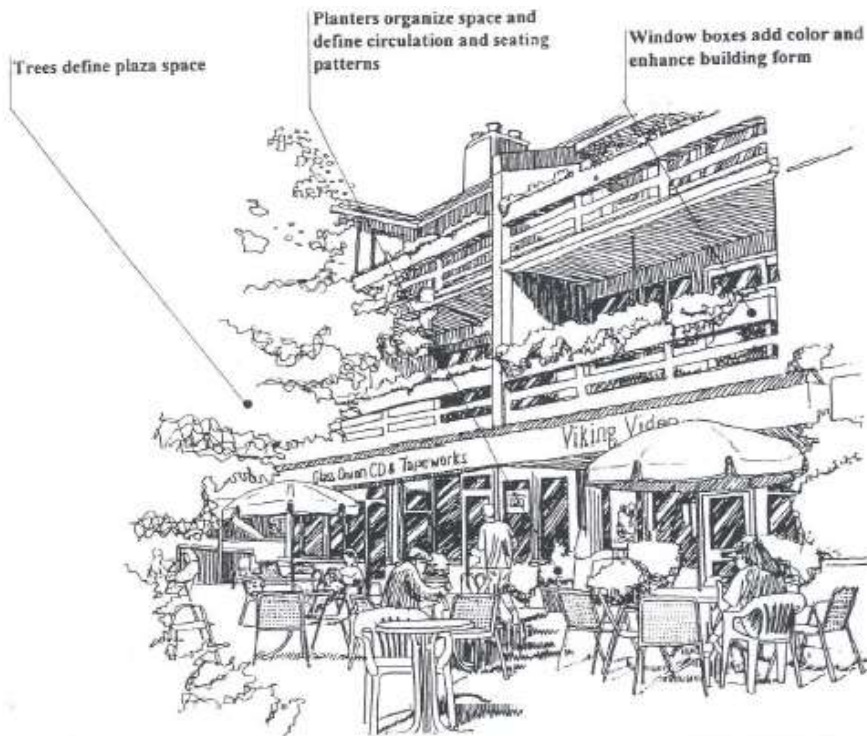
16 (b) Use plant materials as visual barriers or to add ambiance to special site elements
17 for pedestrians, such as seating areas. Additions such as sculpture, artwork,
18 kiosks, and site furniture are encouraged. Lighting for areas where pedestrians
19 might congregate at night ~~should~~ shall be provided at approximately 4 foot-
20 candles average for pedestrian-oriented spaces. Landscape materials should be in
21 scale with the area.
22

Commented [PB43]: Informational Note:
This change makes this section consistent with parking lot
landscaping requirements (Section V.B.4.b)



23
24
25 *Figure V.B.2.(a). Note how landscaping enhances this shop.*

Landscape Design



1
2 *Figure V.B.2.(b). Landscape enhancements serve several roles in this plaza.*
3
4

5 3. General

6 (a) Landscape unpaved areas in one of the following ways:

- 7 • Retention and/or enhancement of natural vegetation.
 - 8 • Cultivated trees, shrubs, and/or ground cover.
 - 9 • Other landscaping as approved by Kitsap County.
- 10

11 4. Parking Lot Landscaping

12 The following also applies to outdoor storage areas and outdoor sales areas visible from
13 a street right-of-way or adjacent property.

14
15 The intent of parking lot landscaping is to develop a positive image ~~for the business~~
16 ~~district~~ by providing an attractive appearance, reducing the summertime heat and glare
17 build-up within and adjacent to parking lots, improving the views of parking areas for
18 shoppers and area residents, lessening surface water run-off impacts, and providing

Landscape Design

1 landscaped areas within parking areas in addition to landscape buffers around the
2 perimeters of parking lots.

3
4 (a) Screen in one or more of the ways identified in Figure V.B.4(a) all parking lots
5 and outdoor storage and sales areas within 100 feet of a public right-of-way and
6 visible from the street.

7
8 An alternative to the required perimeter parking area landscaping ~~guidelines~~
9 ~~design standards~~ above may be submitted, provided that a better solution can be
10 found in terms of public benefit for one or more of the following items:

- 11 • Integrating interior surface parking area landscaping with required biofiltration
12 swales or surface water detention ponds.
- 13 • Incorporating or protecting natural features, including wetlands, significant
14 trees and vegetation, and slopes.
- 15 • Preserving distant views.
- 16 • Providing significant pedestrian-oriented spaces, such as a “pocket park” or
17 amphitheater, in excess of what is required under the Kitsap County Zoning
18 Ordinance.
- 19 • Creating an extension of or connection to a local park or a regional
20 bicycle/pedestrian trail system.

21
22 (b) Provide internal parking lot landscaping (but not for outdoor sales and storage
23 areas). Specifically, all surface parking lots (excluding underground or structured
24 parking ~~areas facilities~~) with more than 14 stalls and within 100 feet of the public
25 right-of- way (excluding alleys) and not separated from that street by another
26 building shall conform to the following standards:

27
28

<u>Total Number</u> <u>of Parking Spaces</u>	<u>Minimum Required</u> <u>Landscape Area</u>
15 to 50	15 square feet/parking space
51 to 99	25 square feet/parking space
100 or more	35 square feet/parking space

33

34 Provide shrubs and ground cover in the required landscape areas. Plant shrubs at a
35 density of five per 100 square feet of landscape area. Up to 50 percent of shrubs
36 may be deciduous. Select and plant ground cover so as to provide 90 percent
37 coverage within three years of planting, provided that mulch is applied until plant
38 coverage is complete.

39 Do not locate a parking stall more than 50 feet from a landscape area.
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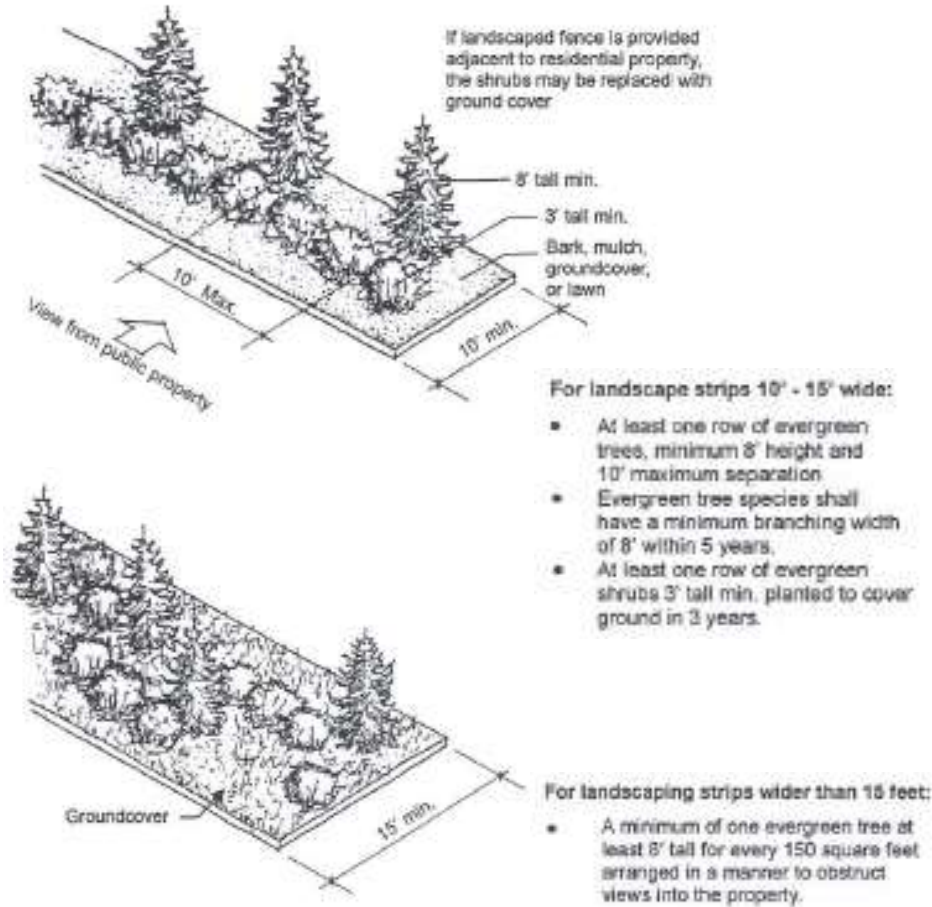
Landscape Design

- 1 Provide at least one tree for every six parking spaces. The trees required for
2 parking lot landscaping ~~may~~ shall not be considered ~~as~~ street trees. Exceptions:
- 3 • Kitsap County may exempt parking lots not visible from or not within 100 feet
4 of a street or park from interior parking lot landscaping, but not ~~necessarily~~
5 from perimeter parking lot landscaping.
 - 6 • The applicant may propose alternative parking lot landscaping schemes. See
7 Figure V.B.4.(a) for some other options.
8

Type A Landscaping

PURPOSE

To provide a dense site barrier separating land uses or screening specific activities or elements:



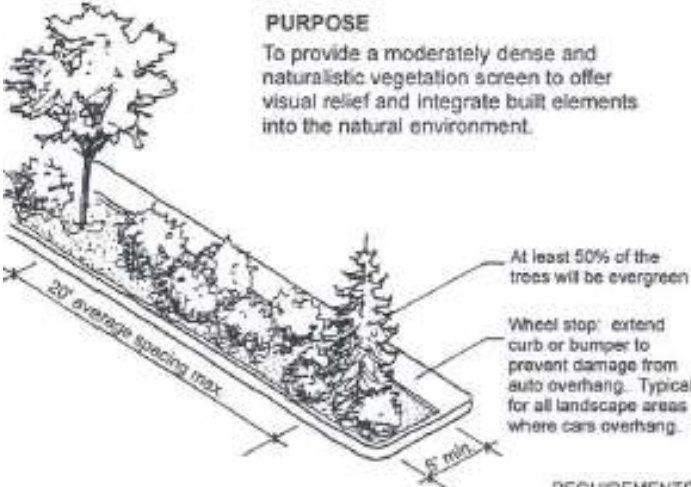
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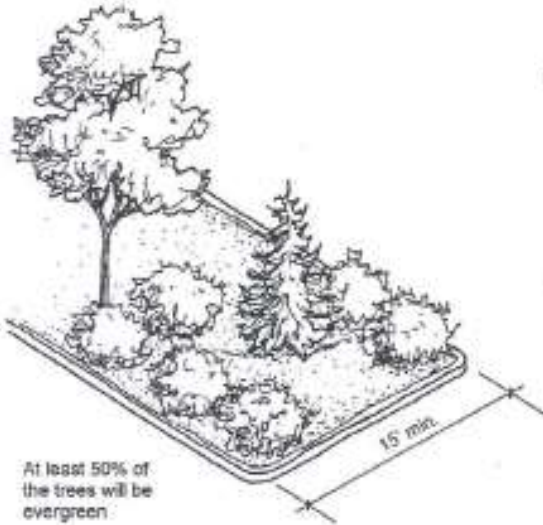
Figure V.B.4.(a). Types of acceptable parking lot screening.

Type B Landscaping

PURPOSE
To provide a moderately dense and naturalistic vegetation screen to offer visual relief and integrate built elements into the natural environment.



- REQUIREMENTS**
- For landscaped areas less than 15 feet wide:
- Informal groupings of evergreen (min. Ht. 8') and deciduous trees (min. caliper 2" as measured from 4' from root ball). Trees will be spaced at an average of 20' on center but may be grouped in asymmetrical arrangements.
 - Shrubs at least 3' tall shall be planted to cover the landscaped area in 3 years.



- Alternate for areas wider than 15 feet:
- At least one tree per 300 square feet of landscaped area
 - Shrubs covering at least 50% of landscaped area
 - Groundcover which will cover the remaining area within 3 years.

Figure V.B.4.(a) (continued)

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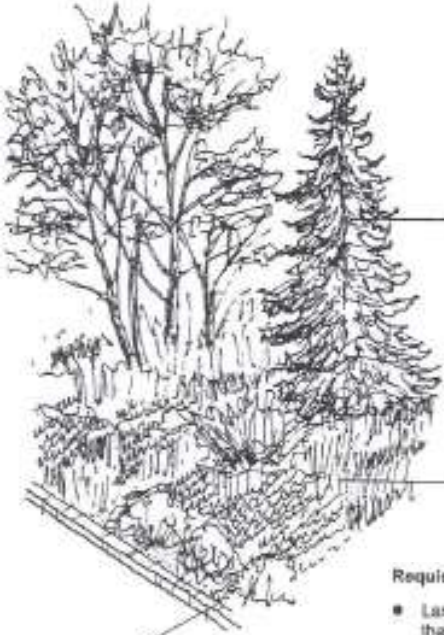
Landscape Design

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Type C Landscaping

PURPOSE

To enhance the natural areas and to integrate developments into the existing conditions.



If landscaping is used as part of a required landscape strip, the planting shall include at least one evergreen tree 8' tall planted with an average separation of 20' but not greater than 30' along the strip.

Mixture of native trees, shrubs and groundcover arranged in a naturalistic setting if used as part of the screen of required front yard treatment.

Requirements:

- Landscaping shall consist of trees, shrubs and groundcovers that are native to the Puget Sound basin and are appropriate to the conditions of the site. Species are subject to the approval by the City.
- Arrangements of plants shall be asymmetrical and plant material shall be sufficient in quantity to cover the soil in one growing season.

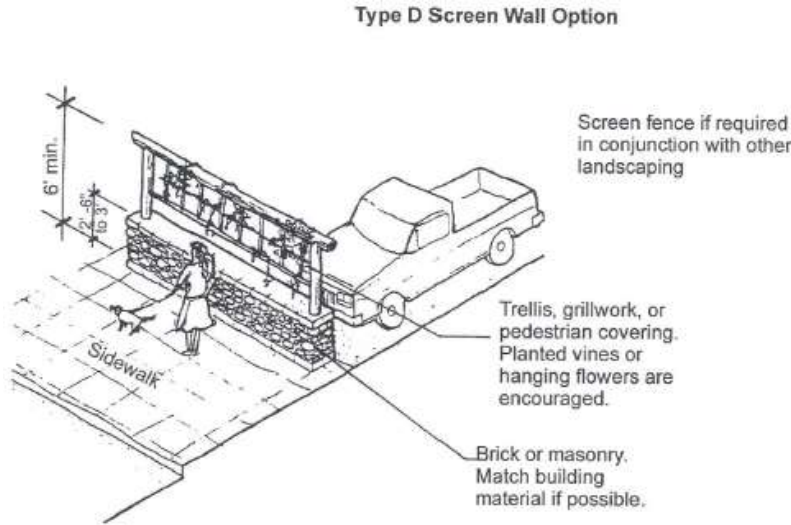
Min. 20' if used as part of screen or required front yard treatment

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Figure V.B.4.(a) (continued)

Landscape Design

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Figure V.B.4.(a) (continued)

(c) Provide landscaping to screen parking lots from adjacent or neighboring properties. Specifically, screen parking lots with over 14 stalls and within 10 feet of adjacent property lines by one of the methods described in Figure V.B.4.(a). Kitsap County staff may waive this requirement if parking is shared by the adjacent uses.

C. Retention of Significant Trees

Objectives:

- To retain substantial natural vegetation.
- To preserve and protect stands of mature trees.
- To aid in the stabilization of soil by preventing erosion.
- To reduce storm water runoff and costs associated with it.
- To provide an important visual buffer and screen against traffic and noise.
- To conserve and enhance the unique character of the area and protect and increase property values.

23

Landscape Design

1 **1. Significant Tree Protection**

- 2 (a) Consider alternative building and parking siting strategies to preserve existing
3 trees. Kitsap County staff will review site plans and may require modifications in
4 order to save stands of significant trees.
5
6 (b) Consider integrating pedestrian and bicycle paths with stands of mature trees,
7 where feasible, to provide a connection to adjacent uses.
8



9
10 *Figure V.C.1. Note how the evergreen trees form a characteristic backdrop.*
11
12
13

14 **D. Approved Plant List**

15 **Objectives:**

- 16 • To encourage the use of hardy, attractive, and easily maintained plant material.
17 • To encourage at least two seasons of interest for trees and shrubs.
18 • To provide visual continuity by using plant materials from a specified plant list of a
19 limited number of varieties and species, yet be open to new and interesting plant
20 materials.
21 • To encourage the use of trees and shrubs ~~within the business district~~ as an important
22 unifying element to strengthen the image and continuity of the streetscape.
23
24

Landscape Design

1 **1. Trees**

2 (a) When planting evergreen trees for required landscaping, incorporate plant
3 material from the approved list below or alternative plant species as approved by
4 Kitsap County.

<i>Arbutus unedo</i>	Strawberry Tree	Flowers and fruit
<i>Calocedrus decurrens</i>	Incense Cedar	Horizontal branching habit
<i>Cupressus glabra</i>	Smooth Arizona Cypress	Reddish bark
<i>Garrya elliptica</i>	Coast Silk Tassel	Beautiful catkins
<i>Magnolia grandiflora</i>	Evergreen Magnolia	Fragrant white flowers
<i>Pinus cembra</i>	Swiss Stone Pine	Dense foliage
<i>Pinus contorta contorta</i>	Shore Pine	Native tree
<i>Pinus thunbergii</i>	Japanese Black Pine	Irregular branching
<i>Quercus ilex</i>	Holly Oak	Evergreen, wind tolerant

6 (b) When planting deciduous trees for required landscaping, incorporate plant
7 material from the approved list below or alternative plant species as approved by
8 Kitsap County.

<i>Acer campestre</i>	Hedge Maple	Yellow fall color
<i>Acer circinatum</i>	Vine Maple	Native
<i>Acer ginnala</i>	Amur Maple	Red fall color
<i>Carpinus betulus</i>	Narrow European Hornbeam	Yellow fall color
<i>Cercidiphyllum japonicum</i>	Katsura	Yellow fall color
<i>Cornus mas</i>	Cornelian Cherry	Flowers in winter
<i>Fagus sylvatica</i>	Beech	Smooth gray bark
<i>Fraxinus angustifolia</i>	Raywood Ash	Seedless, great fall color
<i>Malus sp.</i>	Flowering Crab Apples	Spring flowers
<i>Nyssa sylvatica</i>	Sour Gum	Early fall color, very red
<i>Oxydendron arboreum</i>	Sour Wood	Scarlet fall color, white flowers
<i>Parrotia persica</i>	Persian Ironwood	Good fall color
<i>Pyrus calleryana 'Capitol'</i>	Capitol Pear	Good for narrow spots
<i>Quercus robur 'Fastigiata'</i>	Upright English Oak	Good for narrow spots
<i>Stewartia sp.</i>	Stewartia	Flowers, good fall color, good bark
<i>Tilia cordata</i>	Littleleaf Linden	Fragrant flowers in summer

Landscape Design

2. and Ground Covers

(a) When planting evergreen shrubs for required landscaping, incorporate plant material from the approved list below or alternative plant species as approved by Kitsap County.

<i>Arctostaphylos x media</i>	Hybrid Kinnikanik	Hybrid native
<i>Cistus sp.</i>	Rockroses	Many varieties
<i>Lavandula sp.</i>	Lavender	Many varieties
<i>Mahonia aquafolium</i>	Oregon Grape	Native
<i>Mahonia nervosa</i>	Low Oregon Grape	Native
<i>Myrica californica</i>	Pacific Wax Myrtle	Salt tolerant, good hedge
<i>Rosmarinus</i>	Rosemary	Many varieties
<i>Vaccinium ovatum</i>	Evergreen Huckleberry	Native
<i>Viburnum tinus</i>	Laurustinus	Winter flowering

(b) When planting deciduous shrubs for required landscaping, incorporate plant material from the approved list below or alternative plant species as approved by Kitsap County.

<i>Amelanchier alnifolia</i>	Service Berry	Native	White flowers and good fall color
<i>Buddleia davidii</i>	Butterfly Bush		Purple flowers in summer
<i>Oemleria cerasiformis</i>	Oso Berry	Native	Early white spring flowers
<i>Rhamnus purshiana</i>	Cascara	Native	Good fall color
<i>Acer circinatum</i>	Vine Maple	Native	Good fall color
<i>Salix sp.</i>	Willows		Good for wet areas
<i>Ribes sanguineum</i>	Red Flowering Currant	Native	Pink flowers in spring
<i>Cornus stolonifera</i>	Yellow Twig Dogwood	Native	Yellow stems in winter, wet areas
<i>Cornus stolonifera</i>	Red Osier Dogwood	Native	Red stems in winter, wet areas
<i>Philadelphus lewisii</i>	Mock Orange	Native	Fragrant white flowers in late June
<i>Spiraea douglasii</i>	Hardhack Spirea	Native	Pink flowers in summer
<i>Vaccinium parvafolium</i>	Red Huckleberry	Native	Edible berries, good fall color

(c) When planting ground covers for required landscaping, incorporate plant material from the approved list below or alternative plant species as approved by Kitsap County.

<i>Arctospholos uvi-ursi</i>	Kinnikanik	Native	Evergreen, red berries in winter
<i>Gaultheria shallon</i>	Salal	Native	Evergreen, can get shrubby
<i>Gaultheria procumbens</i>	Checkerberry/Wintergreen		Evergreen, stays low, holds berries
<i>Polysticum munitum</i>	Sword Fern	Native	Evergreen, low maintenance
<i>Cotoneaster sp.</i>	Cotoneaster		Many varieties
<i>Erica sp.</i>	Heather		Many varieties
<i>Calluna sp.</i>	Heath		Many varieties
<i>Epemedium sp.</i>	Epemedium		Many varieties

(d) When planting grasses for required landscaping, incorporate plant material from the approved list below or alternative plant species as approved by Kitsap County.

<i>Fescue sp.</i>	Many varieties
<i>Carex sp.</i>	Many varieties

Chapter VI

Signage

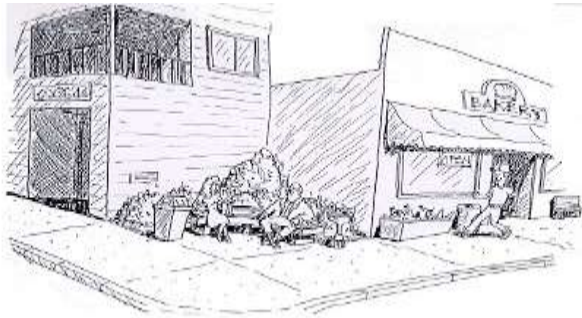


Illustration courtesy of Bob Hines.

~~**Definition:** The display of information by portable or permanent signs of all types, such as reflective, illuminated, neon, or fluorescent signs; overhead banners; posters; and painted or engraved signs.~~

Commented [PB44]: Informational Note:
Removed to avoid conflicts with the definitions in the Kitsap County Sign Code (KCC 17.510)

~~Signage shall be provided as required by KCC 17.510 (Sign Code) and the following design standards. Where there is a dimensional or design conflict, the following design standards shall apply.~~

Goal: To provide a means of disseminating useful information to residents and visitors in a manner that is user-friendly, attractive, and compatible with the appearance of downtown Kingston.

~~**Objective:** All signs shall meet the requirements of the Kitsap County Zoning Ordinance section 445. They shall be divided into three categories: informational signs; commercial and business signs; and personal, political, and event signs.~~

A. Informational Signs

Informational signs are recommended as a service to visitors and residents. They are proposed to be located at strategic entrance points and other appropriate locations in Kingston as required, including:

- Highway 104 at the ferry landing.
- Lindvog Road at Highway 104.
- West Kingston Road at the Community Center.

Signage

1 These signs shall include a map locating parks, beach access, parking, schools,
2 Community Center, fire station, sheriff's office, and post office.

3
4
5 **B. Commercial and Business Signs**

6 **1. Commercial and Business**

7 (a) Keep signs as simple as possible, relying on symbols to avoid needless clutter and
8 complexity.

9
10 ~~(b) Use landscaping in conjunction with pole signs for safety as well as appearance.~~



12
13
14 *Figure VI.B.1.(a). The United States Post Office provides an excellent example of appropriate*
15 *signage. The message is clear and the lettering simple. The materials are durable and of high*
16 *quality. The logo is appropriate and well crafted. The sign and the building's architecture are*
17 *well integrated. Note how the recess in the gable provides an attractive frame around the sign*
18 *and adds scale and interest to the building façade in a straightforward manner.*



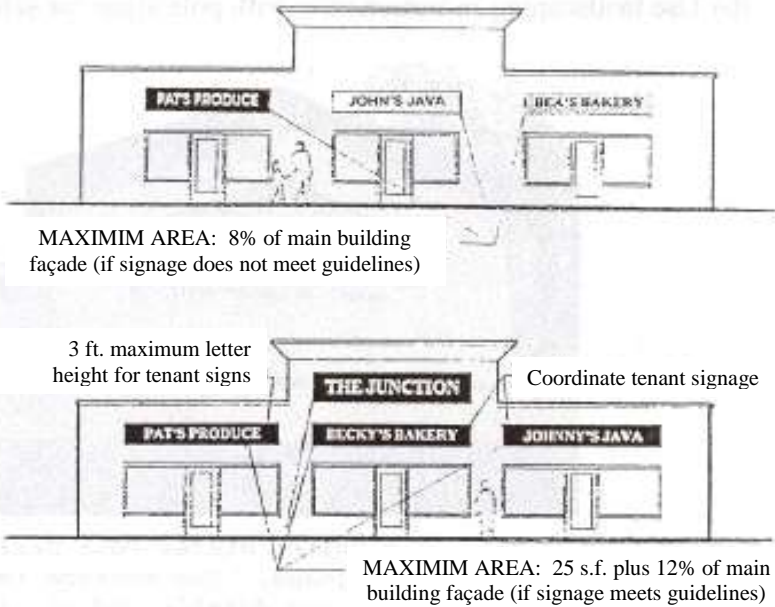
20
21
22 ~~*Figure VI.B.1.(b). This pole sign is attractively designed, scaled, and constructed. Note how the*~~
23 ~~*tree frames the view of the sign.*~~

Commented [PB45]: Informational Note:
Removed for consistency with KCC 17.510 (sign code) since pole signs are discourage and only allowed where there are no feasible alternatives. Landscaping is required by KCC 17.510.

Commented [PB46]: Informational Note:
Removed for consistency with KCC 17.510 (sign code) since pole signs are discourage and only allowed where there are no feasible alternatives.

Signage

- 1
- 2 (c) Where multiple businesses operate from a central location, group signage to avoid
- 3 the proliferation of signs. This is considered to be in the public interest to reduce
- 4 cost of merchandising and to avoid clutter and confusion.
- 5
- 6 (d) Do not install neon lighting on the exterior of any building unless it is shielded
- 7 from direct view. Neon advertising signs shall be confined to the interior of the
- 8 buildings they serve.
- 9
- 10 (e) Make signs small and low-level, oriented to pedestrians. (See recommended
- 11 heights and types ~~on Figure VI.B.1(d)~~ below)
- 12

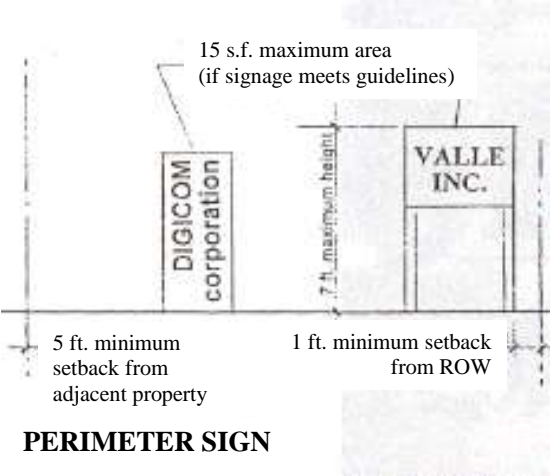


BUILDING SIGN

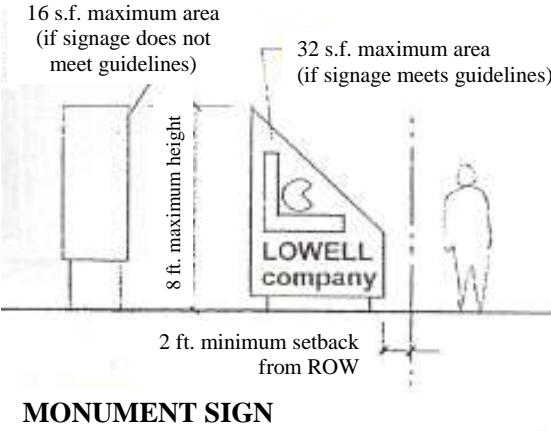
Figure VI.B.1.(d e)-1. Recommended sign types and sizes.

Signage

1



2
3
4
5



6
7

Figure VI.B.1.(e)-1. Continued.

Signage

1



Figure VI.B.1.(f)(e)-2. Pedestrian-oriented "blade" signs, especially attractively designed ones such as this example, are appropriate.

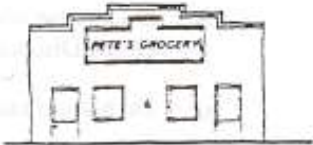
2
3
4
5
6

Don't



Dwarf the rest of building elements

Do



Keep with a reasonable scale relative to other elements



"Apply" sign indiscriminately to a building



Incorporate sign as part of building element such as awning or canopy

7
8
9
10

Figure VI.B.1.(e)(f). Some inappropriate and appropriate sign configurations.

Signage

- (f) Mount building-mounted signs perpendicular to or flat against the building, integrating the sign and building design. ~~(See Figure VI.B.1.(e).)~~
- (g) Limit “A”-board or freestanding open signs to one aesthetically pleasing sign per business entrance. Locate such signs so as not to obstruct pedestrian circulation on public sidewalks.
- (h) Do not use animated or audible signs or signs illuminated by any intermittent, flashing, or scintillating light. This shall not apply to time and temperature displays.
- (i) Do not use internally illuminated signs (can) in the Old Town/Waterfront or Village Green Design Districts.
- (j) Do not use banners, flags, etc.
- (k) Background colors (i.e., colors that are not part of lettering, graphics, or decoration) ~~shall be complementary to the colors of the building from the historic color palette of any major manufacturer approved by the County.~~
- (l) Mounted or freestanding reader-board signs are not acceptable.

~~C. Personal, Political, and Event Signs~~

~~Signs in this category include the following:~~

- ~~• Personal Signs
 - ~~• Garage sale signs.~~
 - ~~• Yard sale signs.~~
 - ~~• Moving sale signs.~~
 - ~~• For rent signs.~~
 - ~~• For sale by owner signs.~~
 - ~~• Lost pet signs.~~~~
- ~~• Political Signs
 - ~~• Individual candidate campaign signs.~~
 - ~~• Regional or state issues campaign signs.~~
 - ~~• Local issues campaign signs.~~~~
- ~~• Event Signs
 - ~~• Parade signs and instructions.~~
 - ~~• Farmers market signs.~~
 - ~~• Church meeting or social signs.~~
 - ~~• Concert or entertainment signs.~~~~

~~Traffic signs regulating the flow of vehicular traffic throughout the Kingston area are exempt from these regulations.~~

Commented [PB47]: Informational Note:
This section is being removed because it is redundant and partially in conflict with the Kitsap County Sign Code (KCC 17.510).

Signage

- 1 ~~1. Personal, Political, and Event Signs~~
- 2 ~~(a) Remove all personal, political, and event signs within five days after the event.~~
- 3
- 4 ~~(b) Do not display vinyl banners unless associated with a specific special event and/or~~
- 5 ~~date.~~
- 6
- 7 ~~(c) Do not display any signs in this category for more than 30 days.~~

Chapter VII

Lighting

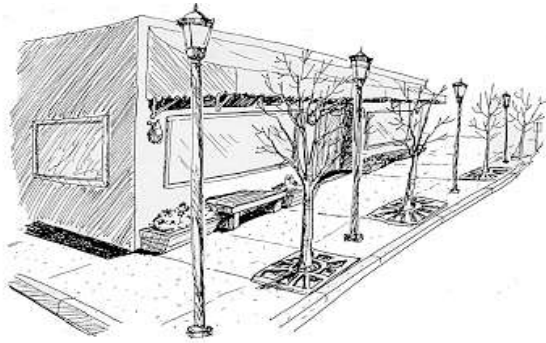


Illustration courtesy of Bob Hines.

~~**Definition:** This standard defines the recommended ambient illumination required during periods of darkness throughout the entire downtown area, including all streets, parking lots, building entries, and pedestrian walkways intended for public use.~~

Commented [PB48]: Informational Note:
This is not a definition. Applicability is specified, as needed, in the standards below.

Goals:

- To provide a suitable level of illumination for pedestrian walkways and building entries at night throughout the downtown area.
- To minimize the possibility of illegal activities by providing suitable illumination at night.
- To improve the confidence and safety of pedestrians walking throughout the downtown area at night.

Objectives and General Specifications: Existing streetlights generally provide sufficient illumination in the immediate area of their location and are intended to illuminate cross streets and intersections. They are suitable for vehicular traffic but are inadequate for pedestrian safety. Additional illumination ~~must~~ shall be provided along pedestrian walkways and vehicular parking lots.

For the illumination levels described below, conformance shall be determined with the measuring instrument located approximately 5 feet above the walking surface. Measurements shall be taken during the darkest portion of the night. Existing lighting fixtures, whether illuminated advertising signs, building-mounted lights, or streetlights, may contribute to the measured levels stipulated.

All electrical lighting fixtures shall be installed in compliance with the current edition of the National Electrical Code.

Lighting

1 **Technical Definitions:**

- 2 • ~~Foot candle: A unit equivalent to the illumination produced by a source of one candle at a~~
3 ~~distance of one foot and equal to one lumen incident per square foot.~~
- 4 • ~~Foot lambert: A unit of luminance equal to the luminance of a surface emitting a luminous~~
5 ~~flux of one lumen per square foot.~~
- 6 • ~~Lux: A unit of illumination equivalent to 0.0929 foot candle and equal to the illumination~~
7 ~~produced by luminous flux of one lumen falling perpendicularly on a surface one meter square.~~
- 8 • ~~Lumen: The unit of luminous flux equal to the luminous flux emitted in a unit solid angle by a~~
9 ~~point source of one candle intensity.~~

Commented [PB49]: Informational Note:
This word is not used anywhere else in this document and therefore does not need to be defined.

Commented [PB50]: Informational Note:
This word is not used anywhere else in this document and therefore does not need to be defined.

11 **A. Lighting**

12 **1. Lighting Standards**

- 13 (a) Provide a minimum illumination level of 2 foot-candles for all public pedestrian
14 walkways. Building entries require 4 foot-candles or greater.
- 15
- 16 (b) Provide a minimum illumination of 1 foot-candle throughout public and private
17 parking areas. Parking lot light fixtures shall be of attractive and harmonious
18 design for the area they serve and shall provide maximum illumination down
19 while minimizing illumination up. They shall be high-pressure sodium or as
20 approved by the County.
- 21
- 22 (c) Direct lighting downward to the intended area to be lighted. All exterior lighting
23 fixtures ~~should shall~~ incorporate cutoff shields to prevent spillover into residential
24 areas. ~~Broadcast lighting fixtures that project lighting outward rather than~~
25 ~~downward are discouraged.~~ Outdoor lighting ~~should shall~~ be arranged so that the
26 light ~~is directed away from~~ ~~does not negatively affect~~ adjoining properties.
27 Temporary high intensity construction lights should be oriented so that the bulb
28 itself is not visible from the nearest property line.
- 29
- 30 (d) Limit freestanding light fixtures to 14 feet in height.
- 31
- 32 (e) Vehicular roadway and highway lighting shall be subject to the County
33 requirements.
- 34
- 35 (f) Where auxiliary lighting fixtures are required to meet the illumination levels
36 described above, install fixtures that conform to the existing style as used in Old
37 Town/Waterfront Design District: HOLOPHANE Model UPT350 (or an
38 acceptable equivalent) providing a nominal 9500 Lumens with a 100 watt bulb
39 installed.
- 40

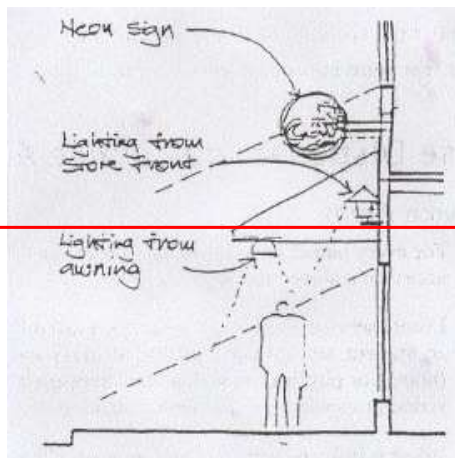
Commented [PB51]: Informational Note:
This sentence conflicts with the sentences before and after.

Lighting



Figure VII.A.1.(f). Kingston Old Town street lights.

- (g) Do not use mercury vapor light fixtures.
- (h) When using decorative miniature lighting strings, do not use bulbs larger than 11 watts each.
- (i) Do not use back-lit awnings.
- (j) Consider light from buildings as an attractive component of streetscape lighting.



~~Figure VII.A.1.(j). A combination of light sources can be desirable.~~

Commented [PB52]: Informational Note:
Removing because neon signs, as depicted, are not allowed per Chapter VI, Section B.1.(d).

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1 Chapter VIII
2 Utilities/Refuse/Service Areas
3

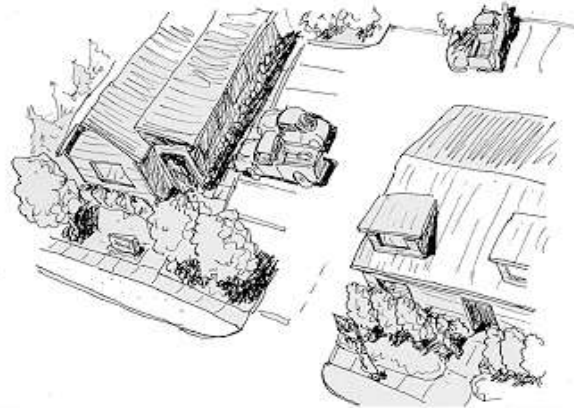


Illustration courtesy of Bob Hines.

4
5
6
7 **Goals:**

- 8 • To reduce the visibility of unsightly uses (containers, for example) ~~in the business~~
9 ~~districts.~~
- 10 • To minimize the visibility of "backs" of buildings, where service areas are located.
- 11 • To reduce the noise impacts of service areas.
12

13 **A. Refuse Disposal and Service Areas**

14 **1. Service Areas**

- 15 (a) ~~For every parcel with a structure, provide a trash receptacle on the premises or~~
16 ~~access to a shared facility. Refuse areas must meet Kitsap County Code Titles 12~~
17 ~~Stormwater Drainage and 17 Zoning. Design consideration shall be given to the~~
18 ~~proposed use; however, the landowner is encouraged to consider future tenant~~
19 ~~uses of the site as well (ie, a use with 'wet waste' refuse—such as what food~~
20 ~~services generate—shall be required to connect to mainline sewer or provide a~~
21 ~~catch basin with a maintenance schedule).~~
- 22
- 23 (b) Locate service areas (loading docks, trash dumpsters, compactors, mechanical
24 equipment, and storage yards) so as to not have a negative visual, auditory (noise),
25 or physical impact on the street environment and adjacent residentially zoned
26 properties. Locate service areas at the rear of the lot, if possible, while considering
27 feasible access to the service area by the service provider for ease of refuse

Commented [PB53]: Information Note:
Section removed because trash receptacles are regulated by
the Kitsap Public Health District.

Utilities/Refuse/Service Areas

1 dumping.

2
3 (c) When refuse and service areas are visible from the sidewalk or adjacent
4 properties, screen the elements. The screen ~~should~~ shall incorporate similar
5 materials and architectural forms as the site's principal building.

6
7 ~~(d) Provide trash receptacles that comply with adopted Public Works standards and~~
8 ~~are of sufficient size to accommodate the trash generated. All receptacles shall be~~
9 ~~screened on three sides with a fence and/or landscaping.~~

Commented [PB54]: Information Note:
Section removed because trash receptacles are regulated by
the Kitsap Public Health District.

10
11 (e) Ensure architectural consistency between the design of any structure enclosing an
12 outdoor collection point or any building primarily used to contain a collection
13 point and the design of the primary structure(s) on the site.

14
15 ~~(f) Install a 6-foot wall or fence to enclose any outdoor refuse collection point or~~
16 ~~dumpster service area, excluding collection points located in industrial~~
17 ~~developments that are greater than 100 feet from residentially zoned property. The~~
18 ~~fence shall be a solid material, such as wood or masonry, rather than chain link~~
19 ~~fence.~~

Commented [PB55]: Informational Note:
Section removed because it is redundant (and partially in
conflict) with Sections (c) and (e) above.

20
21 **B. Utilities**

22 **1. Poles and Overhead Wires**

23 (a) Eliminate overhead wires and poles where possible.

24
25 (b) When planned projects such as street redesign, placement of water, sewer lines or
26 other major construction that would require the removal of overhead wires and
27 poles, coordinating meetings shall be held by the project designer/engineer with
28 the utility companies (e.g. power co., telephone co., etc.) to discuss and plan the
29 feasibility of burying utility facilities to eliminate overhead wires and/or poles.

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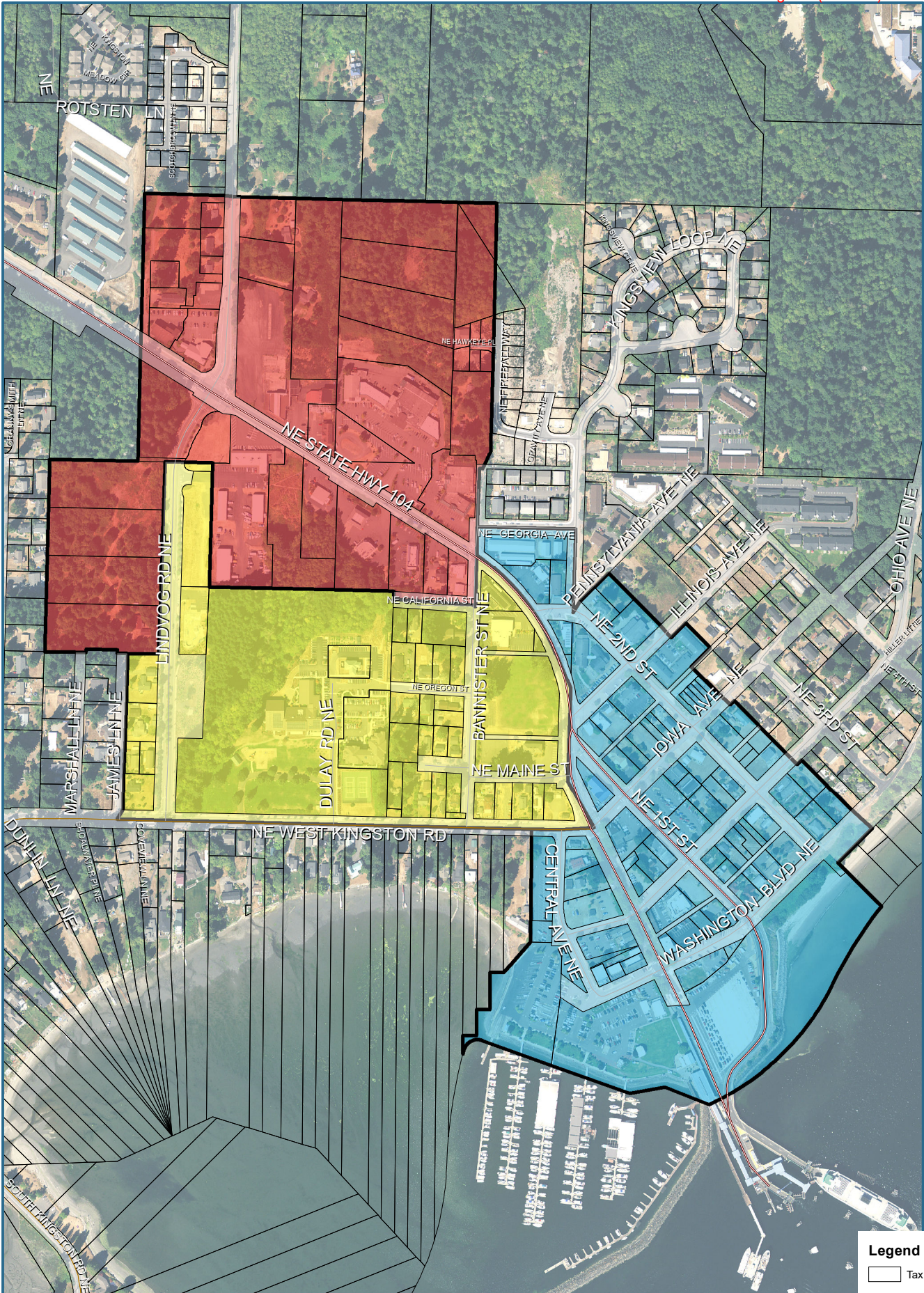
Design Standards for the Community of Kingston

Appendices



Appendix A

Kingston Design Districts



Legend

Tax Parcels (Full Ownership)

DISTRICT NAME

- Lindvog Commercial
- Old Town
- Village Green



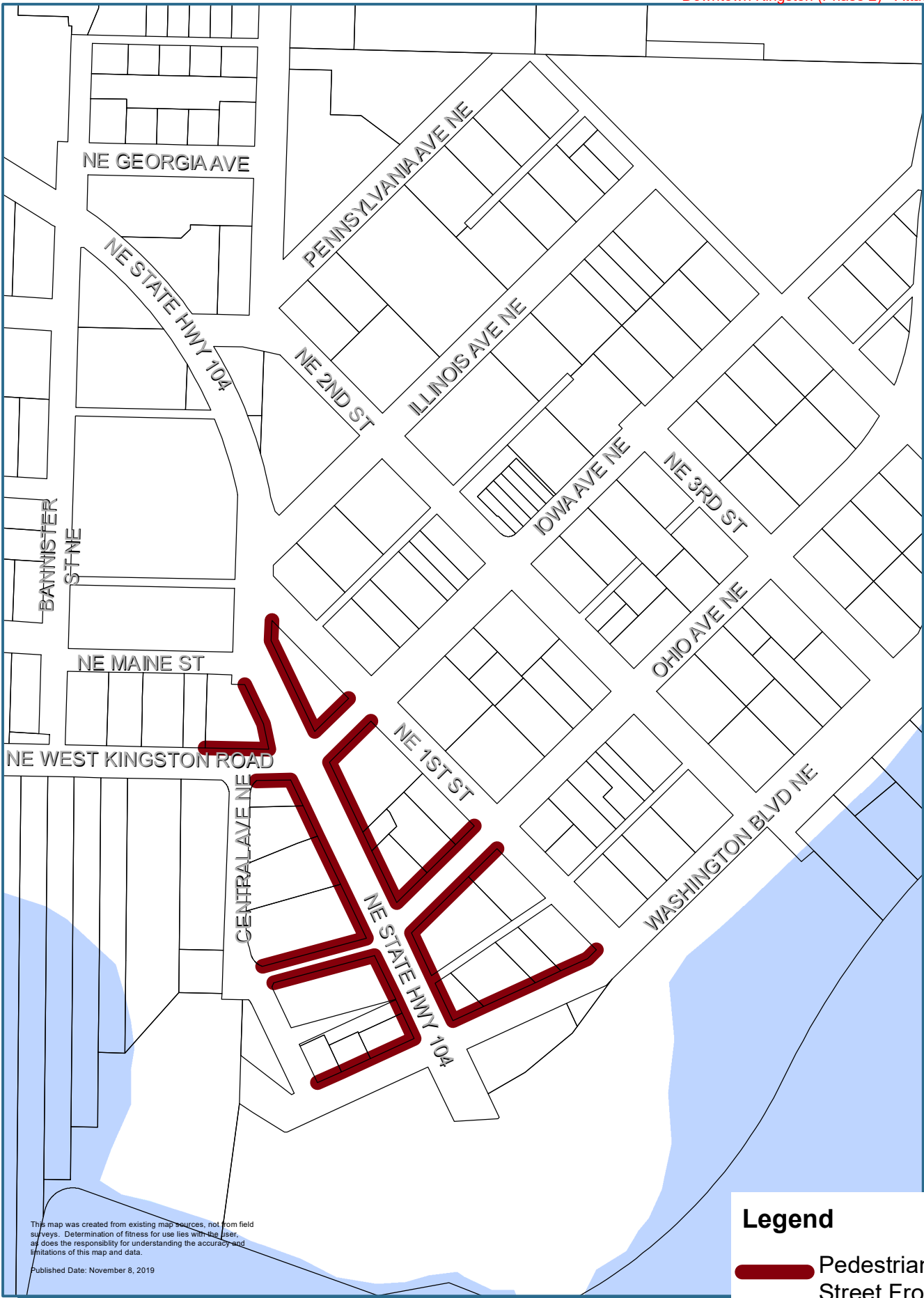
Kitsap County Department of Community Development
 614 Division Street, MS-36, Port Orchard, Washington 98366
 VOICE (360) 337-5777 * FAX (360) 337-4925

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 Ord No. 534-2016 Comprehensive Plan 10-year update
 Map Published Date July 18, 2019





Appendix B

Downtown Kingston Pedestrian-Oriented Commercial Street Frontage



Legend

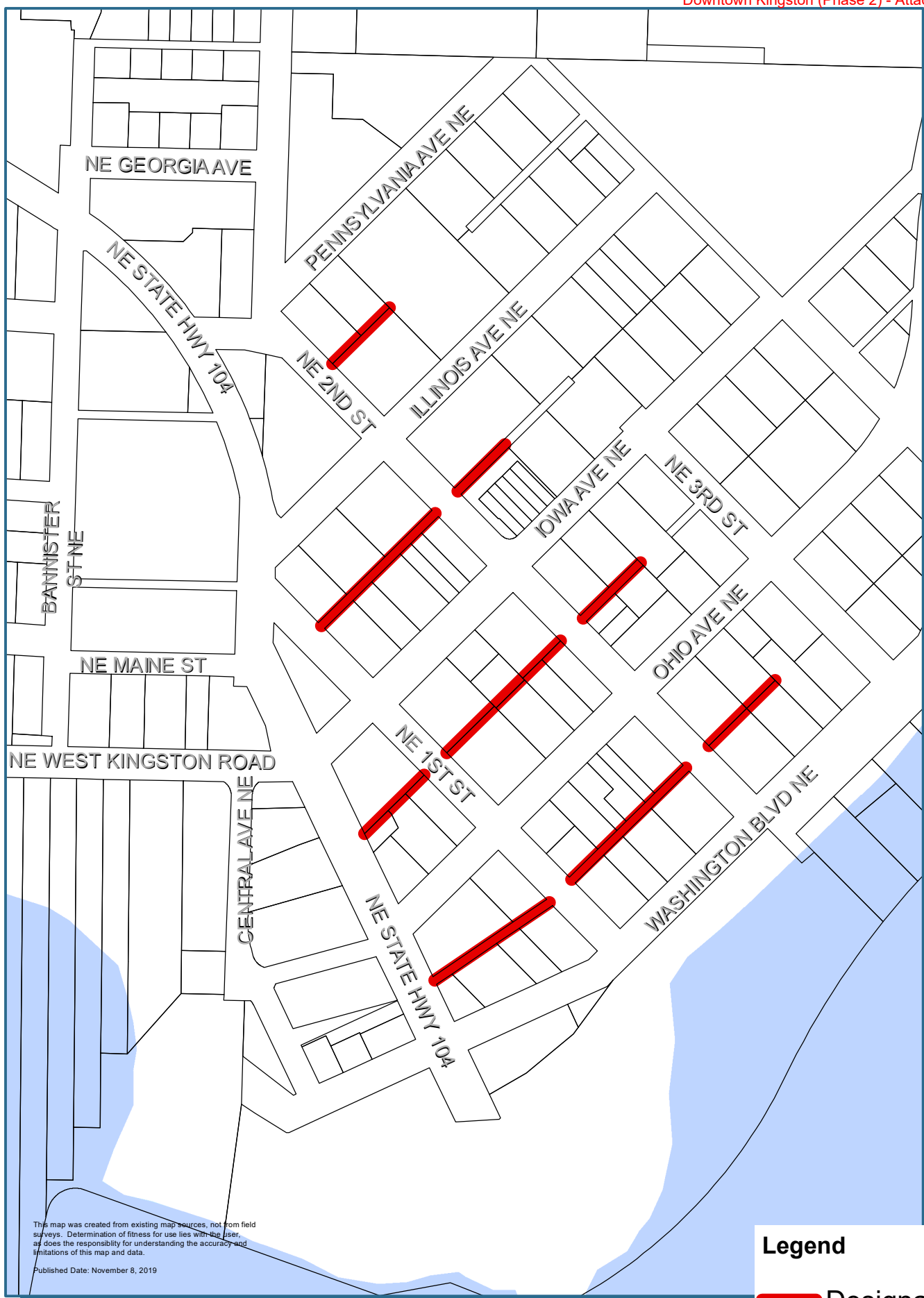
-  Pedestrian-Oriented Street Frontage
-  Tax Parcels

This map was created from existing map sources, not from field surveys. Determination of fitness for use lies with the user, and does the responsibility for understanding the accuracy and limitations of this map and data.
Published Date: November 8, 2019







Downtown Kingston Alley Plan



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 Published Date: November 8, 2019

Legend

-  Designated Alley
-  Tax Parcels









Sherwin-Williams proudly presents Heritage Colors™—40 historic Nineteenth Century hues that capture the grace and elegance of another era.

Heritage Colors have been authenticated by Dr. Roger Moss, and documented in his book, *Century of Color: Exterior Decoration for American Buildings, 1820-1920*.

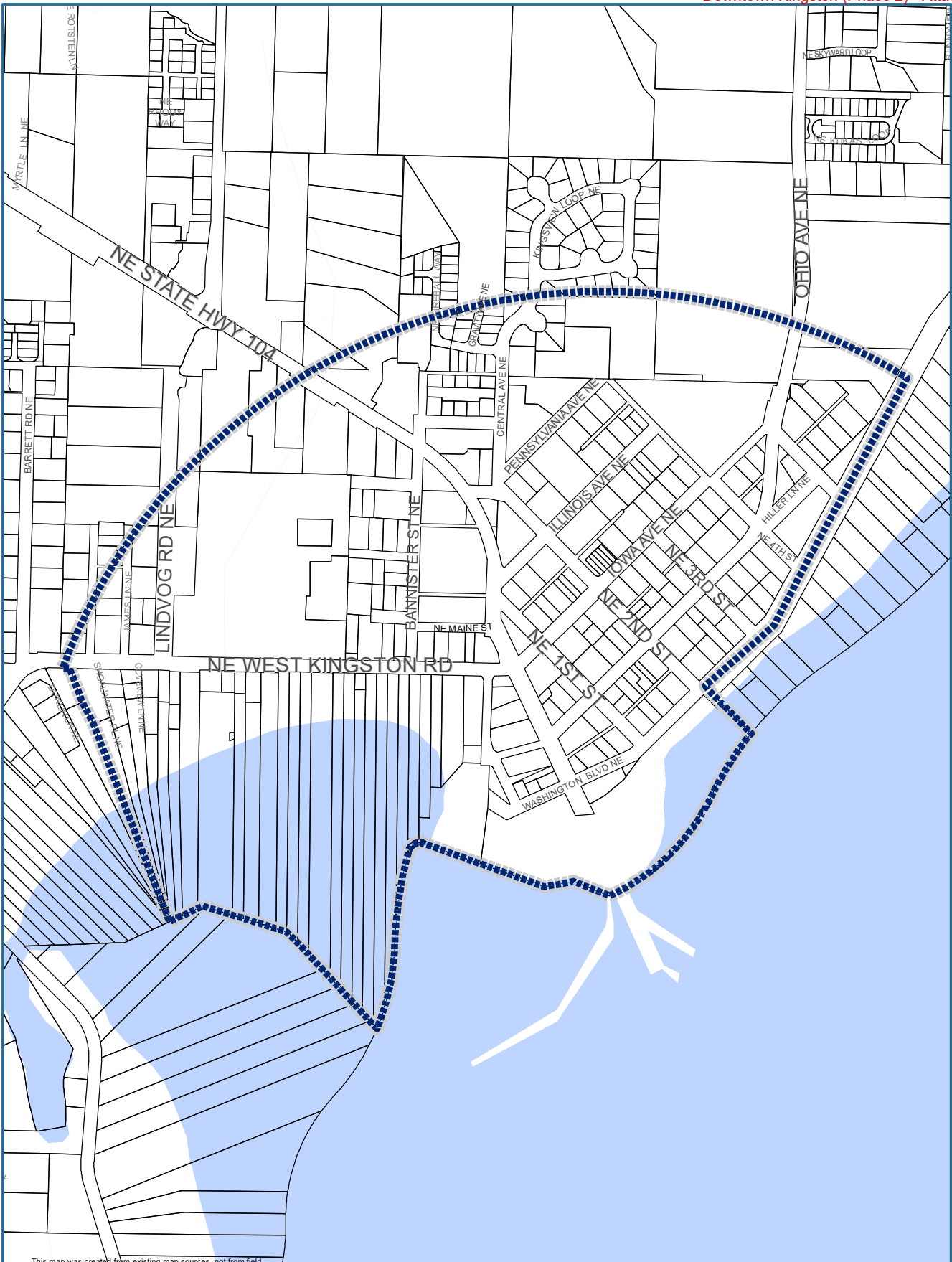
Restore classic beauty to your Traditional or Victorian styled home with a selection from this distinguished collection — the best of America's past.

Heritage Colors are available in
 • SUPERPAINT™ Exterior Latex House & Trim Paint...The best paint ever made by Sherwin-Williams
 • SWP® Gloss House & Trim Paint (Oil Base)

HERITAGE COLORS™ 1820-1920

								
	Rookwood Amber (Trim) 94184	Colonial Revival Ivory 94432	Downing Cream 94028	Colonial Revival Yellow 94440	Rookwood Olive 94150	Rookwood Jade 94283	Rookwood Dark Red (Trim) 94275	
								
	Rennick Beige 94101	Rennick Yellow 94176	Downing Straw 94077	Classical White 94408	Tiffany Bronze 94366	Rennick Fence Green 94119	Rookwood Dark Red (Trim) 94275	
								
	Rookwood Dark Brown (Trim) 94242	Downing Sand 94044	Rookwood Antique Gold 94192	Downing Yellow 94085	Tiffany Olive 94382	Rookwood Blue Green 94218	Rookwood Dark Red (Trim) 94275	
								
	Rennick Rose Beige 94168	Rennick Gold 94127	Downing Stone 94069	Tiffany Moss Green 94374	Tiffany Palm Green 94390	Rookwood Sash Green 94333	Rookwood Dark Red (Trim) 94275	
								
	Rookwood Medium Brown (Trim) 94291	Rookwood Terra Cotta (Trim) 94358	Rookwood Olive 94317	Rookwood Dark Olive 94267	Rookwood Dark Green (Trim) 94259	Rookwood Shutter Green 94341	Rookwood Dark Red (Trim) 94275	
								
	Rennick Golden Oak 94135	Downing Earth 94036	Rookwood Olive 94317	Rookwood Olive 94317	Rookwood Dark Olive 94267	Rookwood Dark Green (Trim) 94259	Rookwood Dark Red (Trim) 94275	

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Downtown Kingston High Capacity Transit Station Area

This map was created from existing map sources, not from field surveys. Determination of fitness for use lies with the user, as does the responsibility for understanding the accuracy and limitations of this map and data.

Published Date: November 8, 2019

Legend



High Capacity Transit Station Area



Tax Parcels



Kitsap County Department of Community Development
614 Division Street, MS-36, Port Orchard, Washington 98366
VOICE (360) 337-5777 * FAX (360) 337-4925

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Attachment B

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KITSAP COUNTY DEPARTMENT OF COMMUNITY DEVELOPMENT

614 DIVISION STREET MS-36, PORT ORCHARD WASHINGTON 98366-4682 JEFF RIMACK, DIRECTOR
(360) 337-5777 HOME PAGE - www.kitsapgov.com/dcd/

DETERMINATION OF NONSIGNIFICANCE

Description of Proposal: 2019 Kitsap County Comprehensive Plan Amendment – Downtown Kingston Phase 2 (a non-project action).

This amendment expands on and implements Phase 1 amendments adopted in 2018, is intended to remove regulatory barriers to achieving the existing adopted vision for downtown Kingston, and includes the following substantive amendments:

Kitsap County Code

- Repeal redundant design standards and relocated others.
- Define a high capacity transit station area around the Kingston ferry.
- Create new parking standards for high capacity transit station areas.
- Update allowed uses for the Kingston UVC zone.

Design Standards for the Community of Kinston

- Improve administration and consistency with other codes
- Update the applicability of the design standards
- Incorporate the Kingston Complete Streets Plan
- Designate pedestrian-oriented commercial street frontages
- Designate alleys
- Add off-site flexibility for some open space/landscaping requirements
- Incentivize alternatives to surface parking and count on-street parking
- Increase allowed building heights in the Old Town/Waterfront and Lindvog Commercial design districts from 35-feet to 45-feet
- Repeal the required palette of exterior colors

2016 Kitsap County Comprehensive Plan – Kingston Subarea Plan (Chapter 8)

- Reorganize, update, and retire relevant goals and policies

Lead Agency: Kitsap County

Staff contact: Peter Best, Senior Planner, Kitsap DCD, Planning and Environmental Programs Div.

Location of proposal, including street address, if any: This is a non-project action applicable to the downtown portion of the Kingston Urban Growth Area, Kitsap County.

The lead agency for this proposal has determined that it does not have a probable significant adverse impact on the environment. An environmental impact statement (EIS) is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information on file with the lead agency. This information is available to the public on request.

This DNS is issued under 197-11-340(2); the lead agency will not act on this proposal for 14 days from the date below. The SEPA comment period is extended through January 8, 2020. This is a non-project action.

COMMENTS:

This is a nonproject action per WAC 197-11-704(2)(b). Adoption by ordinance of the amendment by the Kitsap County Board of Commissioners is expected in April of 2020.

Responsible Official: Scott Diener

SEPA Coordinator: Steve Heacock

Position/Title: SEPA Coordinator, Dept. of Community Development Phone: (360) 337-5777

Address: 619 Division Street, Port Orchard, WA 98366

DATE: 12/16/2019

Signature: _____



SEPA ENVIRONMENTAL CHECKLIST

Purpose of checklist:

Governmental agencies use this checklist to help determine whether the environmental impacts of your proposal are significant. This information is also helpful to determine if available avoidance, minimization or compensatory mitigation measures will address the probable significant impacts or if an environmental impact statement will be prepared to further analyze the proposal.

Instructions for applicants:

This environmental checklist asks you to describe some basic information about your proposal. Please answer each question accurately and carefully, to the best of your knowledge. You may need to consult with an agency specialist or private consultant for some questions. You may use "not applicable" or "does not apply" only when you can explain why it does not apply and not when the answer is unknown. You may also attach or incorporate by reference additional studies reports. Complete and accurate answers to these questions often avoid delays with the SEPA process as well as later in the decision-making process.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will help describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

Instructions for Lead Agencies:

Please adjust the format of this template as needed. Additional information may be necessary to evaluate the existing environment, all interrelated aspects of the proposal and an analysis of adverse impacts. The checklist is considered the first but not necessarily the only source of information needed to make an adequate threshold determination. Once a threshold determination is made, the lead agency is responsible for the completeness and accuracy of the checklist and other supporting documents.

Use of checklist for nonproject proposals: [\[help\]](#)

For nonproject proposals (such as ordinances, regulations, plans and programs), complete the applicable parts of sections A and B plus the [SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS \(part D\)](#). Please completely answer all questions that apply and note that the words "project," "applicant," and "property or site" should be read as "proposal," "proponent," and "affected geographic area," respectively. The lead agency may exclude (for non-projects) questions in Part B - Environmental Elements –that do not contribute meaningfully to the analysis of the proposal.

A. Background [\[help\]](#)

1. Name of proposed project, if applicable: [\[help\]](#)

2019 Kitsap County Comprehensive Plan Amendment – Downtown Kingston Phase 2 (a non-project action)

2. Name of applicant: [\[help\]](#)

Kitsap County Department of Community Development

3. Address and phone number of applicant and contact person: [\[help\]](#)

Peter Best, Senior Planner

PBest@co.kitsap.wa.us

360-337-7098

614 Division St – MS36

Port Orchard, WA 98366

4. Date checklist prepared: [\[help\]](#)

December 2, 2019

5. Agency requesting checklist: [\[help\]](#)

Kitsap County

6. Proposed timing or schedule (including phasing, if applicable): [\[help\]](#)

Amendment is expected to be adopted in April 2020.

7. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain. [\[help\]](#)

The Kitsap County Comprehensive Plan, including the Kingston Subarea Plan, and the Kitsap County Code is subject to continuous review and may be amended in future years. Any future amendments to the Kitsap County Code will be consistent with and facilitate implementation of the goals and policies in the Kingston Subarea Plan and will be subject to applicable environmental review.

8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal. [\[help\]](#)

None.

9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain. [\[help\]](#)

Not applicable for this non-project action.

10. List any government approvals or permits that will be needed for your proposal, if known. [\[help\]](#)

This amendment will need to be adopted by Kitsap County ordinance.

11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. (Lead agencies may modify this form to include additional specific information on project description.) [\[help\]](#)

This non-project action expands on and implements Phase 1 amendments adopted in 2018, is intended to remove regulatory barriers to achieving the existing adopted vision for downtown Kingston, and includes the following substantive amendments (see Attachment A):

- Kitsap County Code
 - Repeal redundant design standards in Chapter 17.480 (UVC Design Criteria) and move non-redundant design standards to Chapter 17.470 (Multifamily Design Criteria) and the Design Standards for the Community of Kingston
 - Define a high capacity transit station area around the Kingston ferry terminal
 - Create new parking standards for high capacity transit station areas with generally reduced parking requirements, increased flexibility for off-site parking, and incentivized alternatives to surface parking
 - Update KCC 17.410.044 (allowed uses) for the Kingston UVC zone
- Design Standards for the Community of Kingston
 - Revisions to improve the administration of the design standards and consistency with other codes
 - Update the applicability of the design standards to:
 - Remodels and additions
 - Single-family dwellings, duplexes, ADUs, and ALQs
 - Incorporate the Kingston Complete Streets Plan
 - Designate pedestrian-oriented commercial street frontages
 - Designate alleys
 - Increase flexibility for some open space and landscaping requirements to be off-site
 - Incentivize alternatives to surface parking and count on-street parking toward commercial parking requirements

- Increase maximum building heights in the Old Town/Waterfront and Lindvog Commercial design districts from 35-feet to 45-feet with a required 10-foot upper story setback for buildings over 35-feet high
- Repeal the required palette of exterior colors
- 2016 Kitsap County Comprehensive Plan (Chapter 8)
 - Reorganize, update, and retire relevant goals and policies in the Kingston Subarea Plan

12. Location of the proposal. Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township, and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit applications related to this checklist. [\[help\]](#)

This is a non-project action that affects the downtown area of Kingston, WA. (see Map 1 – Vicinity attached).

B. Environmental Elements [\[help\]](#)

1. Earth [\[help\]](#)

a. General description of the site: [\[help\]](#)

(circle one): Flat, rolling, hilly, steep slopes, mountainous, other _____

b. What is the steepest slope on the site (approximate percent slope)? [\[help\]](#)

Not applicable for this non-project action

c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any agricultural land of long-term commercial significance and whether the proposal results in removing any of these soils. [\[help\]](#)

Not applicable for this non-project action. There is no agricultural land in downtown Kingston.

d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. [\[help\]](#)

Map 3A -Critical Areas, attached, identifies potential geohazard areas. Unstable slopes exist along the shoreline.

e. Describe the purpose, type, total area, and approximate quantities and total affected area of any filling, excavation, and grading proposed. Indicate source of fill. [\[help\]](#)

Not applicable for this non-project action

- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe. [\[help\]](#)

Not applicable for this non-project action

- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)? [\[help\]](#)

Not applicable for this non-project action

- h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

2. Air [\[help\]](#)

- a. What types of emissions to the air would result from the proposal during construction, operation, and maintenance when the project is completed? If any, generally describe and give approximate quantities if known. [\[help\]](#)

Not applicable for this non-project action

- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe. [\[help\]](#)

Not applicable for this non-project action

- c. Proposed measures to reduce or control emissions or other impacts to air, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

3. **Water** [\[help\]](#)

a. Surface Water:

- 1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into. [\[help\]](#)

Marine waters include Appletree Cove and Puget Sound. Two streams identified in Kitsap County GIS data as fish bearing are located in the downtown area. The stream mapped along Iowa Street and Main Street is piped for that distance. No wetlands are currently mapped by Kitsap County in the subject area. See Map 3A – Critical Areas, attached.

- 2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans. [\[help\]](#)

Not applicable for this non-project action

- 3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material. [\[help\]](#)

Not applicable for this non-project action

- 4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known. [\[help\]](#)

Not applicable for this non-project action

- 5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan. [\[help\]](#)

No, the proposal affects upland areas that are predominantly outside of the floodplain. Some shoreline parcels have low lying areas that are mapped in the floodplain by Kitsap County GIS data.

- 6) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge. [\[help\]](#)

Not applicable for this non-project action

b. Ground Water:

- 1) Will groundwater be withdrawn from a well for drinking water or other purposes? If so, give a general description of the well, proposed uses and approximate quantities withdrawn from the well. Will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known. [\[help\]](#)

Not applicable for this non-project action

- 2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals. . . ; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve. [\[help\]](#)

Not applicable for this non-project action

c. Water runoff (including stormwater):

- 1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe. [\[help\]](#)

Not applicable for this non-project action

- 2) Could waste materials enter ground or surface waters? If so, generally describe. [\[help\]](#)

Not applicable for this non-project action

- 3) Does the proposal alter or otherwise affect drainage patterns in the vicinity of the site? If so, describe. [\[help\]](#)

Not applicable for this non-project action

d. Proposed measures to reduce or control surface, ground, and runoff water, and drainage pattern impacts, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

4. **Plants** [\[help\]](#)

- a. Check the types of vegetation found on the site: [\[help\]](#)

various deciduous tree: alder, maple, aspen, other

various evergreen tree: fir, cedar, pine, other

various shrubs

grass

pasture

crop or grain

Orchards, vineyards or other permanent crops.

wet soil plants: cattail, buttercup, bullrush, skunk cabbage, other

water plants: water lily, eelgrass, milfoil, other

other types of vegetation

- b. What kind and amount of vegetation will be removed or altered? [\[help\]](#)

Not applicable for this non-project action

- c. List threatened and endangered species known to be on or near the site. [\[help\]](#)

None known.

- d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

- e. List all noxious weeds and invasive species known to be on or near the site. [\[help\]](#)

None known.

5. **Animals** [\[help\]](#)

- a. List any birds and other animals which have been observed on or near the site or are known to be on or near the site. [\[help\]](#) _____

Examples include:

birds: hawk, heron, eagle, songbirds, other:

mammals: deer, bear, elk, beaver, other:

fish: bass, salmon, trout, herring, shellfish, other _____

Unknown.

- b. List any threatened and endangered species known to be on or near the site. [\[help\]](#)

Chinook Salmon, Steelhead Trout, Southern Resident Killer Whale.

- c. Is the site part of a migration route? If so, explain. [\[help\]](#)

Not applicable for this non-project action

- d. Proposed measures to preserve or enhance wildlife, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

- e. List any invasive animal species known to be on or near the site. [\[help\]](#)

None known.

6. Energy and Natural Resources [\[help\]](#)

- a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc. [\[help\]](#)

Not applicable for this non-project action

- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe. [\[help\]](#)

The proposed action will allow for maximum building heights to increase from 35-feet to 45-feet in some parts of downtown. This is not expected to significantly affect the potential use of solar energy by adjacent properties because allowed building heights in adjacent areas currently range from 35-feet to 55-feet. As a downtown area, solar energy collection would be expected to be located on roofs. Additionally, a significant portion of downtown is on a southward facing slopes, which increases solar access across the area.

- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any: [\[help\]](#)

Not applicable for this non-project action

7. **Environmental Health** [\[help\]](#)

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste, that could occur as a result of this proposal? If so, describe. [\[help\]](#)

Not applicable for this non-project action

- 1) Describe any known or possible contamination at the site from present or past uses. [\[help\]](#)

None known

- 2) Describe existing hazardous chemicals/conditions that might affect project development and design. This includes underground hazardous liquid and gas transmission pipelines located within the project area and in the vicinity. [\[help\]](#)

Not applicable for this non-project action

- 3) Describe any toxic or hazardous chemicals that might be stored, used, or produced during the project's development or construction, or at any time during the operating life of the project. [\[help\]](#)

Not applicable for this non-project action

- 4) Describe special emergency services that might be required. [\[help\]](#)

Not applicable for this non-project action

- 5) Proposed measures to reduce or control environmental health hazards, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

b. Noise [\[help\]](#)

- 1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)? [\[help\]](#)

None

2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site. [\[help\]](#)

Not applicable for this non-project action. Future development may generate noise from traffic, construction, and operations.

3) Proposed measures to reduce or control noise impacts, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

8. Land and Shoreline Use [\[help\]](#)

a. What is the current use of the site and adjacent properties? Will the proposal affect current land uses on nearby or adjacent properties? If so, describe. [\[help\]](#)

The downtown area includes a mix of commercial, residential, transportation, and other public uses. The proposed amendment will not affect current land uses in or adjacent to the downtown area. The proposed amendment will allow for additional uses in the UVC zone that will be compatible with the general mix of uses expected in a downtown area.

b. Has the project site been used as working farmlands or working forest lands? If so, describe. How much agricultural or forest land of long-term commercial significance will be converted to other uses as a result of the proposal, if any? If resource lands have not been designated, how many acres in farmland or forest land tax status will be converted to nonfarm or nonforest use? [\[help\]](#)

No

1) Will the proposal affect or be affected by surrounding working farm or forest land normal business operations, such as oversize equipment access, the application of pesticides, tilling, and harvesting? If so, how: [\[help\]](#)

Not applicable for this non-project action

c. Describe any structures on the site. [\[help\]](#)

The downtown area contains a mix of one, two and three story residential and commercial structures.

d. Will any structures be demolished? If so, what? [\[help\]](#)

This non-project action will not demolish existing structures but is expected to stimulate development within the downtown area consistent with the existing vision and planned growth for the area in the Kitsap County Comprehensive Plan, including the Kinston Subarea Plan. Existing structures may be demolished in the future when parcels are redeveloped, which would be subject to applicable permitting and environmental review.

e. What is the current zoning classification of the site? [\[help\]](#)

The subject area includes the following zoning classifications: Commercial (C), Urban Village Center (UVC), Neighborhood Center (NC), Urban High Residential (UH), Urban Medium Residential (UM), Urban Low Residential (UL), Urban Restricted (UR), and Park (P). This non-project action will not change zoning classifications or zoning boundaries.

f. What is the current comprehensive plan designation of the site? [\[help\]](#)

The subject area includes the following land use designations: Urban High-Intensity Commercial (UHIC), Urban Low-Intensity Commercial (ULIC), Urban High-Density Residential (UHDR), Urban Medium-Density Residential (UMDR), Urban Low-Density Residential (ULDR), and Public Facilities (PF). This amendment will not change land use designations or land use boundaries.

g. If applicable, what is the current shoreline master program designation of the site? [\[help\]](#)

Small portions of the downtown area are covered by High Intensity, Shoreline Residential, and Urban Conservancy shoreline designations.

h. Has any part of the site been classified as a critical area by the city or county? If so, specify. [\[help\]](#)

Yes, portions are mapped as geologically hazardous areas, critical aquifer recharge areas, frequently flooded areas, and eagle habitat. See Maps 3A and 3B, attached.

i. Approximately how many people would reside or work in the completed project? [\[help\]](#)

This non-project action is expected to stimulate development within the downtown area consistent with the existing vision for the area in the Kitsap County Comprehensive Plan, including the Kingston Subarea Plan. Land capacity estimates based on the proposed amendments indicate the population and employment capacity of the Kingston UGA will not change significantly. The population capacity of the UGA is estimated to increase by 58 people to 2,831 (1.3% below the UGA target). The employment capacity of the UGA is estimated to decrease by 33 jobs to 718 (14.7% above the UGA target). Countywide capacities are estimated to remain within +/- 5% of adopted targets. Kitsap County considers planned capacities within +/- 5% of adopted targets to be in balance.

j. Approximately how many people would the completed project displace? [\[help\]](#)

This non-project action is expected to increase residential and commercial capacity within the downtown area and is not expected to cause general displacement. Limited or temporary displacement may occur in the future when parcels are redeveloped.

k. Proposed measures to avoid or reduce displacement impacts, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any: [\[help\]](#)

The proposed amendment addresses policy and regulatory barriers to achieving the existing vision for development and planned growth for downtown Kingston in the Kitsap County Comprehensive Plan, including the Kingston Subarea Plan.

m. Proposed measures to reduce or control impacts to agricultural and forest lands of long-term commercial significance, if any: [\[help\]](#)

Not applicable for this non-project action

9. Housing [\[help\]](#)

a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing. [\[help\]](#)

This non-project action is expected to stimulate new housing development within the downtown area consistent with the vision for the area in the Kitsap County Comprehensive plan, including the Kingston Subarea Plan. Land capacity estimates based on the proposed amendments indicate the capacity for housing in the UGA will increase by 32 units, which are likely to be market rate.

- b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing. [\[help\]](#)

This non-project action is not expected to eliminate housing units.

- c. Proposed measures to reduce or control housing impacts, if any: [\[help\]](#)

Not applicable for this non-project action. This non-project action is expected to stimulate new housing development within downtown area consistent with the vision in the Kitsap County Comprehensive Plan, including the Kingston Subarea Plan. Any future development will be subject to applicable permitting and environmental review.

10. *Aesthetics* [\[help\]](#)

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed? [\[help\]](#)

This non-project action will not build new structures. However, proposed changes to development code would allow building heights within the Old Town/Waterfront and Lindvog Commercial design districts to increase from 35-feet to 45-feet, with a 10-foot upper story setback for buildings over 35-feet. Most of the surrounding areas currently have allowed building heights up to 45-feet, while small portions of the surrounding area have allowed building heights up to 35-feet and 55-feet.

- b. What views in the immediate vicinity would be altered or obstructed? [\[help\]](#)

This non-project action will not build new structures. However, proposed changes to development code would allow building heights within the Old Town/Waterfront and Lindvog Commercial design districts to increase from 35-feet to 45-feet, with a 10-foot upper story setback for buildings over 35-feet. Future buildings of this height could alter or obstruct views from existing buildings in the area, however slopes in the area help minimize view impacts for many surrounding areas. Most of the surrounding areas currently have allowed building heights up to 45-feet, while small portions of the surrounding area have allowed building heights up to 35-feet and 55-feet. Therefore, future buildings constructed at allowed heights would be of comparable height.

- b. Proposed measures to reduce or control aesthetic impacts, if any: [\[help\]](#)

A 10-foot upper story setback will be required for buildings over 35-feet, which will help mitigate some view impacts. Slopes in the area help minimize view impacts for many surrounding areas. Any future development will be subject to applicable permitting and environmental review.

11. **Light and Glare** [\[help\]](#)

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur? [\[help\]](#)

Not applicable for this non-project action. This non-project action will not change existing requirements in Kitsap County regarding light and glare.

- b. Could light or glare from the finished project be a safety hazard or interfere with views? [\[help\]](#)

Not applicable for this non-project action. This non-project action will not change existing requirements in Kitsap County regarding light and glare.

- c. What existing off-site sources of light or glare may affect your proposal? [\[help\]](#)

Not applicable for this non-project action.

- d. Proposed measures to reduce or control light and glare impacts, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

12. **Recreation** [\[help\]](#)

- a. What designated and informal recreational opportunities are in the immediate vicinity? [\[help\]](#)

The Village Green Park and Community Center, Kola Kole Park, Washington Boulevard Park, and the Port of Kingston (park, marina, boat ramp) are in or adjacent to the downtown area.

- b. Would the proposed project displace any existing recreational uses? If so, describe. [\[help\]](#)

Not applicable for this non-project action. This non-project action will not affect existing recreational opportunities in the downtown area.

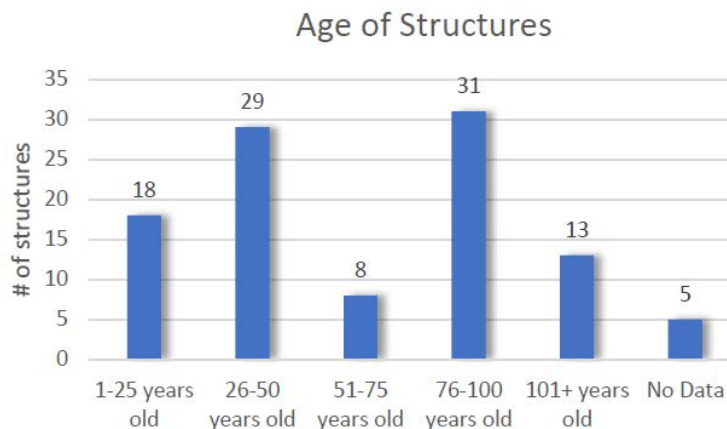
- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

13. **Historic and cultural preservation** [\[help\]](#)

- a. Are there any buildings, structures, or sites, located on or near the site that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers ? If so, specifically describe. [\[help\]](#)

Unknown. A distribution of existing structure age is shown below.



- b. Are there any landmarks, features, or other evidence of Indian or historic use or occupation? This may include human burials or old cemeteries. Are there any material evidence, artifacts, or areas of cultural importance on or near the site? Please list any professional studies conducted at the site to identify such resources. [\[help\]](#)

Unknown

- c. Describe the methods used to assess the potential impacts to cultural and historic resources on or near the project site. Examples include consultation with tribes and the department of archeology and historic preservation, archaeological surveys, historic maps, GIS data, etc. [\[help\]](#)

Not applicable for this non-project action

- d. Proposed measures to avoid, minimize, or compensate for loss, changes to, and disturbance to resources. Please include plans for the above and any permits that may be required. [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

14. **Transportation** [\[help\]](#)

- a. Identify public streets and highways serving the site or affected geographic area and describe proposed access to the existing street system. Show on site plans, if any. [\[help\]](#)

The entire downtown area is accessed by public streets and highways as well as Washington State auto ferry service.

- b. Is the site or affected geographic area currently served by public transit? If so, generally describe. If not, what is the approximate distance to the nearest transit stop? [\[help\]](#)

The downtown area is located adjacent to the Washington State ferry service to downtown Edmonds, WA and the Kitsap Transit passenger-only fast ferry service to downtown Seattle, WA. Kitsap Transit provides routed and non-routed bus service from areas in North Kitsap through the downtown area to the ferry terminals.

- c. How many additional parking spaces would the completed project or non-project proposal have? How many would the project or proposal eliminate? [\[help\]](#)

This non-project action will not construct or eliminate parking spaces. However, proposed changes to development code would reduce the amount of parking required for future development within the proposed high capacity transit station area.

- d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities, not including driveways? If so, generally describe (indicate whether public or private). [\[help\]](#)

This non-project action is expected to stimulate development within the downtown area largely consistent with the assumptions used for the Kitsap County Capital Facilities Plan and will not require new improvements beyond those already planned or as would be required through the applicable permitting and environmental review of any development projects.

- e. Will the project or proposal use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe. [\[help\]](#)

The downtown area is located adjacent to Washington State ferry service to downtown Edmonds, WA and Kitsap Transit passenger-only fast ferry service to downtown Seattle, WA. Commuter rail service is available in downtown Edmonds, WA.

- f. How many vehicular trips per day would be generated by the completed project or proposal? If known, indicate when peak volumes would occur and what percentage of the volume would be trucks (such as commercial and nonpassenger vehicles). What data or transportation models were used to make these estimates? [\[help\]](#)

This non-project action is expected to stimulate development within the downtown area largely consistent with the assumptions used for the Kitsap County Capital Facilities Plan and will not require new improvements beyond those already planned or as would be required through the applicable permitting and environmental review of any development projects.

- g. Will the proposal interfere with, affect or be affected by the movement of agricultural and forest products on roads or streets in the area? If so, generally describe. [\[help\]](#)

Not applicable for this non-project action

- h. Proposed measures to reduce or control transportation impacts, if any: [\[help\]](#)

None. This non-project action is expected to stimulate development within the downtown area largely consistent with the assumptions used for the Kitsap County Capital Facilities Plan and will not require new improvements beyond those already planned or as would be required through the applicable permitting and environmental review of any development projects.

15. Public Services [\[help\]](#)

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, public transit, health care, schools, other)? If so, generally describe. [\[help\]](#)

Proposed amendments to parking requirements have the potential to increase demand on public transit over time. This non-project action is expected to stimulate development within the downtown area largely consistent with the assumptions used for the Kitsap County Capital Facilities Plan and will not increase demand on other public services.

- b. Proposed measures to reduce or control direct impacts on public services, if any. [\[help\]](#)

None. This non-project action is expected to stimulate development within the downtown area largely consistent with the assumptions used for the Kitsap County Capital Facilities Plan and will not require new improvements beyond those already planned or as would be required through the applicable permitting and environmental review of any development projects.

16. Utilities [\[help\]](#)

- a. Circle utilities currently available at the site: [\[help\]](#)
 electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system,
other _____

- b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed. [\[help\]](#)

None. This non-project action is expected to stimulate development within the downtown area largely consistent with the assumptions used for the Kitsap County Capital Facilities Plan and will not require new improvements beyond those already planned or as would be required through the applicable permitting and environmental review of any development projects.

C. Signature [\[help\]](#)

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision.

Signature: 

Name of signee: Peter Best

Position and Agency/Organization: Senior Planner, Kitsap County Department of
Community Development

Date Submitted: 12/2/2019

D. Supplemental sheet for nonproject actions [\[help\]](#)

(IT IS NOT NECESSARY to use this sheet for project actions)

Because these questions are very general, it may be helpful to read them in conjunction with the list of the elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage, or release of toxic or hazardous substances; or production of noise?

This non-project action is not likely to increase discharge to water; emissions to air; production storage, or release of toxic or hazardous substances; or production of noise.

Proposed measures to avoid or reduce such increases are:

None. Any future development will be subject to applicable permitting and environmental review.

2. How would the proposal be likely to affect plants, animals, fish, or marine life?

This non-project action is not likely to affect plants, animals, fish, or marine life.

Proposed measures to protect or conserve plants, animals, fish, or marine life are:

None. Any future development will be subject to applicable permitting and environmental review.

3. How would the proposal be likely to deplete energy or natural resources?

This non-project action is not likely to deplete energy or natural resources.

Proposed measures to protect or conserve energy and natural resources are:

None. Any future development will be subject to applicable permitting and environmental review.

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection; such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, floodplains, or prime farmlands?

This non-project action is not likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection.

Proposed measures to protect such resources or to avoid or reduce impacts are:

None. Any future development will be subject to applicable permitting and environmental review.

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans?

This non-project action is not likely to significantly affect land and shoreline use and will not allow or encourage land or shoreline uses incompatible with existing plans. This non-project action is expected to stimulate development within the downtown area consistent with the existing vision for the area in the Kitsap County Comprehensive Plan, including the Kingston Subarea Plan.

Proposed changes to development code would allow additional land uses within the UVC zone which are compatible with existing uses and a downtown environment.

Proposed changes to development code would also allow building heights within the Old Town/Waterfront and Lindvog Commercial design districts to increase from 35-feet to 45-feet, with a 10-foot upper story setback for buildings over 35-feet. Future buildings of this height could alter or obstruct views from existing buildings in the area, however slopes in the area help minimize view impacts for many surrounding areas. Most of the surrounding areas currently have allowed building heights up to 45-feet, while small portions of the surrounding area have allowed building heights up to 35-feet and 55-feet. Therefore, future buildings constructed at allowed heights would be of comparable height.

Proposed measures to avoid or reduce shoreline and land use impacts are:

A 10-foot upper story setback will be required for buildings over 35-feet, which will help mitigate some view impacts. Slopes in the area help minimize view impacts for many surrounding areas. Any future development will be subject to applicable permitting and environmental review.

6. How would the proposal be likely to increase demands on transportation or public services and utilities?

Proposed amendments to parking requirements have the potential to increase demand on public transit over time. Otherwise, this non-project action is expected to stimulate development within the downtown area largely consistent with the assumptions used for the Kitsap County Capital Facilities Plan and will not require new improvements beyond those already planned or as would be required through the applicable permitting and environmental review of any development projects.

Proposed measures to reduce or respond to such demand(s) are:

None. Any future development will be subject to applicable permitting and environmental review.

7. Identify, if possible, whether the proposal may conflict with local, state, or federal laws or requirements for the protection of the environment.

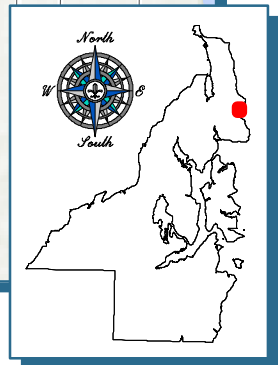
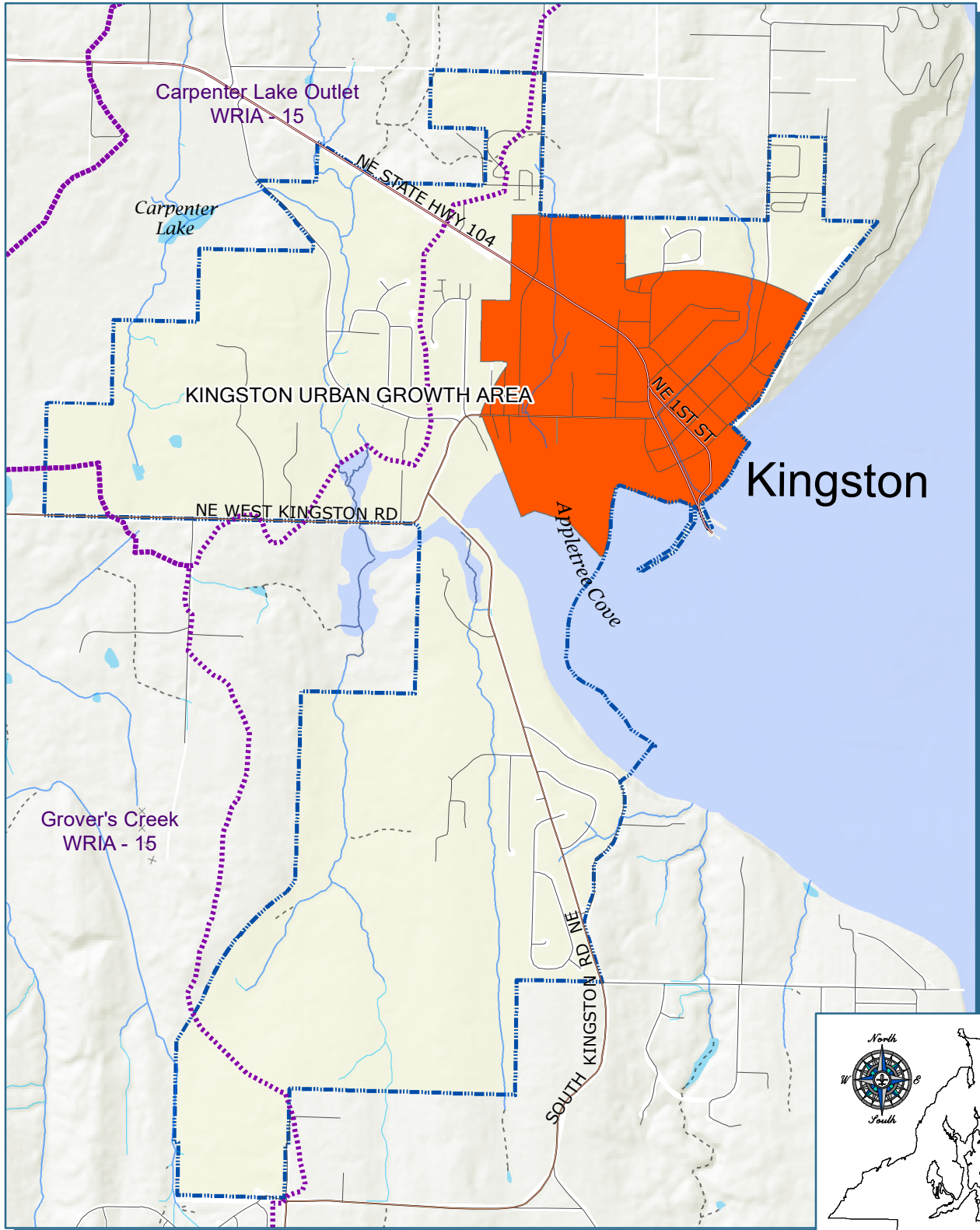
This non-project action does not conflict with local, state, or federal laws or requirements for the protection of the environment.

Attachment C

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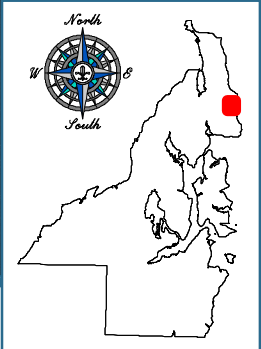


Map 1 - Vicinity Downtown Kingston



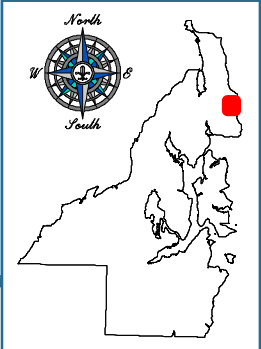
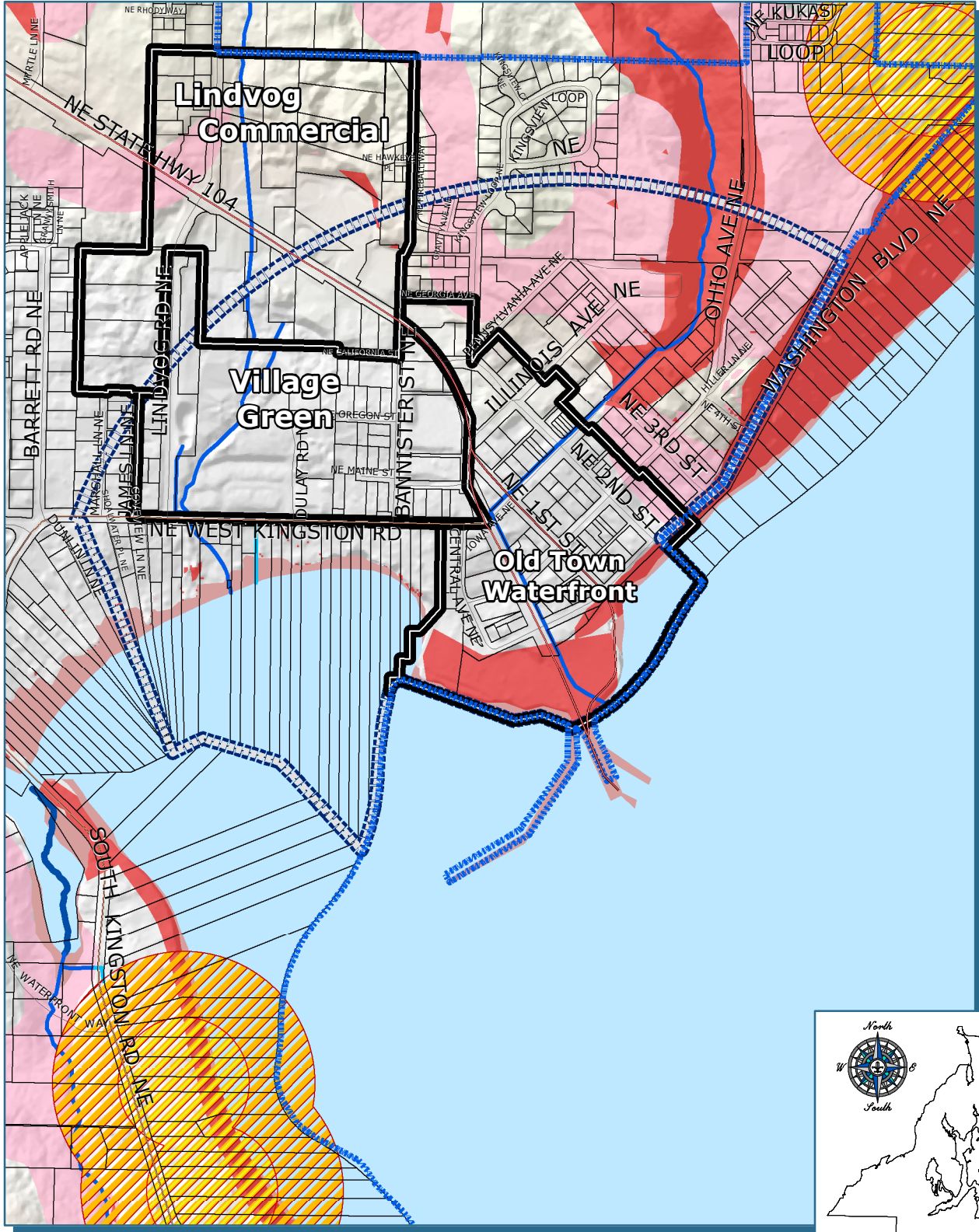


Map 2 - Aerial Photo (2017) Downtown Kingston



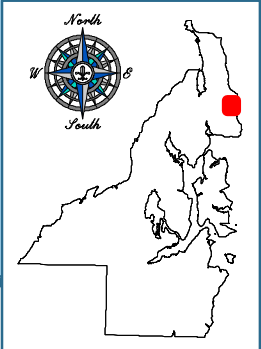
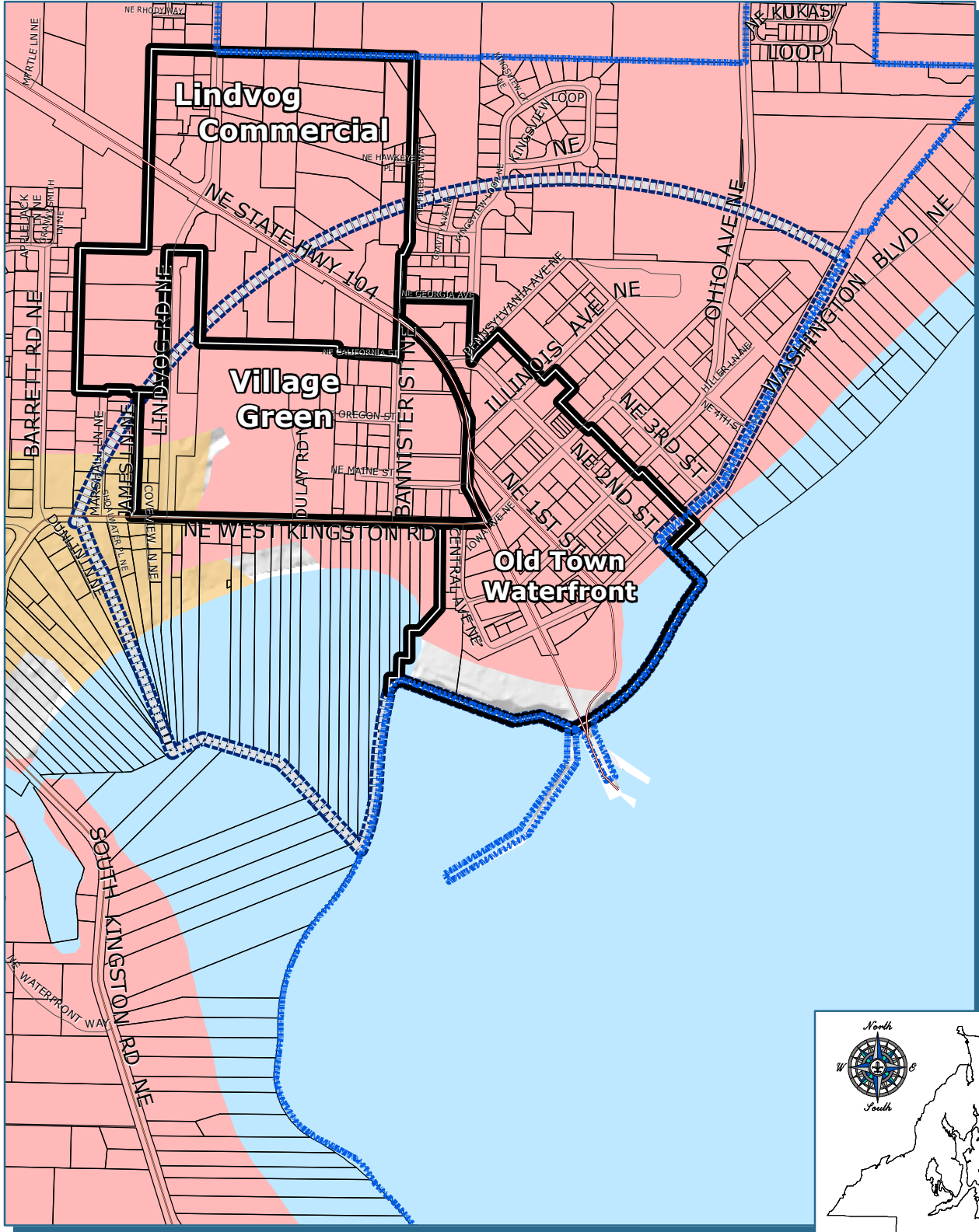


Map 3A - Critical Areas Downtown Kingston



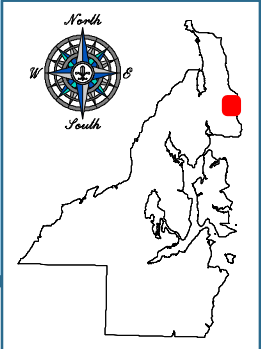
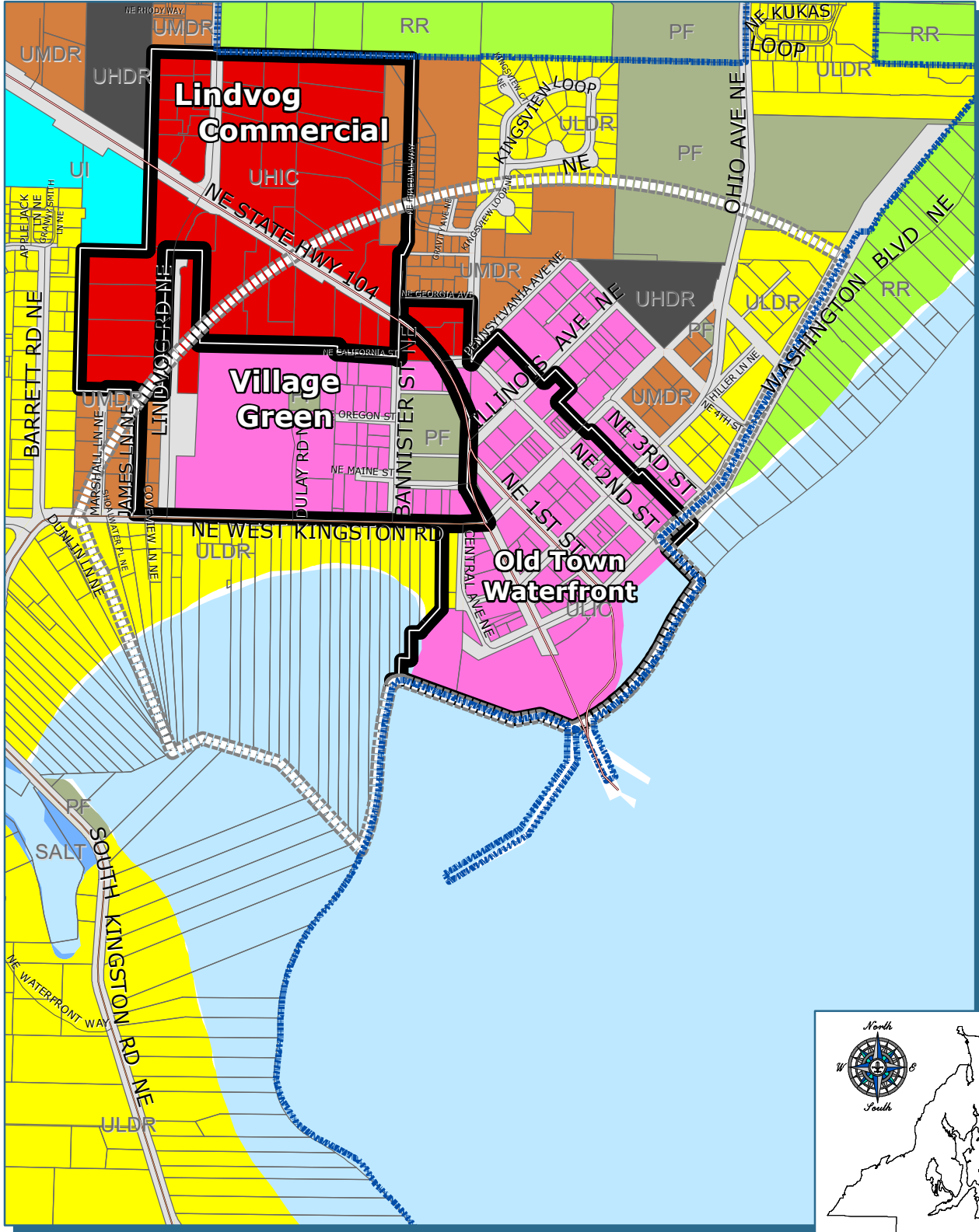


Map 3B - Critical Aquifer Recharge Areas Downtown Kingston



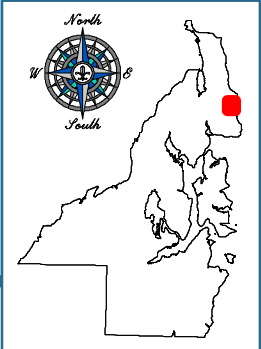
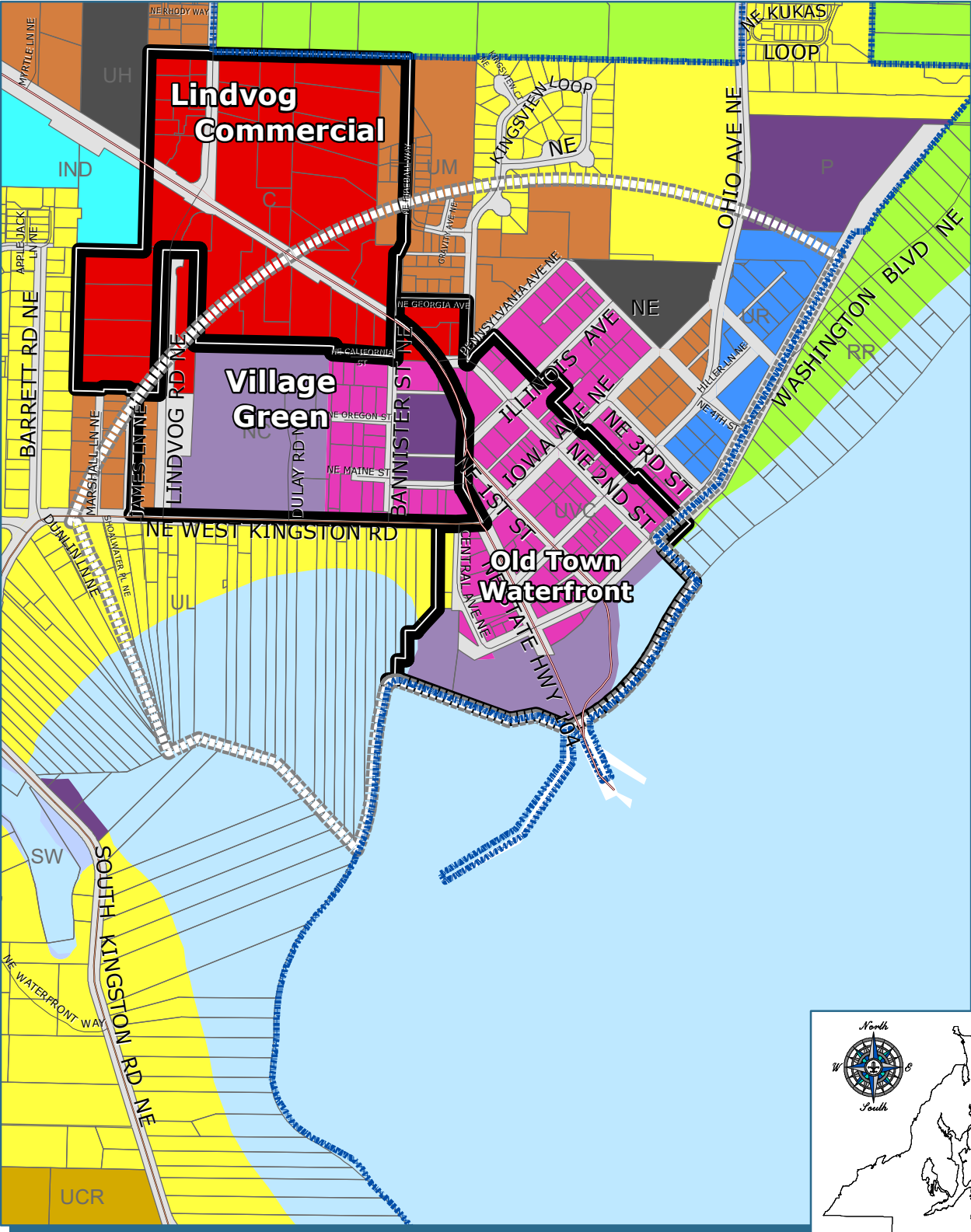


Map 4 - Current Land Use Designation Map Downtown Kingston





Map 5 - Current Zoning Classification Map Downtown Kingston



Legend for Maps 1 - 3


 Greater Puget Sound Hydrology

 High Capacity Transit Station Area

Fish Habitat Water Type Code

 (S) Designated Shoreline of the State

 (F) Fish Habitat

 (N) Non-fish Habitat

 Design District

Street Center Lines

 State Highway

 Collector / Arterial

 Local Access; Local Road


FEMA Flood Hazard Zone

 High Risk Areas Zones

 Subject Parcel(s)

 Tax Parcels


Geohazards


 High Hazard Area

 Moderate Hazard Area

 Major Watershed Boundary

Critical Aquifer Recharge Areas

 Category I
Critical aquifer recharge areas are those areas where the potential for certain land use activities to adversely affect groundwater is high.

 Category II
Critical aquifer recharge areas are areas that provide recharge effects to aquifers that are current or potentially will become potable water supplies and are vulnerable to contamination based on the type of land use activity.

Designated Urban Growth Areas

 Kingston Unincorporated Urban Growth Area

 Unincorporated Urban Growth Area Boundary

Eagle Habital Buffers

 330 feet

 660 feet

Legend for Map 4

Comprehensive Plan Designations

Plan Description

-  Rural Residential
-  Urban Industrial
-  Urban High-Intensity Commercial/Mixed Use
-  Urban Low-Intensity Commercial/Mixed Use
-  Urban Low-Density Residential
-  Urban Medium-Density Residential
-  Urban High-Density Residential
-  Public Facility

Designated Urban Growth Areas

-  Unincorporated Urban Growth Area

-
-  High Capacity Transit Station Area

-
-  Design Districts

Street Center Lines












- State Highway
- Collector / Arterial
- Local Access; Local Road

-
-  Tax Parcels (Full Ownership)

-
-  Greater Puget Sound Hydrology

Legend for Map 5

Zoning Designations

-  RR - Rural Residential (1 DU/5 Ac)
-  IND - Industrial
-  Commercial (10-30 DU/Ac)
-  NC - Neighborhood Commercial (10-30 DU/Ac)
-  UCR - Urban Cluster Residential (5-9 DU/Ac)
-  UR - Urban Restricted (1-5 DU/Ac)
-  UL - Urban Low Residential (5-9 DU/Ac)
-  UM - Urban Medium Residential (10-18 DU/Ac)
-  UH - Urban High Residential (19-30 DU/Ac)
-  Urban Village Center (min 10 DU/Ac)
-  Park




Designated Urban Growth Areas


-  Unincorporated Urban Growth Area

-
-  High Capacity Transit Station Area

-
-  Design Districts

Street Center Lines

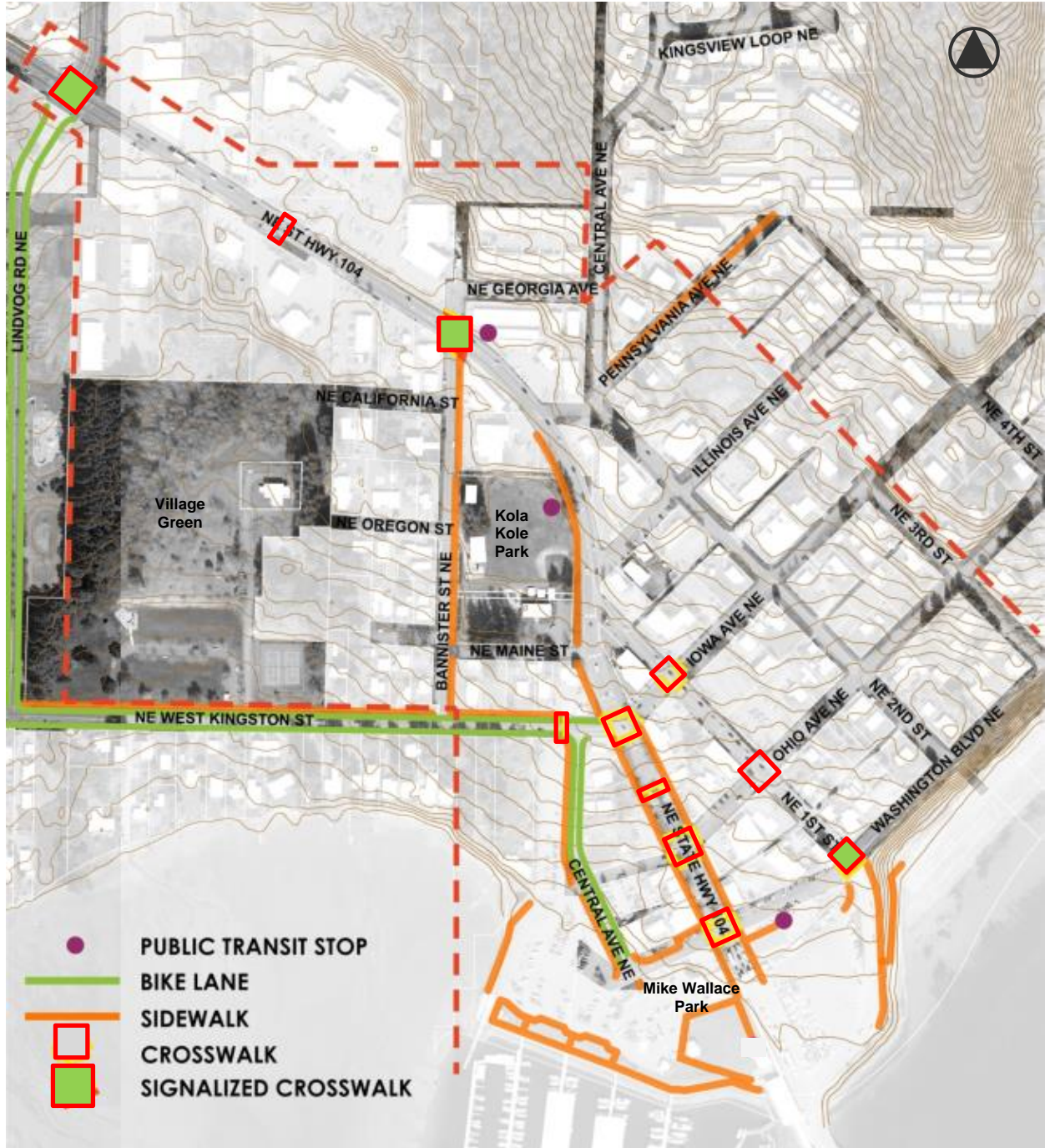
-  State Highway
-  Collector / Arterial
-  Local Access; Local Road

-
-  Tax Parcels (Full Ownership)

-
-  Greater Puget Sound Hydrology

Map 6A

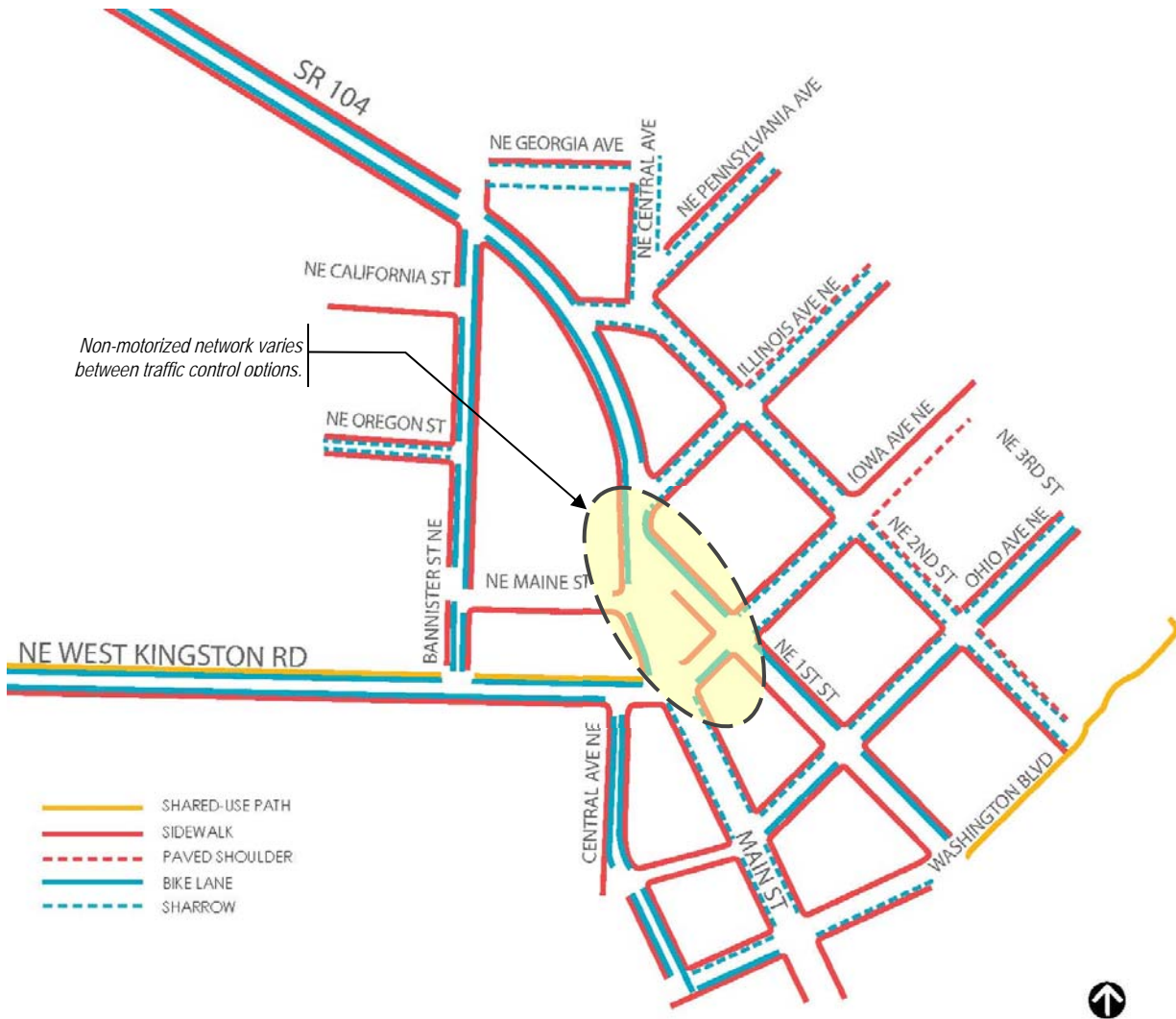
Figure 5. Non-Motorized Existing Facilities



Source: MacLeod Reckord, Heffron Transportation, 2015.

Map 6B

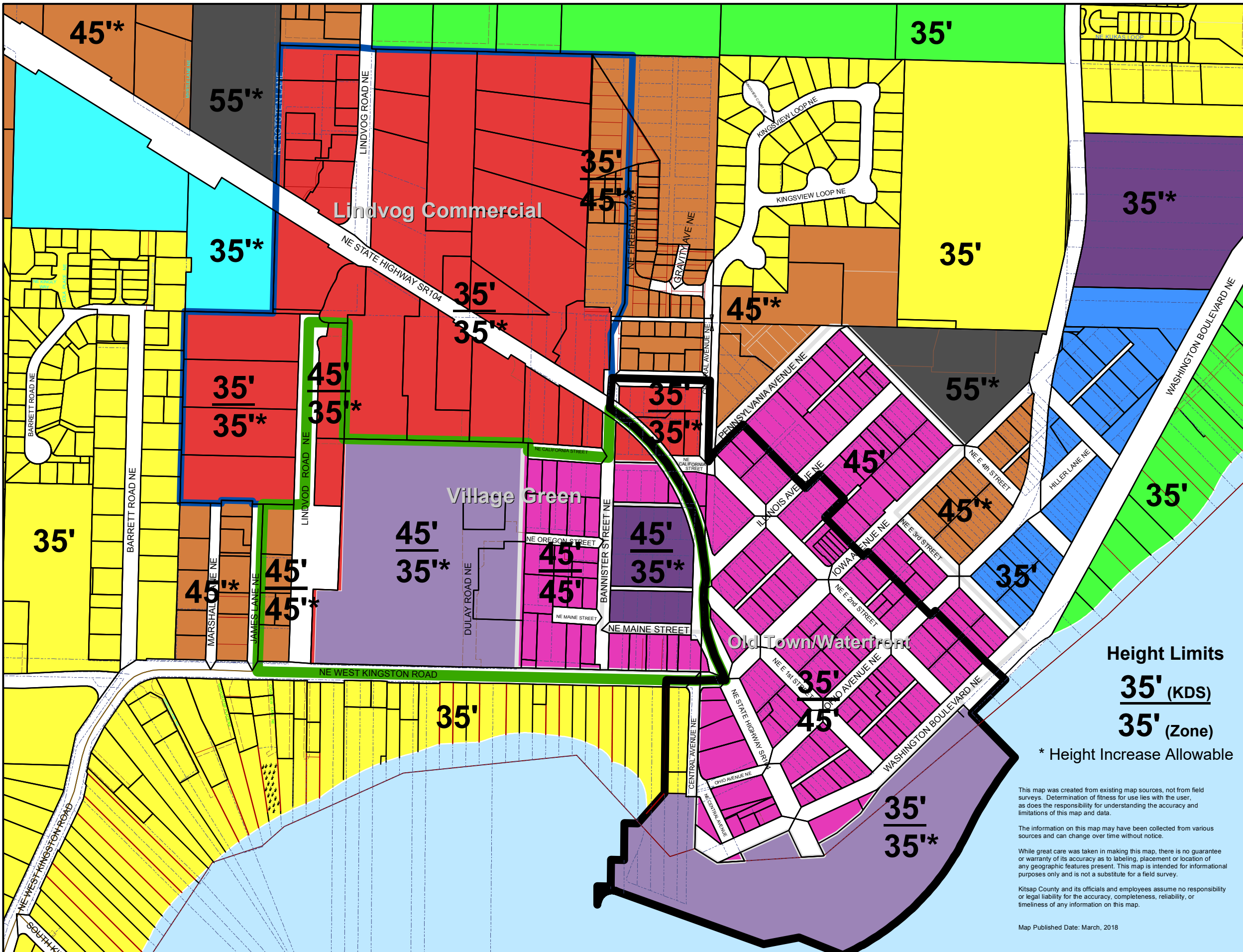
Figure 11. Non-Motorized Network with Implementation of Complete Streets Projects



Source: MacLeod Reckord, 2016.

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Map 7 - Current Allowed Maximum Building Height (Downtown Kingston)



Legend

- Village Green Design District
- Old Town/Waterfront Design District
- Lindvog Commercial Design District
- Real Property Tax Accounts

Zoning Designations

- RR - Rural Residential (1 DU/5 Ac)
- Commercial (10-30 DU/Ac)
- IND - Industrial
- NC - Neighborhood Commercial (10-30 DU/Ac)
- UR - Urban Restricted (1-5 DU/Ac)
- UH - Urban High Residential (19-30 DU/Ac)
- UL - Urban Low Residential (5-9 DU/Ac)
- UM - Urban Medium Residential (10-18 DU/Ac)
- UVC - Urban Village Center (up to 18 DU/Ac)
- Park

Comprehensive Plan 10-year update Ord No. 534-2016

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 614 Division Street, MS-36, Port Orchard, Washington 98366
 VOICE (360) 337-5777

0 100 200 400 600 800 1,000 Feet

Height Limits
35' (KDS)
35' (Zone)
 * Height Increase Allowable

This map was created from existing map sources, not from field surveys. Determination of fitness for use lies with the user as does the responsibility for understanding the accuracy and limitations of this map and data.

The information on this map may have been collected from various sources and can change over time without notice.

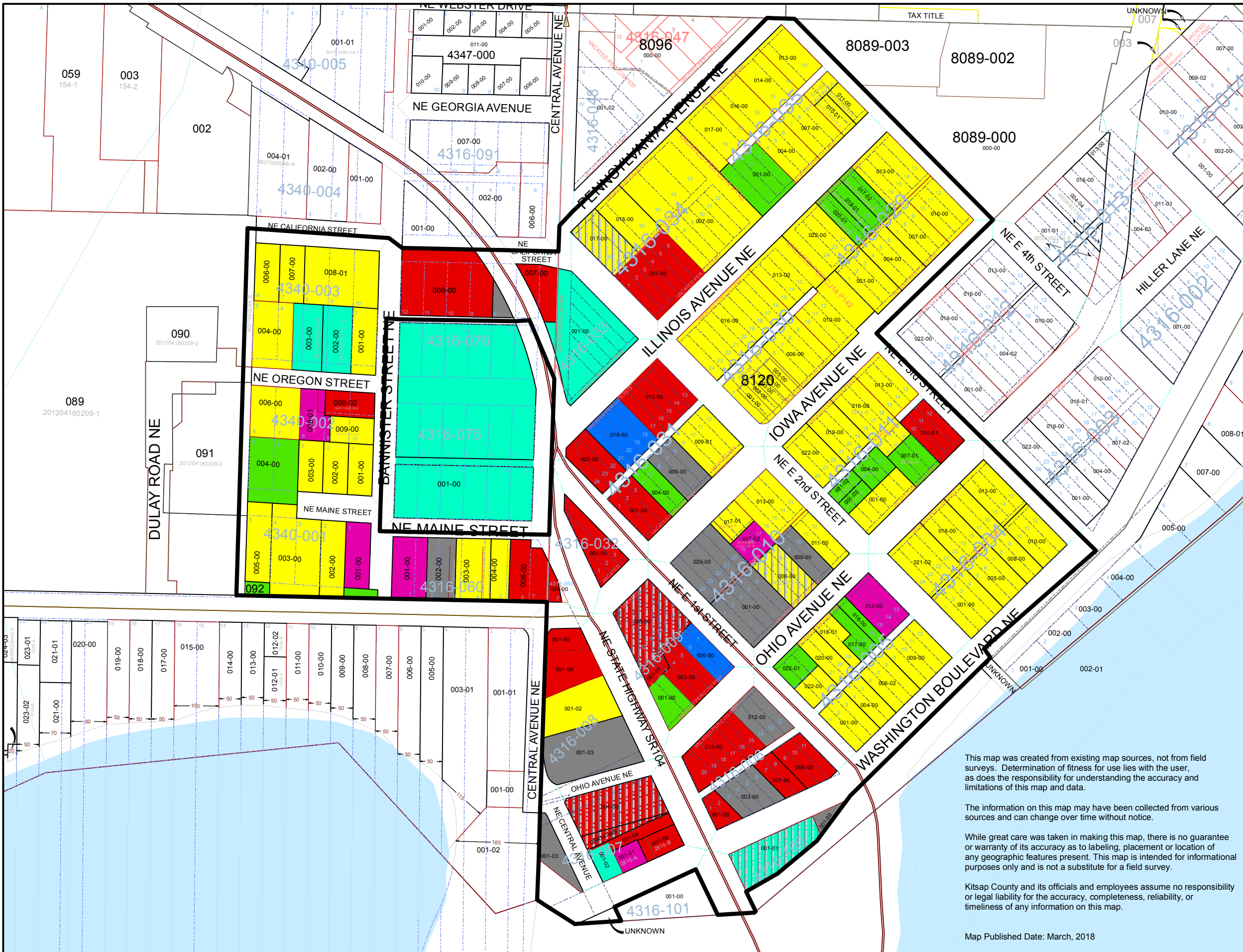
While great care was taken in making this map, there is no guarantee or warranty of its accuracy as to labeling, placement or location of any geographic features present. This map is intended for informational purposes only and is not a substitute for a field survey.

Kitsap County and its officials and employees assume no responsibility or legal liability for the accuracy, completeness, reliability, or timeliness of any information on this map.

Map Published Date: March, 2018

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Map 8 - Existing Uses (Kingston UVC Zone)



Legend

- Urban Village Center Zone Boundary
- Real Property Tax Accounts

Use

- Commercial only
- Cultural/Recreational/School
- Government/Utilities
- Mixed Use
- Parking
- Residential only
- Undeveloped

Secondary Use overlay

- Commercial Parking

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0 100 200 400 600 Feet

North
South
West
East

This map was created from existing map sources, not from field surveys. Determination of fitness for use lies with the user, as does the responsibility for understanding the accuracy and limitations of this map and data.

The information on this map may have been collected from various sources and can change over time without notice.

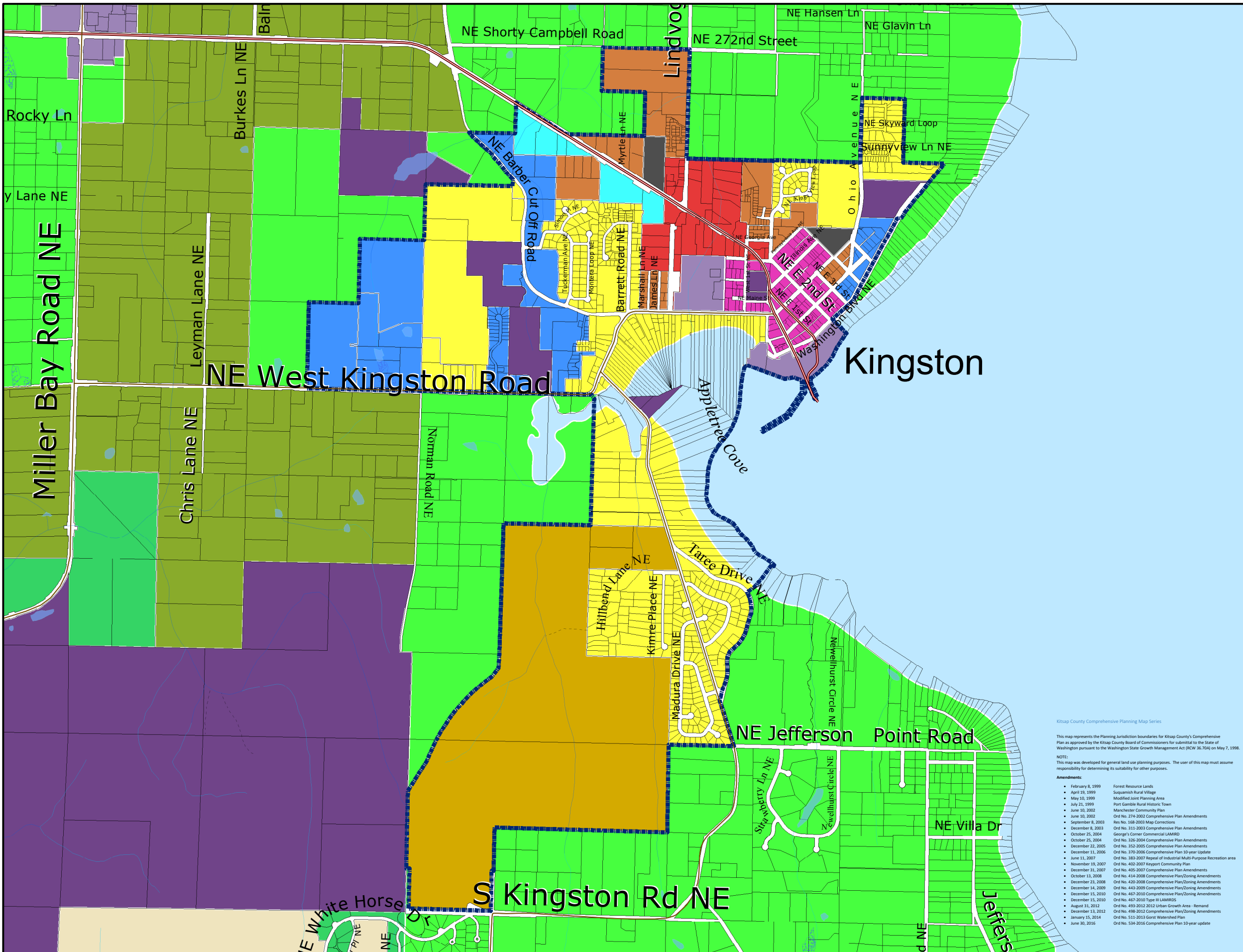
While great care was taken in making this map, there is no guarantee or warranty of its accuracy as to labeling, placement or location of any geographic features present. This map is intended for informational purposes only and is not a substitute for a field survey.

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Map Published Date: March, 2018

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Map 9 - Current Zoning Classifications (Kingston Urban Growth Area)



Legend

Kitsap County Zoning Classifications

- RP - Rural Protection (1 DU/10 Ac)
- RR - Rural Residential (1 DU/5 Ac)
- RW - Rural Wooded (1 DU/20 Ac)
- Commercial (10-30 DU/Ac)
- IND - Industrial
- NC - Neighborhood Commercial (10-30 DU/Ac)
- UCR - Urban Cluster Residential (5-9 DU/Ac)
- UR - Urban Restricted (1-5 DU/Ac)
- UH - Urban High Residential (19-30 DU/Ac)
- UL - Urban Low Residential (5-9 DU/Ac)
- UM - Urban Medium Residential (10-18 DU/Ac)
- UVC - Urban Village Center (up to 18 DU/Ac)
- Park
- T - Tribal Land
- Urban Growth Area Boundary
- Real Property Tax Accounts

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 614 Division Street, MS-36, Port Orchard, Washington 98366
 VOICE (360) 337-5777

0 500 1,000 2,000 3,000 4,000 Feet

North
South
West
East

Kitsap County Comprehensive Planning Map Series

This map represents the Planning Jurisdiction boundaries for Kitsap County's Comprehensive Plan as approved by the Kitsap County Board of Commissioners for submittal to the State of Washington pursuant to the Washington State Growth Management Act (RCW 36.70A) on May 7, 1998.

NOTE:
 This map was developed for general land use planning purposes. The user of this map must assume responsibility for determining its suitability for other purposes.

Amendments:

- February 8, 1999 Forest Resource Lands
- April 19, 1999 Squamish Rural Village
- May 10, 1999 Modified Joint Planning Area
- July 21, 1999 Port Gamble Rural Historic Town
- June 10, 2002 Manchester Community Plan
- June 10, 2002 Ord No. 274-2002 Comprehensive Plan Amendments
- September 8, 2003 Res No. 148-2003 Map Corrections
- December 8, 2003 Ord No. 311-2003 Comprehensive Plan Amendments
- October 25, 2004 George's Corner Commercial LAMIBD
- October 25, 2004 Ord No. 326-2004 Comprehensive Plan Amendments
- December 22, 2005 Ord No. 352-2005 Comprehensive Plan Amendments
- December 11, 2006 Ord No. 370-2006 Comprehensive Plan 10-year Update
- June 11, 2007 Ord No. 383-2007 Repeal of Industrial Multi-Purpose Recreation area
- November 19, 2007 Ord No. 403-2007 Keppert Community Plan
- December 31, 2007 Ord No. 405-2007 Comprehensive Plan Amendments
- October 13, 2008 Ord No. 414-2008 Comprehensive Plan/Zoning Amendments
- December 23, 2008 Ord No. 420-2008 Comprehensive Plan/Zoning Amendments
- December 14, 2009 Ord No. 443-2009 Comprehensive Plan/Zoning Amendments
- December 15, 2010 Ord No. 467-2010 Comprehensive Plan/Zoning Amendments
- December 15, 2010 Ord No. 467-2010 Type II LAMIBD5
- August 21, 2012 Ord No. 493-2012 Urban Growth Area - Remaind
- December 13, 2012 Ord No. 498-2012 Comprehensive Plan/Zoning Amendments
- January 15, 2014 Ord No. 511-2013 Gorst Watershed Plan
- June 30, 2016 Ord No. 534-2016 Comprehensive Plan 10-year update

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**2019 Annual Comprehensive Plan Amendment Process
Downtown Kingston (Phase 2)**



Summary of Process Used to Develop the Amendment

This document is intended to provide a concise summary of the process used to develop the recommendations of the Kingston UVC Workgroup and of the Department of Community Development.

Kingston UVC Workgroup

The members of the Kingston UVC Workgroup listed below were appointed by Kitsap County Commissioner Rob Gelder (District 1, North Kitsap) in February 2018 and completed their work in two phases for the 2018 (Phase 1) and 2019 (Phase 2) annual Comprehensive Plan amendment cycles.

<u>Workgroup Members</u>		<u>Meetings</u>
Beth Berglund	Kingston Citizen Advisory Committee - Chair	3/8/2018
Betsy Cooper	Citizen	3/15/2018
Dave Wetter	Developer	3/22/2018
Jet Woelke	Realtor	3/29/2018
Johnny Walker	Downtown Business Owner	4/5/2018
Jon Rose	Developer	4/12/2018
Jim Pivarnik	Port of Kingston - Executive Director	4/26/2018
Mike Brown	Developer	5/10/2018
Nancy Langwith	North Kitsap Tourism Coalition	5/24/2018
Rick Lanning	Developer	6/28/2018
		7/9/2018
		8/9/2018
<u>Staff</u>		8/29/2018
Peter Best, Senior Planner		8/30/2018
Liz Williams, Planner		5/20/2019

The Workgroup was chartered with the following scope of work:

The Workgroup will develop alternative solutions to address existing policy or regulatory barriers to achieving the existing vision for development and planned growth in the Kingston UVC zone. This is not a new subarea or master planning process. Community development is influenced by regulations, markets, and infrastructure. The Workgroup will focus on a regulatory review while acknowledging how regulations interplay with local market and infrastructure factors unique to the Kingston UVC zone.

The scope of the Workgroup’s review will be on the following, as related to the UVC:

- *Kingston Subarea Plan policies (2016 Comprehensive Plan)*
- *Land Use Map boundaries (2016 Comprehensive Plan)*
- *Zoning Map boundaries*
- *KCC 17.260 (Urban Village Center)*
- *KCC 17.410 (Allowed Uses)*
- *KCC 17.420 (Density, Dimensions, and Design)*
- *KCC 17.480 (Urban Village Center Design Criteria)*
- *Design Standards for the Community of Kingston*

- *Other sections of KCC Title 17 as specifically needed to support changes to the items above*

The scope of the Workgroup's review does not include the following development regulations:

- *KCC Title 12 (Storm Water Drainage)*
- *KCC Title 16 (Land Division and Development)*
- *KCC Title 19 (Critical Areas Ordinance)*
- *KCC Title 22 (Shoreline Master Program)*

Identifying and Prioritizing Barriers

The Department of Community Development conducted a preliminary assessment of potential policy/regulatory, market, and infrastructure barriers to achieving the existing vision for development and planned growth in the Kingston UVC zone (located below in this attachment). The Workgroup reviewed the staff's preliminary assessment, identified additional potential barriers, and independently sorted the potential barriers by significance (located below in this attachment). The Workgroup then reviewed the results of their sorting exercise and selected the following priority barriers to address that were consistent with their charter.

Priority Policy Barriers (Phase 1)

- Mixed-use requirements
- The need for residential development first (commercial will follow)
- Parking

Priority Regulatory Barriers (Phase 2)

- Residential use restrictions (limiting housing options):
 - Detached SFR prohibited
 - Accessory dwellings prohibited
 - Unconventional housing options (i.e. boarding houses, bedrooms for rent)
- Non-residential use restrictions (particularly for a downtown)
- Redundant and conflicting design criteria
- Remodels/additions and changing the use of existing structures (e.g. from residential to business) requires the entire property to meet current design standards (instead of proportionally)
- Maximum building height: different height limits affect potential density; view corridor issues
- Parking requirements:
 - On-site parking (located on the development site)
 - Off-site parking (located on a different property than the use it is serving)

Workgroup Recommendations (Phase 1 & 2)

With the support of staff, the Workgroup identified and evaluated preliminary alternative approaches to addressing these priority barriers. Staff provided research and analysis, including comparative assessments of comparable communities (see Attachments 4 & 5), and Workgroup members provided valuable insights regarding local markets, development trends, prior planning in the community, and community interests as well as significant assessments regarding parking (see Attachment 6). The Workgroup found consensus on recommendations regarding all of the barriers and therefore, formal alternatives were not developed. Phase 1 recommendations regarding Comprehensive Plan policy changes were adopted in December 2018. Phase 2 recommendations were focused on development code.

Department Recommendations (Phase 2)

The Department used a broader group of staff than had worked with the Workgroup, including current and long-range planning staff, to review the Workgroup's recommendations specifically and the areas of the development code subject to the amendments generally to:

- Further improve clarity, administration, and consistency of the applicable codes.
- Expand the scope of the amendment to the broader downtown area since the barriers and solutions identified are not limited to the UVC zone.
- Integrate recent planning for the area consistent with existing Comprehensive Plan policies (i.e. Kingston Complete Streets Plan).
- Integrate regional planning policies that were outside the scope of the Workgroup's charter (i.e. high capacity transit station area).
- Provide additional flexibility in the code that would better achieve the existing vision for a pedestrian-oriented downtown Kingston.
- Clarify, re-organize, and retire policies in the Kingston Subarea Plan as relevant to the changes proposed in the Phase 2 amendments.

The Department respects and values the significant efforts of the Workgroup. In most cases where the Department modified the Workgroup's recommendations or proposed alternative recommendations, they were intended to achieve the same or similar outcomes as intended by the Workgroup in a way the Department believes will be easier to administer or will be more consistent with other existing provisions of the development code. The Department's recommendations and the Workgroup's recommendations disagree in the following instance:

- Designation of pedestrian-oriented commercial street frontages (Amendment #2, Exhibit 1 - Section IV.A.1.c and Appendix B)

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Annual Comprehensive Plan Amendment Process for 2018 Review of the Kingston Urban Village Center (UVC) Zone



Summary of Preliminary Market Assessment

The Department of Community Development reviewed existing planning documents and data to answer the following questions.

- Is there market demand for development in the UVC zone?
- Is there property owner willingness to develop in the UVC zone?
- Is there market awareness in the UVC zone?
- Are there undeveloped or underdeveloped parcels in the UVC zone?
- How can market participants be better informed?

Summaries of market information responsive to the questions is attached. Below is a list of market issues identified by the preliminary staff assessment for consideration as potential market barriers.

- Lack of predictable customer base for commercial merchants – tourists vs. residents.
- Willingness to invest ahead of the market – some businesses want to re-invest but are waiting for stable residential population, or have a wait and see mind set.
- Small lot sizes and highly fragmented ownership.
- Existing parcels are underdeveloped based on density requirements for the UVC zone.
- Supply is limited -
 - Existing residential properties do not align with demand.
 - Existing upper-story apartments have been converted to office space or storage.
 - Housing affordability – existing residents may be priced out of the market if supply does not keep up with demand.
- Lack development support – system needed to walk landowners through the development process.
- Residents are moving to Edmonds and other communities to obtain desired housing choices.



**Annual Comprehensive Plan Amendment Process for 2018
Review of the Kingston Urban Village Center (UVC) Zone**



Market Information Summary

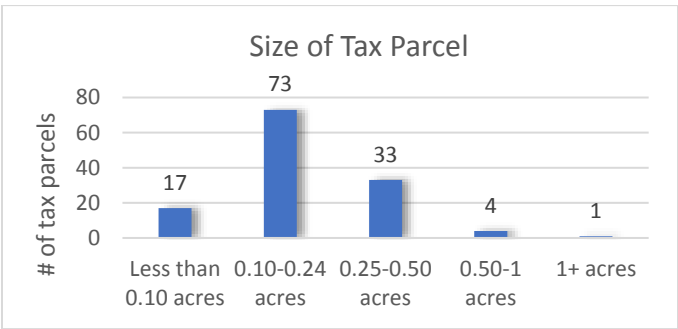
General Characteristics

- The Urban Village Center zone contains a total of 44.72 acres which is divided into 128 parcels.
- There are currently 99 landowners in the Urban Village Center.
- The assessed value of property in the Urban Village Center is \$28,173,245.

Parcels	Landowners	Total Acres	Acres (right of way not included)	Land Value	Building Value	Total Value
128	99	44.72	29.37	\$9,725,220	\$18,646,010	\$28,173,245

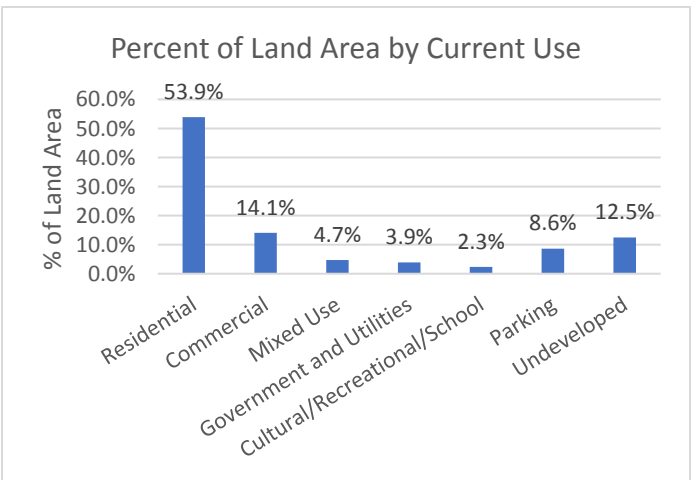
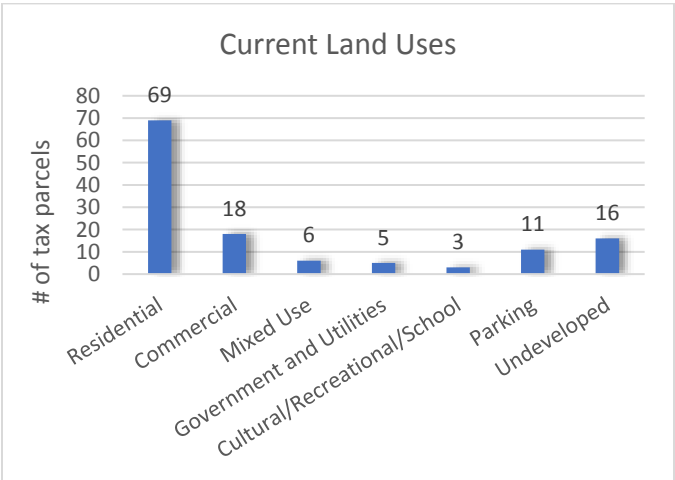
Lot Size

- The Urban Village Center was platted in 1890 (Town of Kingston) and 1932 (Worthington’s Community Beach Tracts). The chart below shows size distribution of current tax parcels, which typically include multiple platted lots.



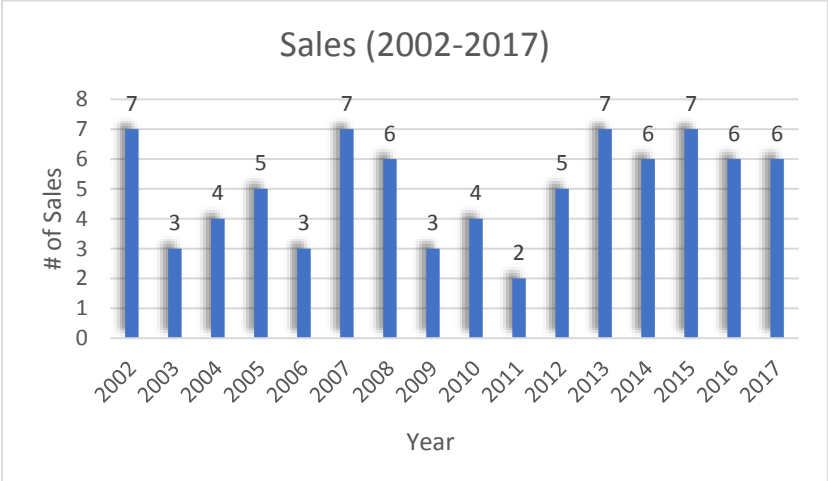
Land Use

- A map showing the distribution of current land uses can be found in Section 2.5.3.
- Residential uses make up approximately 54% of the parcels in the Urban Village Center.
- There are 16 undeveloped parcels which contain approximately 2.16 acres.



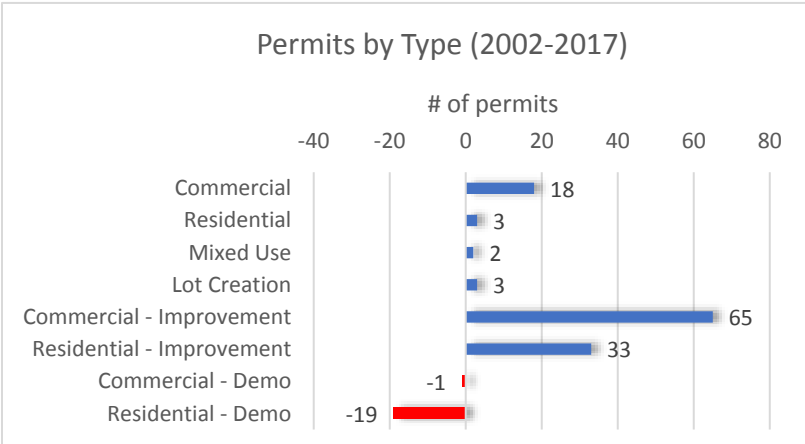
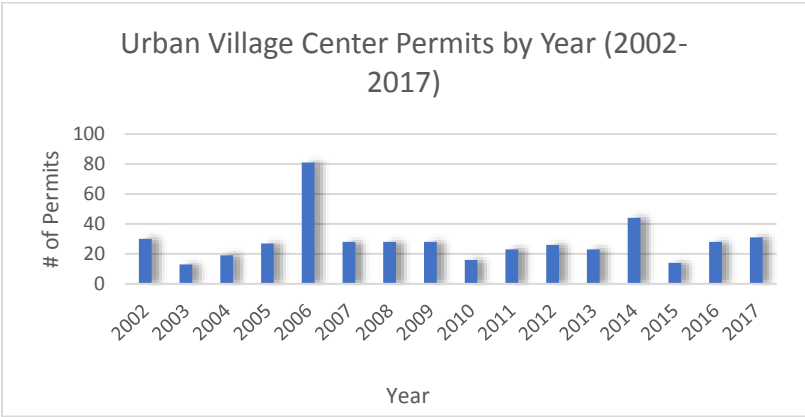
Sales (2002-2017)

- A total of 81 sales occurred in the Urban Village Center between 2002 and 2017.
- The Urban Village Center has averaged 5 sales transactions per year.



Permit Activity (2002-2017)

- A total of 238 permits were issued in the Urban Village Center between 2002 and 2017.
- There were 23 permits for construction of commercial, residential, and mixed-use structures.
- A total of 20 permits were issued for demolition of commercial or residential structures.



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Annual Comprehensive Plan Amendment Process for 2018 Review of the Kingston Urban Village Center (UVC) Zone



Summary of Preliminary Infrastructure Assessment

The Department of Community Development reviewed existing planning and regulatory documents to answer the following questions.

- What capital facilities service the UVC zone?
- Will capital facilities support buildout of the UVC zone?
- What capital facilities are needed to support the buildout of the UVC zone?
- What resources are needed to support the buildout of the UVC zone?
 - Funding opportunities
 - Land acquisition

Summaries of infrastructure information responsive to the questions is attached. Below is a list of infrastructure issues identified by the preliminary staff assessment for consideration as potential infrastructure barriers.

- Cost of frontage improvements for landowners that seek to complete infill and redevelopment projects.
- Perceived lack of coordination between Kitsap County, Kitsap Transit, Washington State Department of Transportation, Ferry System, and Port of Kingston.
- Lack of coordinated investment in district-wide infrastructure improvements.
- On-street parking is limited and passenger ferry service to Seattle will increase demand significantly (walk-on ferry accommodates 350 passengers).
- Stormwater runoff has the potential to negatively impact the Port of Kingston.



Annual Comprehensive Plan Amendment Process for 2018 Review of the Kingston Urban Village Center (UVC) Zone



Infrastructure Information Summary

The following infrastructure assessment provides a summary of information gathered from the 2016 Kitsap County Capital Facilities Plan and Kingston Complete Streets Report.

Transportation

- Washington Boulevard Project – Status: active project with anticipated completion 1/2/2020. This project will consist of constructing pedestrian/bike facilities on Washington Boulevard from SR 104 (outbound) to 3rd Street and pedestrian facilities from SR 104 (outbound) to Central Avenue. The project implements the Kingston Complete Streets Plan and includes ADA ramps as needed, luminaries as required, pedestrian lighting, sidewalks, and stormwater facilities.
- Kitsap Transit has an established bus route to the Urban Village Center which provides access to regional public transportation options.
- Kingston Complete Streets Plan provides recommended improvements and suggested strategies for funding future improvements (see Section 3.3 and 3.4 for more information).
- Kingston Ferry route is identified as a deficient roadway. A work committee is working to address issues with Highway 104 and ferry traffic.

Stormwater

- Current level of service complies with a 2007 National Pollution Discharge Elimination System permit.
- Land development activities requiring land use approval from Kitsap County are conditioned to meet the water quality, runoff control, and erosion control requirements of Kitsap County's Stormwater Design Manual.
- Kingston Regional Stormwater Facility Upgrade Project – Status: Feasibility and Preliminary Design Underway. Project involves water quality retrofit of existing development in Kingston. Project design and construction will depend on grant funding.
- Kingston - Eastside Green Street Project – Status: Conceptual (Design). This project will create a green street on Eastside in Kingston. Green street projects include techniques designed to handle rainwater in a more natural way that are built in a neighborhood setting, usually in the road right-of-way. Project funding has not been secured at this time.
- Kingston - Bannister Green Street Project – Status: Conceptual (Design). This project will create a green street on Bannister in Kingston. Green street projects include techniques designed to handle rainwater in a more natural way that are built in a neighborhood setting, usually in the road right-of-way. Project funding has not been secured at this time.

Water

- Kinston is served by the North Peninsula water system owned by Kitsap Public Utility District.
- The system serves residential and commercial customers.
- The current water system has the capacity to serve the planned growth in Kingston.

Waste Water

- The Kingston treatment plant serves the Kingston UGA and is managed by Kitsap County.
- Expansive sewer facility system located in Old Town Kingston which is described as predominately Puget Sound to the east, portion of Ohio Avenue to the northeast, Pennsylvania Avenue to the northwest and Appletree Cove to the southwest.
- Strategy identified for future expansions include: developer extension, ULID, and possible new funding sources (Community Development Districts (CDDs), Local Infrastructure Financing Tool (LIFT), etc.)
- Wastewater system has sufficient capacity to serve the planned growth of Kingston.

Solid Waste

- Solid waste in Kingston is managed by Kitsap County Public Works and processed at the Hansville Recycling and Garbage Facility.
- Solid waste facilities have capacity to serve planned growth in Kingston.

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**Annual Comprehensive Plan Amendment Process for 2018
Review of the Kingston Urban Village Center (UVC) Zone
Summary of Preliminary Policy & Regulatory Assessment**



Policy Review

The Department of Community Development reviewed the policies in the Kingston Subarea Plan (2016 Comprehensive Plan) and did not identify any issues during the preliminary staff assessment to put forward for consideration as barriers. Several policies were identified that directly support a review of development regulations, including the following:

Kingston Policy 41. After public parking availability and accessibility study is complete, consider updates or changes to parking requirements relating to occupancy in the Kingston Urban Growth Area.

Kingston Policy 47. Review mixed use standards for Urban Village Center and amend as necessary.

Kingston Policy 48. Increase residential density allowance in Urban Village Center.

Kingston Policy 50. Explore new code provisions to support redevelopment.

Kingston Policy 51. Update Kingston Downtown Design Standards to address ongoing public input.

Kingston Policy 52. Prioritize the required infrastructure to encourage urban medium and high density residential uses near commercial areas and public spaces.

Development Regulation Review

The Department of Community Development reviewed existing planning and regulatory documents to answer the following questions.

- What types of uses and densities are allowed in the UVC zone?
- What are the site development requirements in the UVC zone?
- Are there regulatory barriers to developing in the UVC zone?

Summaries of regulatory information responsive to the first two questions is attached. Below is a list of regulatory issues identified by the preliminary staff assessment for consideration as potential regulatory barriers.

- Kinston UVC zone boundary
 - The UVC zone boundary does not align with the boundaries of the Kingston Design Study districts; the Kingston Design Standards do not apply to part of the UVC zone
 - Given the current lot coverage standard for commercial or mixed-use development only, the extent of UVC zone may exceed the needed capacity for commercial and mixed-use development

- Lot Coverage
 - Standards require commercial-only or mixed-use development; Residential-only development is not allowed anywhere in the UVC (KCC 17.420.054)
 - Maximum lot coverage standards for mixed-use development in the UVC (KCC 17.420.054) are more restrictive than County-wide standards (KCC 17.420.035 A-B)
- Allowed uses
 - The table of allowed uses (KCC 17.410.044) may restrict uses that align with the vision for downtown Kingston
- Application of development standards to redevelopment and changes in use
 - Changing the use of an existing structure (e.g. changing an existing SFR from residential to office use) requires full compliance with existing development standards
 - Modifications of existing structures (e.g. minor addition) requires full compliance with existing development standards
 - Standards do not have thresholds allowing for partial/proportional compliance
- Density
 - The UVC zone density (10-18 DU/Ac) is consistent with the adjacent Urban Medium (UM) zone, but is less than the adjacent Neighborhood Commercial (NC) and Commercial (C) zones at 10-30 DU/Ac
- Building Height
 - The Kingston Design Standards reduce maximum building heights in the Old Town District to 35-feet (down from 45-feet allowed in base UVC zone)
- Parking requirements
 - Standards for the number of parking spaces may be higher than needed for an urban downtown close to ferry and transit center (KCC 17.
 - Off-street/on-site parking standards may be higher than feasible for small lots in the UVC zone
 - Off-street/off-site parking standards (e.g. 300' max distance) may be too restrictive
- Redundant design criteria
 - Multiple codes (KCC 17.470, 17.480) and Kingston Design Standards address the same design criteria categories
 - KCC 17.480 is geared towards large development of vacant land (i.e. Urban Town Center zone at McCormick Woods, which was removed in 2016 when annexed by Port Orchard) and may not be applicable to the UVC



**Annual Comprehensive Plan Amendment Process for 2018
Review of the Kingston Urban Village Center (UVC) Zone**



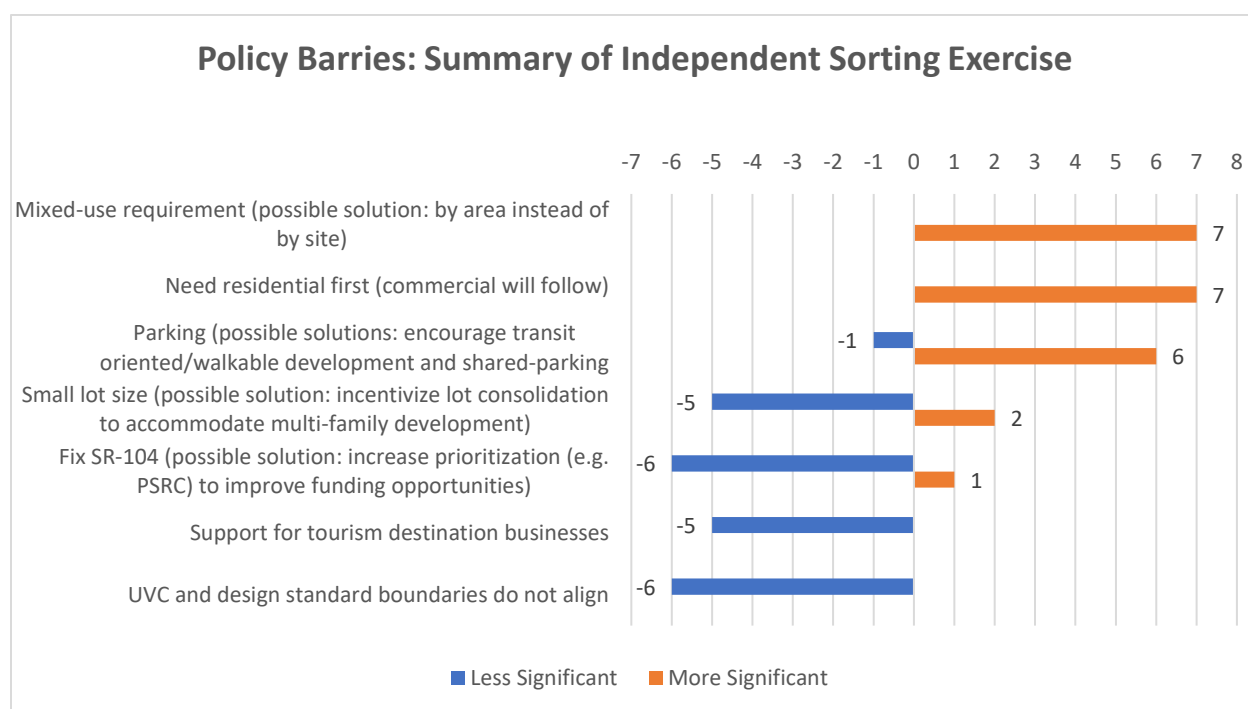
Workgroup Potential Barriers and Prioritization Exercise Results

Potential Market Barriers	Potential Infrastructure Barriers
Unpredictable customer base	Individual frontage improvement costs
Business investment waiting for more population	Lack of agency coordination
Small lot size & fragmented ownership	Lack of district-wide coordinated effort
Underdeveloped Density	Perceived lack of on-street parking
Demand for single-family residential	Increase demand for ferry parking
Does the market want mixed use	Regional stormwater management
Does market want businesses across entire UVC	Stormwater conveyance capacity
Mixed-use does not work in most of Kitsap County	Shared parking lots for off-site parking
Commercial rents are too low	Funding for regional and zone infrastructure
Demands will change as growth increases	Bus/Ferry transfer center
Awareness of transportation options	Non-ferry transit is very limited
Supply of one-level living	
Dwellings converted to business space	
Supply does not align with demand	
Affordability – prices increase when demand is greater than supply	
Loss of residents to other markets/areas	
Knowledge of how to navigate permit process	

The Workgroup was not asked to prioritize market and infrastructure barriers, but applied the understanding gained from identifying these barriers to their work identifying, prioritizing, and addressing policy and regulatory barriers.

Potential Policy Barriers

- Mixed-use requirement (possible solution: by area instead of by site)
- Need residential first (commercial will follow)
- Parking (possible solutions: encourage transit oriented/walkable development and shared-parking)
- Small lot size (possible solution: incentivize lot consolidation to accommodate multi-family development)
- Fix SR-104 (possible solution: increase prioritization (e.g. PSRC) to improve funding opportunities)
- Support for tourism destination businesses
- UVC and design standard boundaries do not align

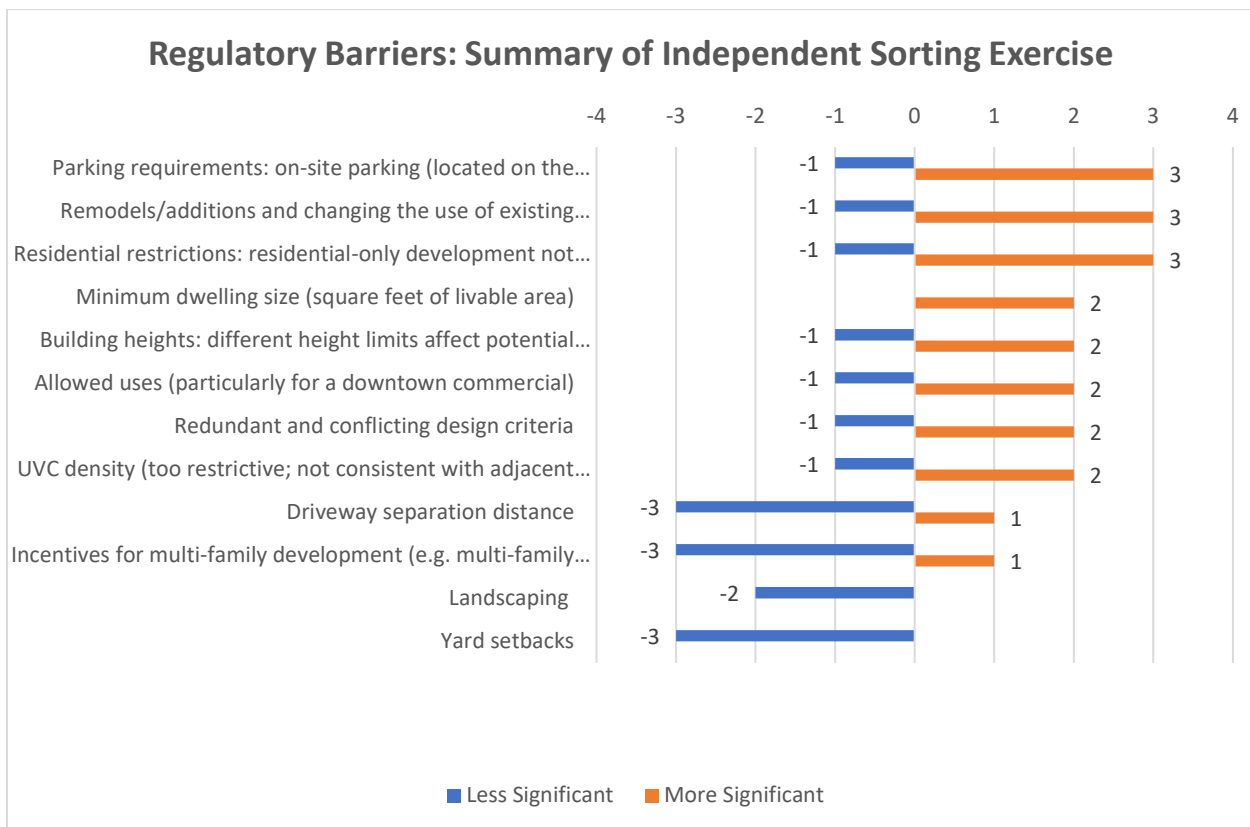


Each Workgroup member was asked to independently sort each potential barrier into three prioritization categories (“more significant”, “less significant”, and “not a barrier”). The charts in this document only show the number of responses for the more significant and less significant categories.

Note: The blue bars should not be read as negative numbers – they were only plotted as negative numbers so that the chart would plot the two categories on opposite sides of the axis for visual clarity.

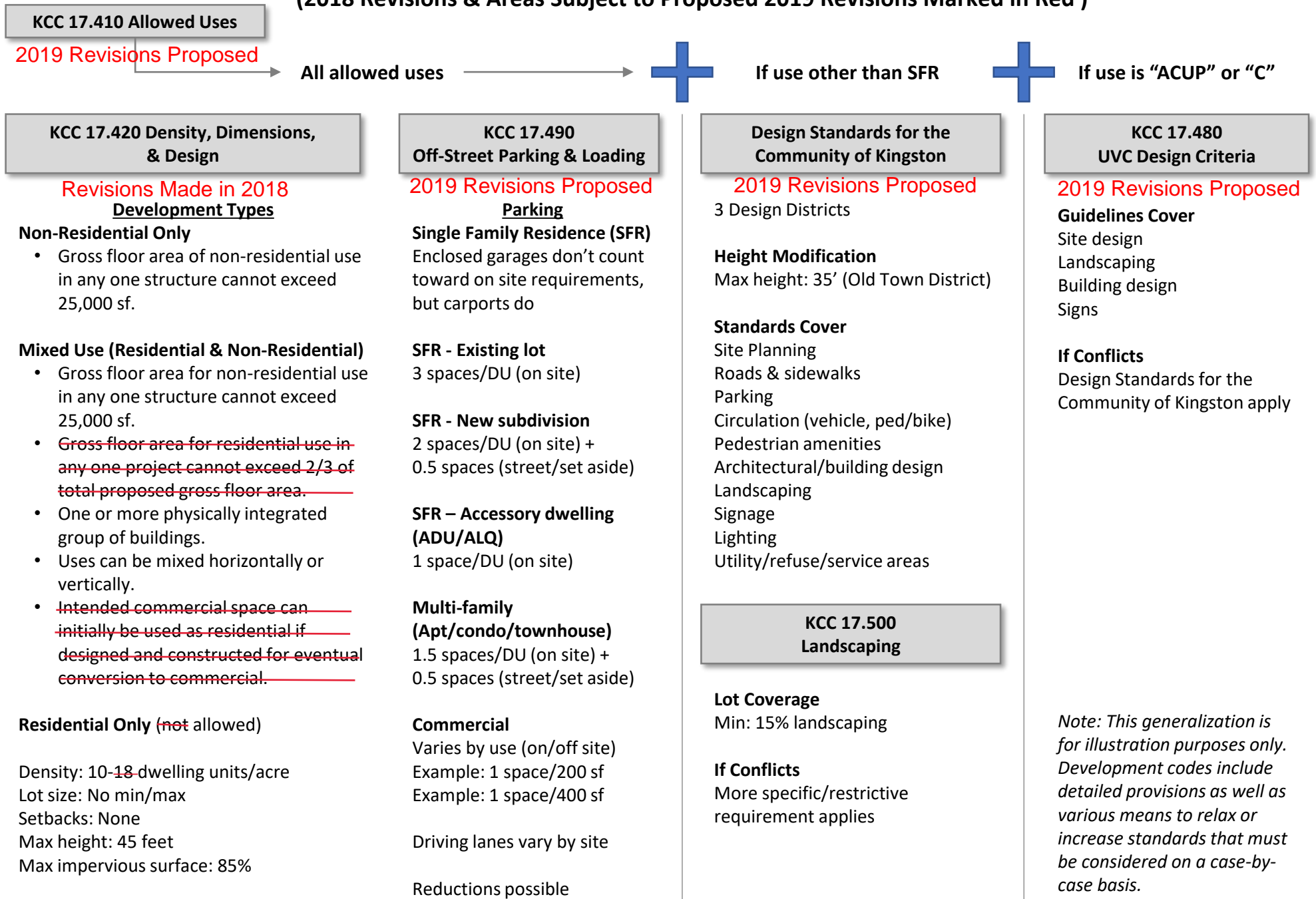
Potential Regulatory Barriers

- Parking requirements: on-site parking (located on the development site); off-site parking (located on a different property than the use it is serving)
- Remodels/additions and changing the use of existing structures (e.g. from residential to business) requires entire property to meet current development standards (instead of proportionally)
- Residential restrictions: residential-only development not allowed; accessory dwellings and detached SFR prohibited; unconventional housing options (i.e. boarding houses, bedrooms for rent)
- Minimum dwelling size (square feet of livable area) **Note: Outside the Workgroup's scope**
- Building heights: different height limits affect potential density; view corridor issues (consider 45 feet height in strategic locations)
- Allowed uses (particularly for a downtown commercial area)
- Redundant and conflicting design criteria
- UVC density (too restrictive; not consistent with adjacent commercial zones)
- Driveway separation distance
- Incentives for multi-family development (e.g. multi-family tax exemption)
- Landscaping
- Yard setbacks



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An Introduction: Principle Development Regulations Subject to the 2018 Urban Village Center (UVC) Review and Summary of Existing Development Standards (2018 Revisions & Areas Subject to Proposed 2019 Revisions Marked in Red)



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Annual Comprehensive Plan Amendment Process for 2018
Review of the Kingston Urban Village Center (UVC) Zone



Comparison of Allowed Uses by Community

Legend

-- = not permitted

P = permitted

ACUP = administrative conditional use permit

C = conditional use permit

	17.410 (Allowed Uses)	Urban Village Center Zone (30)(48) (57)(101)	Urban Medium Zone (30) (47) (48)(101)	Neighborhood Commercial Zone (19) (30) (48) (57)(101)	2010 UVC Update Not Adopted	Friday Harbor	La Conner	Steilacoom	Poulsbo	Clinton
Residential Uses										
100	Accessory dwelling units (1)	--	P	--	P	--	--	C	--	--
102	Accessory living quarters (1)	--	P	--	P	--	--	--	--	--
104	Accessory use or structure (1)(17)(51)	P	P	P	P	--	--	C	--	P
106	Adult family home	ACUP P (41)	ACUP P (41)	--	ACUP P (41)	--	--	--	--	--
108	Bed and breakfast house or vacation rental	ACUP C (34)	ACUP C (34)	ACUP C (34)	ACUP P (34)	--	--	P	P	--
109	Boarding house (102)	P (99)	P (99)(102)	P (99)	N/A	P	--	--	--	--
110	Caretaker's dwelling	ACUP	ACUP	ACUP	P	--	--	--	--	--
112	Convalescent home or congregate care facility (97)	ACUP	C	ACUP (79)	ACUP	--	CUP	P	C	--
114	Cottage housing developments	ACUP	ACUP	--	P	--	--	--	--	--
116	Dwelling, duplex	ACUP	P	--	P	--	--	--	--	--
118	Dwelling, existing	P	P	P	P	--	ACU P	C	P	P
120	Dwelling, multifamily	ACUP	P	P	ACUP P	P	C	--	P	C
122	Dwelling, single-family attached	P	P	P	P	--	ACU P	--	--	--
124	Dwelling, single-family detached	--	P (26)(43)	P	P	--	--	--	--	--

	17.410 (Allowed Uses)	Urban Village Center Zone (30)(48) (57)(101)	Urban Medium Zone (30)(47) (48)(101)	Neighborhood Commercial Zone (19) (30) (48) (57)(101)	2010 UVC Update Not Adopted	Friday Harbor	La Conner	Steilacoom	Poulsbo	Clinton
126	Guest house (1)	--	P	--	P	--	C	--	--	--
128	Home business (1)(53)	P	ACUP	ACUP	P	--	P	--	P	--
130	Hotel/motel	ACUP	--	C	P	--	P	P	AC UP	P
132	Mobile homes	-- (43)	C (24)(43)	--	-- (43)	--	--	--	--	--
134	Residential care facility	ACUP	P	--	ACUP	--	P	--	--	--
Commercial/Business Uses										
200	Accessory use or structure (1)(17)(51)	P	P	P	P	--	--	--	--	--
202	Adult entertainment (1)	--	--	--	--	--	--	C	--	--
204	Ambulance service	C	--	C	C	--	--	--	AC UP	
206	Auction house (55)	ACUP	--	--	ACUP	--	--	--	--	--
208	Auto parts and accessory stores (65)	--	--	P	P	--	--	--	--	--
210	Automobile rentals	P (56)	--	P (56)	P (56)	--	P	--	AC UP	--
212	Automobile repair and car washes (65)	--	--	ACUP (54)	ACUP (54)	C	--	C	--	--
214	Automobile service station (6)	--	--	ACUP	--	--	--	--	P	--
216	Automobile, recreational vehicle or boat sales	--	--	--	-- (Boat Sales ACUP/P)	--	P	P	P	--
218	Nonmotorized recreation rentals (95)	P	ACUP	P	N/A	--	--	--	--	--
220	Boat/marine supply stores	--	--	--	P	--	--	--	--	--
222	Brew pubs	ACUP	--	ACUP	ACUP	--	--	--	AC UP	--
224	Clinic, medical	ACUP	--	ACUP	ACUP	--	P	P	P/ AC UP	P
226	Conference center	P	--	--	P	--	--	--	--	--
228	Custom art and craft stores	P (54)	--	P (54)	P (54)	--	P	--	--	--
230	Day-care center (14)	P (54)	ACUP (37)	P (54)	P (54)	--	P	P	AC UP	P

	17.410 (Allowed Uses)	Urban Village Center Zone (30)(48) (57)(101)	Urban Medium Zone (30)(47) (48)(101)	Neighborhood Commercial Zone (19) (30) (48) (57)(101)	2010 UVC Update Not Adopted	Friday Harbor	La Conner	Steilacoom	Poulsbo	Clinton
232	Day-care center, family (14)	ACUP (54)	ACUP (37)	ACUP (54)	ACUP (54)	--	P	P	AC UP	P
234	Drinking establishments	ACUP	--	ACUP	ACUP	--	P	C	--	--
236	Engineering and construction offices	P (54)	--	P (54)	P (54)	--	--	--	P	--
238	Espresso stands (58) (72)	--	--	P	P	--	--	--	--	--
240	Equipment rentals	ACUP	--	--	ACUP	--	--	--	--	--
242	Farm and garden equipment and sales	--	--	--	ACUP	--	C	--	--	--
244	Financial, banking, mortgage and title institutions	P (54)	--	P (54)	P (54)	--	P	--	P	P
245	Fitness center	C	--	--	N/A	--	--	--	--	--
246	General office and management services – less than 4,000 s.f.	P	--	P	P	P	P	--	P/AC UP	--
248	General office and management services – 4,000 to 9,999 s.f.	ACUP	--	ACUP	ACUP	P	P	--	P/AC UP	--
250	General office and management services – 10,000 s.f. or greater	ACUP	--	--	ACUP	--	--	--	P/AC UP	--
252	General retail merchandise stores – less than 4,000 s.f.	P	--	P	P	P	P	P	P/AC UP	P/C
254	General retail merchandise stores – 4,000 to 9,999 s.f.	ACUP	--	ACUP	ACUP	P	P	P	P/AC UP	P/C
256	General retail merchandise stores – 10,000 to 15,000 s.f.	C	--	--	C	P	P	P	P/AC UP	P/C
258	General retail merchandise stores – 15,001 to 24,999 s.f.	C	--	--	C	P	P	P	P/AC UP	P/C
260	General retail merchandise stores – 25,000 s.f. or greater	--	--	--	--	P	P	P	P/AC UP	P/C
262	Kennels or pet day-cares (1)	--	--	C	C	--	--	--	P	--
264	Kennels, hobby	P	P	P	P	--	--	--	--	--

	17.410 (Allowed Uses)	Urban Village Center Zone (30)(48) (57)(101)	Urban Medium Zone (30)(47) (48)(101)	Neighborhood Commercial Zone (19) (30) (48) (57)(101)	2010 UVC Update Not Adopted	Friday Harbor	La Conner	Steilacoom	Poulsbo	Clinton
266	Laundromats and laundry services	P (54)	--	P (54)	P (54)	--	P	--	P	--
268	Lumber and bulky building material sales	--	--	--	--	--	P	--	--	--
270	Mobile home sales	--	--	--	--	--	--	--	--	--
272	Nursery, retail	ACUP	--	ACUP	ACUP	--	P	--	--	--
274	Nursery, wholesale	ACUP	--	ACUP	ACUP	--	--	--	--	--
276	Off-street private parking facilities	ACUP	--	ACUP	ACUP	P	--	C	AC UP	--
278	Personal services – skin care, massage, manicures, hairdresser/barber (66)	P (54)	--	P (54)	P (54)	--	--	--	P	--
280	Pet shop – retail and grooming	ACUP	--	ACUP	ACUP	--	--	--	--	--
282	Research laboratory	--	--	--	ACUP	--	--	--	AC UP	P
284	Restaurants	P (54)	--	P (54)	P (54)	--	P	P	P	P
286	Restaurants, high-turnover	ACUP	--	C	ACUP	--	P	C	--	--
288	Recreational vehicle rental	--	--	--	--	--	--	--	--	--
290	Temporary offices and model homes (27)	--	P	--	--	--	--	--	--	P
292	Tourism facilities, including outfitter and guide facilities	P	--	--	P	--	--	P	--	--
294	Tourism facilities, including seaplane and tour boat terminals	--	--	--	C	--	--	P	--	--
296	Transportation terminals	C	--	C	C	--	--	--	--	--
298	Veterinary clinics/animal hospitals	ACUP	--	ACUP	ACUP	--	C	C	AC UP	P
Recreational/Cultural Uses										
300	Accessory use or structure (1)(17)(51)	P	P	P	P	--	--	C	--	--
302	Amusement centers	C (11)	--	C	C (11)	--	--	--	--	--
304	Carnival or circus	ACUP (11)	--	C	ACUP (11)	--	--	--	--	--

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306	Club, civic or social (12)	ACUP	ACUP	ACUP	ACUP	--	P	P	P/C	--
308	Golf courses	ACUP	C	ACUP	ACUP	--		--	--	--
310	Marinas	C	C	ACUP	ACUP	P	P	P	C	--
312	Movie/performance theaters, indoor	P	--	ACUP	P	--	P	--	P/ AC UP	P
314	Movie/performance theaters, outdoor	ACUP	--	--	ACUP	--		--	--	
316	Museum, galleries, aquarium, historic or cultural exhibits (67)	P	--	ACUP	P	--	P	--	P	P
318	Parks and open space	P	P	P	P	P	P	--	P	--
320	Race track, major	--	--	--	--	--	--	--	--	--
322	Race track, minor	--	--	--	--	--	--	--	--	--
324	Recreational facilities, private	ACUP	C	ACUP	ACUP	P	P	--	AC UP	P
326	Recreational facilities, public	ACUP	P	ACUP	ACUP	P	P	--	AC UP	P
328	Recreational vehicle camping parks	--	--	C	--	--	P	--	--	--
330	Zoo	--	--	--	--	--	--	--	--	--
Institutional Uses										
400	Accessory use or structure (1)(17)(51)	P	P	P	P	--	--	--	--	--
402	Government/public structures	ACUP	ACUP	ACUP	ACUP	P	P	C	P	P
404	Hospital	C	--	--	C	--	P	P		P
406	Places of worship (12)	C	C	C	C	P	C	P	AC UP /C	P
408	Private or public schools (20)	C	C	C	C	--	P	--	C	P
410	Public facilities and electric power and natural gas utility facilities, substations, ferry terminals, and commuter park-and-ride lots (16)	ACUP	C	ACUP	ACUP	C	--	C	P/ AC UP	P/C
Industrial Uses										
500	Accessory use or structure (1)(17)(51)	P	P	P	P	--	--	--	--	--

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502	Air pilot training schools	P	--	--	P	--	--	--	--	--
504	Assembly and packaging operations	C	--	--	C	--	--	--	--	--
506	Boat yard	--	--	--	--	--	--	--	--	--
508	Cemeteries, mortuaries, and crematoriums (10)	C	C	C	C	--	--	--	--	--
510	Cold storage facilities (69)	--	--	--	--	--	--	--	--	--
512	Contractor's storage yard (21)	--	--	--	--	--	--	--	--	--
514	Food production, brewery or distillery	--	--	--	ACUP	--	P	--	P	--
516	Fuel distributors	--	--	--	--	--	--	--	--	--
518	Helicopter pads (13)	C	--	--	C	--	--	--	--	--
520	Manufacturing and fabrication, light	C	--	--	ACUP	--	C	P	--	--
522	Manufacturing and fabrication, medium	--	--	--	C	--	--	--	--	--
524	Manufacturing and fabrication, heavy	--	--	--	--	--	--	--	--	--
526	Manufacturing and fabrication, hazardous	--	--	--	--	--	--	--	--	--
528	Recycling centers	--	--	--	--	--	--	--	--	--
530	Rock crushing	--	--	--	--	--	--	--	--	--
532	Slaughterhouse or animal processing	--	--	--	--	--	--	--	--	--
534	Storage, hazardous materials	--	--	--	--	--	--	--	--	--
536	Storage, indoor	--	--	--	C	C	--	--	--	--
538	Storage, outdoor	--	--	--	--	--	--	C	--	P
540	Storage, self-service	C	C (37)(40)	C	C	--	--	--	--	P
542	Storage, vehicle and equipment (1)	--	--	--	--	--	--	--	--	--
544	Top soil production, stump grinding	--	--	--	--	--	--	--	--	--

	17.410 (Allowed Uses)	Urban Village Center Zone (30)(48) (57)(101)	Urban Medium Zone (30) (47) (48)(101)	Neighborhood Commercial Zone (19) (30) (48) (57)(101)	2010 UVC Update Not Adopted	Friday Harbor	La Conner	Steilacoom	Poulsbo	Clinton
546	Transshipment facilities, including docks, wharves, marine rails, cranes, and barge facilities	--	--	--	--	--	--	--	--	--
548	Uses necessary for airport operation such as runways, hangars, fuel storage facilities, control towers, etc. (13)	--	--	--	--	--	--	--	--	--
550	Warehousing and distribution (68)	--	--	--	--	--	--	--	--	P
552	Wrecking yards and junk yards (1)	--	--	--	--	--	--	--	--	--
Resource Land Uses										
600	Accessory use or structure (1)(17)(51)	P	P	P	P	--	--	--	--	--
602	Aggregate extractions sites	--	--	--	--	--	--	--	--	--
606	Aquaculture practices	C	C	C	C	--	--	--	--	--
608	Forestry	--	P	P	--	--	--	--	--	--
610	Shellfish/fish hatcheries and processing facilities	--	--	--	--	--	--	--	--	--

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Comparison of Parking Standards By Community

Link to Community Code	http://www.codepublishing.com/WA/KitsapCounty/	http://www.codepublishing.com/WA/Bainbridgelsland/	http://www.codepublishing.com/WA/FridayHarbor/	http://www.codepublishing.com/WA/LaConner/	http://www.townofsteilacoom.com/274/Municipal-Code	http://www.codepublishing.com/WA/Poulsbo/
Parking Section Reference	Chapter 17.490	Chapter 10.08 Chapter 18.15.020	Chapter 10.04 Chapter 17.68	Chapter 9.10 Chapter 15.90	Chapter 10.04 Section 18.20.070 Chapter 19.01	Chapter 10.12 Chapter 18.140
Standard	Kitsap County	Bainbridge Island	Friday Harbor	La Conner	Steilacoom	Poulsbo
Do garage stalls count toward the off-street parking requirement?	No	Yes	Yes	Yes	Yes	Yes
Number of spaces required by use						
Single-family	New lots - 2 stalls/dwelling unit + 0.5 on street; Historical lots - 3/du; + 1 Accessory Dwelling Unit	2 stalls/dwelling unit + 1 stall/accessory dwelling unit	2 stalls/dwelling unit + 1 stall/accessory dwelling unit	2 stalls/dwelling unit	2 stalls/dwelling unit + 1 stall/accessory dwelling unit	1 stall/studio or one bedroom dwelling unit; 2 stalls/two+ bedroom dwelling unit (varies by zone)
Multi-family	1.5 stalls/dwelling unit + 0.5 on-street or set aside	1 stall/studio & 1 bedroom or 2 stalls/dwelling unit with 2+ bedrooms	1.5 stalls/dwelling unit. 1 for ancillary in commercial.	2 stalls/dwelling unit	1 stall/bedroom (maximum of 2 stalls/dwelling unit)	1 stall/studio or one bedroom dwelling unit; 2 stalls/two+ bedroom dwelling unit (varies by zone)
Condo	1.5 stalls/dwelling unit + 0.5 on-street or set aside	2 stalls/dwelling unit + 1 stall/Accessory Dwelling Unit OR 1 stall/studio & 1 bedroom or 2 stalls/dwelling unit with 2+ bedrooms (varies by zone)	1.5 stalls/dwelling unit. 1 for ancillary in commercial.	2 stalls/dwelling unit	2 stalls/dwelling unit + 1 stall/accessory dwelling unit	1 stall/studio or one bedroom dwelling unit; 2 stalls/two+ bedroom dwelling unit (varies by zone)
Townhouse	1.5 stalls/dwelling unit + 0.5 on-street or set aside	2 stalls/dwelling unit + 1 stall/Accessory Dwelling Unit OR 1 stall/studio & 1 bedroom or 2 stalls/dwelling unit with 2+ bedrooms (varies by zone)	1.5 stalls/dwelling unit. 1 for ancillary in commercial.	2 stalls/dwelling unit	2 stalls/dwelling unit + 1 stall/accessory dwelling unit	1 stall/studio or one bedroom dwelling unit; 2 stalls/two+ bedroom dwelling unit (varies by zone)
Restaurant/Bar/Taverns	Under 5,000 square feet: 1 stall/200 square feet. Over 5,000 square feet: 20 stalls + 1 stall per additional 200 square feet	1 stall/4 occupants	1 stall/130 square feet of floor area	1 stall/200 square feet of floor area	1 stall/100 square feet of floor area + establishments with non-resident employees may be required to provide 1 stall/full-time equivalent (FTE).	1 stall/300 square feet of floor area (varies by zone)
Retail Stores - low traffic generation (appliance, furniture, hardware and repair stores)	1 stall/400 square feet	1 stall/200 square feet of floor area	1 stall/450 square feet of floor area	1 stall/400 square feet of floor area	1 stall/300 square feet of floor area + establishments with non-resident employees may be required to provide 1 stall/full-time equivalent (FTE)	1 stall/300 square feet of floor area (varies by zone)
Retail stores – high traffic generation (department stores, drug stores, fitness centers, supermarkets, ice cream parlors, bakeries, and beauty and barber shops)	1 stall/200 square feet	1 stall/250 square feet of floor area	Varies. 1 stall/200-500 square feet of floor area	1 stall/400 square feet of floor area	1 stall/400 square feet of floor area + establishments with non-resident employees may be required to provide 1 stall/full-time equivalent (FTE).	1 stall/300 square feet of floor area (varies by zone)
Professional office	1 stall/300 square feet	By Director	1 stall/300 square feet of floor area	1 stall/400 square feet of floor area	1 stall/400 square feet of floor area + establishments with non-resident employees may be required to provide 1 stall/full-time equivalent (FTE).	1 stall/300 square feet of floor area (varies by zone)

Comparison of Parking Standards By Community

Standard	Kitsap County	Bainbridge Island	Friday Harbor	La Conner	Steilacoom	Poulsbo
Medical and dental office or clinic	1 stall/200 square feet	By Director	1 stall/200 square feet of floor area	1 stall/400 square feet of floor area	1/400 + establishments with non-resident employees may be required to provide 1 stall/full-time equivalent (FTE).	1 stall/300 square feet of floor area
Bank and financial institutions	1 stall/400 square feet	By Director	1 stall/200 square feet of floor area	1 stall/400 square feet of floor area	1/400 + establishments with non-resident employees may be required to provide 1 stall/full-time equivalent (FTE).	1 stall/300 square feet of floor area (varies by zone)
Location of parking facility						
Is off-site parking (on the street or on a different property) allowed?	Yes, commercial only	Yes, for residential 1 stall must be provided on-site	Yes, all uses	Yes, up to 50% for commercial only	Varies	Yes, for multi-family and commercial only
What is the distance/proximity requirement for off-site parking (for parking located on a different property)?	300 feet from building or use	1,000 feet from building or use	200 feet from building or use	200 feet from building or use	200 feet from building or use	300 feet from building or use
What is the distance/proximity requirement for off-site parking (for parking located on-street in the public right-of-way)?	300 feet from building or use	1,000 feet from building or use	200 feet from building or use	200 feet from building or use	200 feet from building or use	N/A
Parking Incentives						
What administrative parking reduction incentives are provided?	By Director; up to 25% reduction	By Director, up to 50% when a common parking lot	The land use administrator may waive all or part of the on-site parking requirements upon written request by the applicant to pay a fee into the parking improvement	By Department, up to 50% via in-lieu parking fee of \$4,800 per space	Varies, By Town Administrator	15% reduction for non-residential uses. Additional reduction is possible if it meets additional criteria
What criteria is used for determining if a development qualifies for the parking reduction allowance?	Parking analysis/traffic impact study required to support parking reduction	Conditional Use Permit criteria	a. The extent to which the parking requirements which apply to the proposed development impose a particular hardship upon the applicant; b. Whether granting the request would be unreasonably burdensome to other property owners in the downtown core area; and c. Whether granting the request would lead to a better overall result than would strict adherence to the parking requirements of this chapter for the purposes of encouraging	Off-street parking requirements are reduced by two off-street parking spaces for each on-street parking space restored by said plan.	Excess parking within 200 feet, shared-parking facility within 200 feet, strict adherence will be detrimental to the character of the Historic District or historic property, or successful implementation of a commute trip reduction program.	Must have an executed joint parking agreement and parking study providing adequate justification
What is the process for granting the parking reduction allowance?	Variance over 10% increase or more than 25% reduction	Conditional Use Permit required	The Land Use Administrator	By Department	By Town Administrator	By review authority
What type of transit oriented development incentives are provided?	N/A	N/A	N/A	N/A	Allows unspecified reduction in the parking requirement.	N/A
What criteria is used for determining if a development qualifies for transit oriented development incentives?	N/A	N/A	N/A	N/A	Participation in commute trip reduction program	N/A



People OVER Parking

Planners are reevaluating parking requirements for affordable housing.

By JEFFREY SPIVAK

LIKE A LOT OF CITIES, Minneapolis has experienced the dual trends of rising multi-family rents and dwindling housing affordability. For years it offered the usual carrots of tax incentives and development subsidies for residential projects with affordable units. But three years ago, it tried a different strategy: The city slashed its multi-family parking requirements in certain parts of town.

The usual ratio of one parking space for every one unit was cut in half for larger apartment projects and was eliminated entirely for projects with 50 or fewer units located near high-frequency transit. Lo and behold, the market mostly responded in the exact ways planners had predicted.

Apartment developers proposed projects with fewer parking spaces. That lowered the cost of construction. So, such projects began offering rents below the market's established levels. New studio apartments, which typically went for \$1,200 per month, were being offered for less than \$1,000 per month.

"There's definitely a new type of residential unit in the market that we haven't seen much before," says

Nick Magrino, a Minneapolis planning commissioner who has researched apartment development trends since the parking code change. “Outside of downtown, there’s been a lot of infill development with cheaper, more affordable units.”

Tinkering with minimum parking requirements is not new. Cities have been fiddling with regulations for decades, sometimes raising them, sometimes lowering them, and sometimes giving variances for specific projects. What’s different now is an evolving understanding that urban lifestyles are changing, traditional parking ratios are outdated, and too much supply can be as harmful as too little.

So there’s a burgeoning movement of municipalities across the U.S. reducing or eliminating parking requirements for certain locales or certain types of development or even citywide.

“This would have seemed inconceivable just a few years ago,” says Donald Shoup, FAICP, a Distinguished Research Professor in UCLA’s Department

Carless in Seattle: Plymouth on First Hill’s apartments (opposite) are now home to some of the city’s formerly homeless disabled population. The mixed use transit-oriented development Artspace Mt. Baker Lofts (below) is located on the Central Link light-rail line. It has bicycle storage and a reserved car-share space, but no parking garage.

PHOTO OPPOSITE PAGE
COURTESY SMR ARCHITECTS
AND PLYMOUTH HOUSING
GROUP; THIS PAGE:
COURTESY SMR ARCHITECTS
AND ARTSPACE

of Urban Planning who has studied and written about parking policies for years and is considered the godfather of the current reform movement. (See an article based on his new book, *Parking and the City*, on page 33.)

Over the past three years, a Minnesota-based smart-growth advocacy organization called Strong Towns has compiled, through crowdsourcing, more than 130 examples of communities across the country addressing or discussing parking minimum reforms. And that list hasn’t captured all the cities taking actions.

Communities are reforming these regulations in a variety of ways.

Some have ditched parking minimums entirely. Buffalo, New York, in early 2017 became the first U.S. city to completely remove minimum parking requirements citywide, applied to developments of less than 5,000 square feet. Late last year Hartford, Connecticut, went a step further and eliminated parking minimums citywide for all residential developments.



Some have targeted their reforms to certain areas or development districts. Lexington, Kentucky, earlier this year scrapped parking requirements in a shopping center corridor to allow the development of new multifamily housing. Spokane, Washington, this past summer eliminated parking requirements for four-plus-unit housing projects in denser parts of the city.

Some have tied new policies specifically to spur affordable housing. Seattle this past spring eliminated parking requirements for all nonprofit affordable housing developments in the city, among other provisions. A couple of years ago, Portland, Oregon, waived parking requirements for new developments containing affordable housing near transit. Also in 2016, New York eliminated parking requirements for subsidized and senior housing in large swathes of the city well served by the subway.

Even some suburbs are doing it. Santa Monica, California, removed parking requirements entirely last year for new downtown developments as part of a new *Downtown Community Plan*. And this year, the Washington, D.C., suburban county Prince George's, Maryland, revised its zoning code to significantly reduce parking minimums.

"We're trying to create a new model of mobility and not emphasize the car as much as we've done in the past," says David Martin, Santa Monica's director of planning and community development.

Catalysts for change

Three primary factors are driving this new reform:

1. CITIES ALREADY HAVE MORE THAN ENOUGH PARKING. The Research Institute for Housing America, part of the Washington, D.C.-based Mortgage Bankers Association, used satellite imagery and tax records this year to tally parking space totals in different-sized U.S. cities, and determined that outside of New York City, the parking densities per acre far exceeded the population densities.

Meanwhile, two different groups—TransForm, which promotes walkable communities in California, and the Chicago-based Center for Neighborhood Technology, a nonprofit sustainable development advocacy group—have both conducted middle-of-the-night surveys of parking usage at apartment projects on the West Coast and in Chicago, respectively. They consistently found one-quarter to one-third of spaces sat empty. The Chicago center concluded "it is critical to 'right size' parking at a level below current public standards."

APA HOUSING INITIATIVE: PLANNING HOME

BY EMILY PASI

Planners know better than anyone the critical role that housing plays in our communities, and the severity of the U.S.'s housing affordability and availability crisis. Lack of housing choice and affordability hurts people and limits communities' prosperity. To this end, APA is actively working to develop new tools and better planning practices to encourage and deliver more and better housing options for all.

Earlier this year, APA's board of directors greenlit Planning Home, an organization-wide, multiyear housing initiative that aims to reshape the way planners, elected officials, decision makers, advocates, and the public use planning to address the nation's housing challenges.

Grounded in the philosophy that better tools can get communities the housing people need, APA's Planning Home action agenda is driven by six board-approved principles, which call on policy makers at all levels of government to:

- Modernize state planning laws
- Reform local codes
- Promote inclusive growth strategies
- Remove barriers to multifamily housing
- Turn NIMBY into YIMBY
- Rethink finance

Learn what you can do now to advance APA's Planning Home action agenda at PlanningHome.com.

Pasi is the public affairs manager at APA.

'We want to remove any barriers to the supply of housing options and affordability, and parking is one of them.'

—SAMUEL ASSEFA,
DIRECTOR,
SEATTLE'S OFFICE
OF PLANNING
AND COMMUNITY
DEVELOPMENT

2. TRANSPORTATION PREFERENCES ARE SHIFTING.

A variety of converging trends point to the possibility of fewer cars in the future. Fixed-rail transit lines continue to be developed in more urban centers, and millennials are not driving as much as previous generations. Meanwhile, transportation alternatives are proliferating, from passenger services such as Uber to car-sharing services such as Zipcar. Then there's the potential of driverless cars and the expansion of retail delivery services.

3. BOTTOM LINE: WE'RE GOING TO NEED MUCH LESS SPACE TO STORE CARS.

In fact, Green Street Advisors, a commercial real estate advisory firm, analyzed what it calls the "transportation revolution"—encompassing ride-hailing services, driverless cars, etc.—and estimated that U.S. parking needs

could decline by 50 percent or more in the next 30 years. (See “Future-Proof Parking, March: planning.org/planning/2018/mar/futureproofparking.)

“In the old days, you built an apartment and you expected it needed two cars,” says Doug Bibby, president of the National Multifamily Housing Council, an apartment trade association in Washington D.C. “Those parking ratios are outdated and no longer valid in any jurisdiction.”

Concerns about housing affordability

With the U.S. economy reasonably strong and most urban crime rates on a long-term decline, housing costs have increasingly emerged as a hot-button issue. In Boston University’s nationwide Menino Survey of Mayors last year, housing costs were cited as the number one reason residents move away, and more affordable housing was the top-ranked improvement mayors most wanted to see.

“It’s on the minds of mayors now more than it has been in the past,” says Kimble Ratliff, the National Multifamily Housing Council’s vice president of government affairs.

They’re concerned because there’s ample evidence of a continued national shortage of affordable housing. The latest “State of the Nation’s Housing” report from Harvard University’s Joint Center for Housing

MAPPING MINIMUMS
Strong Towns keeps track of progress on parking minimum removals across the U.S.
EXPLORE
<http://bit.ly/2C1t86k> 665600

Studies noted that a decade-long multifamily construction boom has increased total occupied rental units by 21 percent, but mainly at the top end of the market. Total units deemed “affordable”—costing less than 33 percent of median income—have remained basically static during the last decade, while the number of extremely low-income renter households has grown by more than 10 percent. The 2018 report concluded that there is a “tremendous pent-up demand for affordable rental housing.”

So as cities have searched for ways to generate more affordable housing, parking has emerged as an easy target. Parking ratios are simple to change, and the process doesn’t lead to future cost obligations like subsidies do.

That was the approach taken by Seattle this year. “The number one issue facing our city is the lack of housing options and affordability. We’re looking to remove any barriers to the supply of housing, and parking is one of them,” says Samuel Assefa, the director of Seattle’s Office of Planning and Community Development.

Impacts on housing costs

Planners’ shifting strategies toward parking are now supported by a growing body of evidence that parking requirements negatively impact multifamily housing, especially affordable projects.

In a nutshell, building parking costs a lot, and that cost usually ends up raising tenant rents.

Various studies indicate that surface parking lot spaces cost upwards of \$5,000 each, while above-ground parking garages average around \$25,000 per space and below-ground garages average around \$35,000 per space. That can translate into higher rent, particularly in big cities. Two UCLA urban planning professors studied U.S. rental data and reported in the journal *Housing Policy Debate* in 2016 that garage parking typically costs renter households approximately \$142 per month, or an additional 17 percent of a housing unit’s rent. Other studies have found even larger impacts on rents.

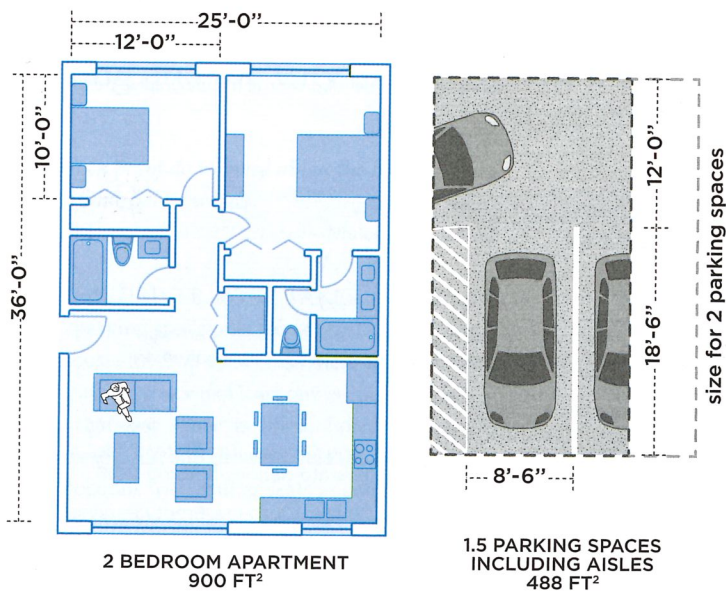
“That can be a significant burden on lower-income households,” says David Garcia, policy director of the Turner Center for Housing Innovation at the University of California–Berkeley.

Changing that equation can help produce additional affordable housing. That’s a scenario actually playing out in Portland, Oregon.

In 2016 the Portland Community Reinvestment Initiatives, a nonprofit developer and manager of low-income housing, began planning a 35-unit

LIVING SPACE VERSUS PARKING SPACE

The typical median parking required for a two-bedroom apartment in many large North American cities is more than half the size of the apartment itself.



SOURCE: SETH GOODMAN, GRAPHINGPARKING.COM

senior housing project called Kafoury Court. At the time, Portland's code required providing five parking spaces for the project, and the developer was struggling to find financing. But late that year, the city changed its parking requirements, and Kafoury now only needs to provide two spaces.

While that change doesn't seem like much, it allowed the development to be totally redesigned. A first-floor parking garage was no longer needed, so the building has been scaled back from five stories to four stories, which led to cost-saving ripple effects. "This has made the project financially feasible," says PCRI's Julia Metz.

She adds: "We prefer to build houses for people, not cars. When it comes down to choosing space for people or parking, we're going to choose people."

Affordable housing projects, with their lower rent revenue streams, are already challenging to finance. So parking is an increasingly key factor in whether or not a project works financially. But to developers, reducing or removing parking requirements does not mean eliminating parking supply. It simply allows developers to decide how many spaces to build based on market and locational demand.

"I've had developers say to me, 'Hey, I could make this deal work if I only had to build a garage that's one-third smaller,'" says Greg Willett, chief economist of RealPage, a provider of property management software and services. "Any way you can take costs out of the deal is meaningful."

'The debate is now won'

When it comes to utilizing parking to augment planning and development policies, U.S. cities still have a long way to go to catch up to some European counterparts. Zurich, Switzerland; Copenhagen, Denmark; and Hamburg, Germany, have all capped the total number of allowable parking spaces in their cities. Oslo, Norway—where a majority of center-city residents don't own cars—is pursuing plans to remove all parking spaces from that district, to be replaced by installations such as pocket parks and phone-charging street furniture.

And last year the largest city in North America, Mexico City, eliminated parking requirements for new developments citywide and instead imposed limits on the number of new spaces allowed, depending on the type and size of building.

In the U.S., however, parking is still sacred in many places. Sometimes when parking reductions are proposed for a certain urban district or a specific new development, nearby residents complain it will

BUILDING PARKING RAISES RENT

Parking costs a lot to build, and that cost usually ends up raising tenant rents.

\$5,000

Cost per surface space

\$25,000

Cost per above-ground garage space

\$35,000

Cost per below-ground garage space

\$142

The typical cost renters pay per month for parking

+17%

Additional cost of a unit's rent attributed to parking

SOURCE: HOUSING POLICY DEBATE, 2016

force new renters to park on their residential streets. Because so many people still own cars, the National Multifamily Housing Council's *2017 Kingsley Renter Preferences Report* ranked parking as renters' second-most desired community amenity, behind only cell-phone reception.

Not surprisingly, then, some places are still demanding more parking, not less. In Boston, for instance, an influx of new residents clamoring for parking in the booming South Boston neighborhood led to zoning code changes in 2016 that require developers to build two-thirds more off-street parking than before.

Nevertheless, the movement to reduce parking is now widespread, involving big cities and small towns, urban districts and suburban locales, affordable housing and market-rate units. "It's pretty well accepted now that reforming parking minimums is a good way to manage cities," says Tony Jordan, founder of Portlanders for Parking Reform, which has advocated for better parking policies. "The debate is now won."

The lessons for planners are, first, to be open to adjusting parking policies in zoning codes and comprehensive plans and, second, to be flexible in crafting new parking limits depending on the location or desired outcome, such as spurring affordable housing development.

"As we update our policies, we as planners need to learn from the past and adjust," says Seattle planning director Assefa. "We constantly need to tweak our policies and face the challenges of what's not necessarily working. More often than not, there's significant space dedicated to the car that is not utilized." ■

Jeffrey Spivak, a market research director in suburban Kansas City, Missouri, is an award-winning writer specializing in real estate planning, development, and demographic trends.

RESOURCES

FROM APA

APA Knowledgebase Collection, "Rethinking Off-Street Parking Requirements": planning.org/knowledgebase/parkingrequirements.

MORE

Harvard University Joint Center for Housing Studies' *The State of the Nation's Housing 2018*: hjchs.harvard.edu/state-nations-housing-2018.

Center for Neighborhood Technology, "Stalled Out: How Empty Parking Spaces Diminish Neighborhood Affordability:" <http://bit.ly/2Mr0bES>.

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Kingston Stakeholders

Parking Committee

2018

Committee Members:

Laura Gronnvoll, Dave Wetter, Ginny Carosso, Jerry Kirschner, Jim Pivarnik

Mission:

To investigate long range needs for the community with regards to parking. This will include parking strategies for existing and future impacts of fast ferry and additional community development.

History:

A combination of small lots in Kingston's Urban Village Center (UVC) zone and Kitsap County parking requirements severely inhibits development within this area. As a result, an early project of the Kingston Stakeholders was to survey the existing parking spaces to determine options for off-site parking that could possibly supplement on-site parking requirements.

The first study was done Peter Brachvogel of BC&J in 2007 (attached). Peter also participated in our Kingston Downtown Master Planning project with then County Planner Peter Sullivan. As part of that project, Peter prepared a drawing entitled "OFF STREET PARKING REQUIREMENTS per Kitsap County Code / Proposed changes. The proposed changes incorporated current parking recommendations from the College of Urban Design.

In 2008, the stakeholders did a supplemental survey looking at 2 hour parking.

Some conclusions from these surveys were:

- 1) Kingston has many undeveloped 60' wide right of ways that could be better utilized for on-street parking which could supplement on-site parking requirements.
- 2) We have enough parking for commuters. But, they might argue that there is not enough close in "FREE" parking.
- 3) We need to do a better job of optimizing and enforcing 2 hour free parking zones to support our local business needs.
- 4) As a WSF landing site, our town needs will more likely develop similar to Winslow than they will develop like Silverdale which has the same Kitsap County parking requirements as Kingston.
- 5) To support UGA planned development in the UVC, we need to incorporate the above recommendations from the 2007 study drawing entitled: "OFF STREET PARKING REQUIREMENTS per Kitsap County Code / Proposed changes.

The Complete Streets project study was done in 2015 and published on 3-7-16 (parking section attached). This study also concludes that there is adequate parking. That additional capacity could be possibly be utilized in various ways:

- 1) Supplement on-site requirements of potential development.
- 2) Remove some for improved pedestrian connections (i.e. Washington Street Park development).
- 3) Supplement passenger only ferry requirements.
- 4) Some combination of the above.

Next Steps:

Data Collection Phase:

Ginny Carosso: Parking survey of existing spaces and rates.

- Kingston wide existing parking survey including:
 - Locations
 - Fee amount
 - Daily/monthly
 - # of spaces
 - Percentage utilized on mid-weekday
 - Photos and map

Jerry Kirschner: To contact Peter Best at Kitsap County for related studies.

Laura Gronnvoll: Survey businesses on needs and availability.

Draft Questions to ask Local Businesses (RE: parking)

1. How many available parking spaces does your business currently have?
2. Are you satisfied with your current number of spaces?
3. Do you have extra parking that is not being utilized?
4. Do you have people using your businesses parking spaces that are not patronizing your business?
5. What do you see as possible solutions for your current issues with parking?
6. Does your business share a common parking area with other businesses?

--Preliminary--

1-08-08

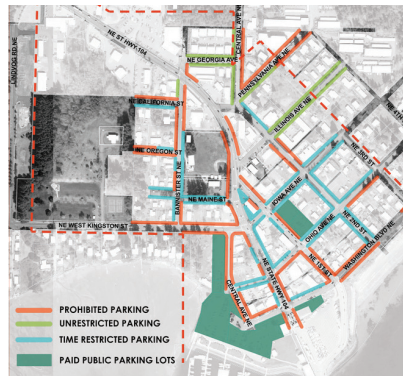
Kingston Downtown Parking Spaces

<u>Location</u>	<u>Private</u>	<u>Paid</u>	<u>Public</u>	<u>Total</u>
Marina *	121	152	62 (2hr)	335
Community Center **	---	---	34	34
WSF lot on 1 st	---	74	---	74
Holding Lane Pub	29	---	6 (2 hr)	35
Ohio (104 to 1 st)	---	---	14 (2hr)	14
Lot (104 & Ohio NW corner)	12	---	---	12
Ohio & 1 st (SE corner)	8	---	---	8
Rick's buildings	28	---	---	28
Rick's lot & alley	---	31	2	33
Alley & 1 st (SE corner)	8	---	---	8
Kingston Inn Property	---	15	---	15
Washington (104 to 1 st)	6	---	28 (2hr)	34
Washington & 104 (SE corner)	---	13	---	13
Washington (104 to Marina)	---	---	19 (2hr)	19
Behind Ale House	18	25	---	43
Ohio (104 to Marina)	---	---	4	4
Lot at Ohio & 104 (SW corner)	---	22	---	22
Lot at Central & Ohio (NW corner)	---	27	---	27
Totals	230	359	169	758
Percent of Total	30 %	48 %	22 %	100%

* = Marina has 44 % of all the above parking.

* = Parking restricted to Community Center activities.

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PARKING

Parking in the project Study Area is accommodated by a combination of public on-street spaces, paid public lots, and privately-owned lots. In the downtown area, on-street parking has a two-hour time limit to discourage long-term commuter parking and ensure that spaces are available for customers of downtown businesses. The majority of private lots provide free customer parking for adjacent businesses, although some are paid lots that appear to accommodate long-term commuter parking. Paid public lots in the Study Area accommodate both commuter and short-term parking. The largest public lot is located on the Port of Kingston property. There is also a WSDOT commuter lot at NE 1st Street and Ohio Avenue NE. The smallest public lot is located along Washington Blvd NE, between Main Street and NE 1st Street.

An inventory of public parking was completed, which included a count of on-street spaces as well as spaces in public lots. A detailed assessment of public parking utilization was conducted on four different occasions: off-peak season at midday, peak season midday, peak season during Saturday Farmer’s

Market, and peak season during a Saturday Concert. The four events chosen for parking counts reflect a range of recurring conditions affecting parking utilization. Midday use during off-peak season represents parking demand for local residents and commuters, midday use during peak season reflects local, commuter and tourist parking demand, while the Farmer’s Market and Saturday Concert are recurring high traffic events which mark the peak parking demand driven by both local residents and tourists.

An informal inventory of private parking was completed, which included a count of spaces located on private businesses but not on private residential property. This additional information on private business parking stall counts was at the request of the Kingston Complete Streets Citizen’s Advisory Committee and only for purposes of informing the relative number of total available parking stalls in the community. For additional information on the details and results of the parking study, refer to Appendix B.2. Transportation.

LEFT: Disorganized parking on Washington Blvd NE

MIDDLE: Parking Study Map

RIGHT: Port of Kingston parking lots

2.6. Freight

WSDOT has established the Washington State *Freight and Goods Transportation System* (FGTS) to classify state highways, county roads, and city streets according to their annual freight tonnage.⁹ There are five freight categories, ranging from T-1 to T-5, depending on the annual tonnage of freight that they carry, with T-1 carrying the highest amount (>10 million tons per year) and T-5 carrying the lowest amount (<100,000 tons per year). WSDOT identifies T-1 and T-2 facilities as statewide freight economic corridors.

In the project study area, SR 104 (mainline and couplet) and NE West Kingston Road are classified as T-3 roadways, carrying between 300 thousand and 4 million tons of freight per year. Although the Kingston streets carry less than a T-2 level of freight tonnage, they serve as a primary conduit for the delivery of freight and goods to north Kitsap County from the east Puget Sound area. The vehicle counts conducted on SR 104 indicated that heavy trucks make up about 6 to 8% of daily traffic through the study area.

2.7. Parking

Parking in the project study area is accommodated by a combination of public on-street spaces, paid public lots, and privately-owned lots, shown on Figure 7. In the downtown area, on-street parking has a two-hour time limit to discourage long-term commuter parking and ensure that spaces are available for customers of downtown businesses. The majority of private lots provide free customer parking for adjacent businesses, although some are paid lots that appear to accommodate long-term commuter parking. Paid public lots in the study area accommodate both commuter and short-term parking. The largest public lot is located on the Port of Kingston property in the southwest corner of the study area. There is also a WSDOT-owned lot at 1st Avenue SE and Ohio Avenue NE. The smallest public lot is located along Washington Boulevard NE, between Main Street and 1st Avenue NE.

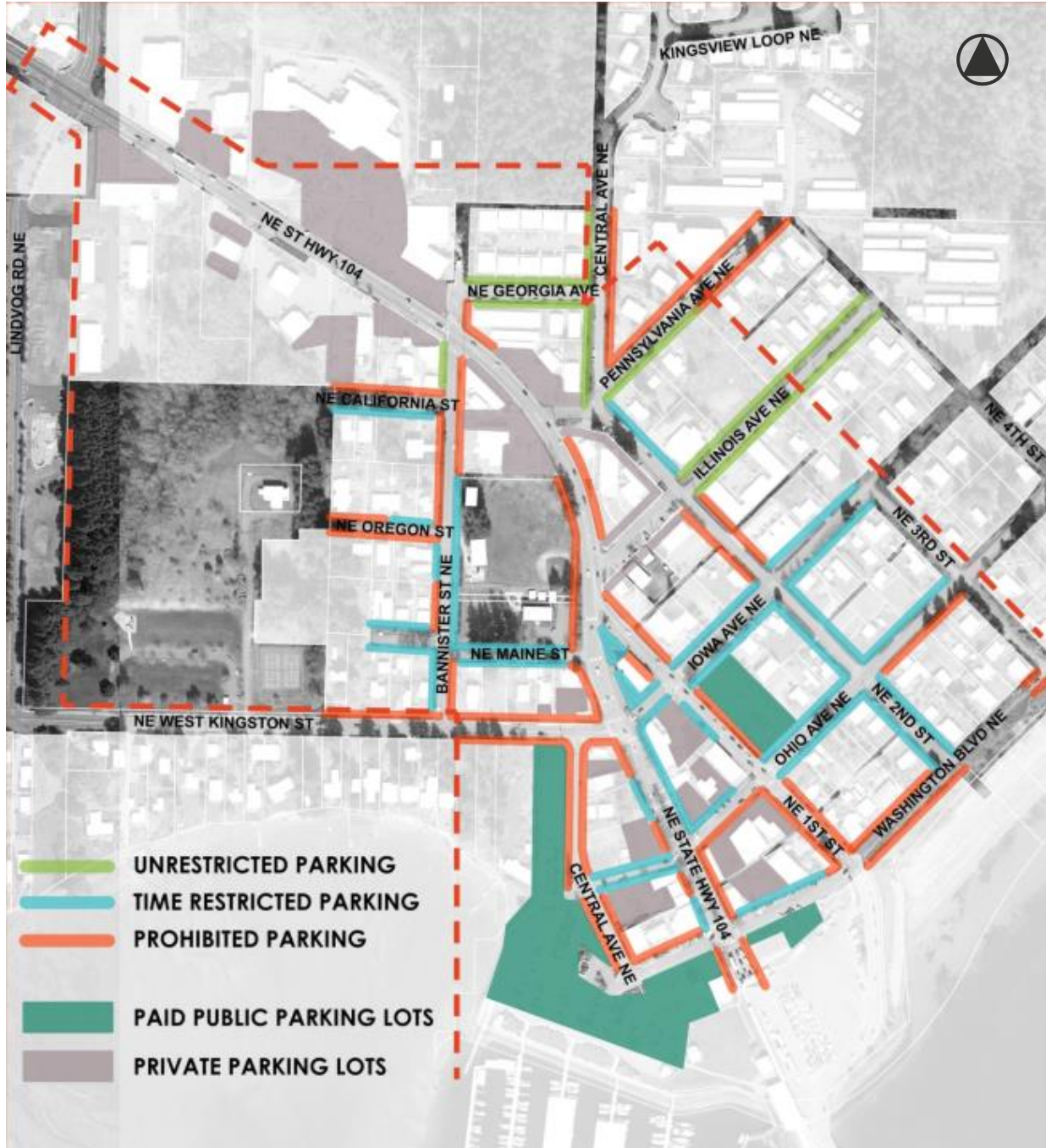
A detailed assessment of public parking utilization was conducted for conditions during peak and off-peak times of year. An inventory of public parking was completed, which included a count of on-street spaces as well as spaces in public lots. The parallel parking supply was estimated using guidelines provided in the City of Seattle's *TIP 117*,¹⁰ which includes conversion rates for the unobstructed distance along which parking is available to an estimated number of parking spaces. This document was used because it is the best available resource for estimating parallel parking supply based on available curb length. It should be noted that these estimates are approximate and are intended only to provide an order-of-magnitude estimate; the actual number of cars that can park along a given length will depend on the size of the cars parked and the space between them.

The off-peak count was conducted on a typical weekday in February, reflecting conditions when tourist and recreational activity is low and the majority of parking demand is generated by regular ferry commuters, local residents, and employees and customers of local businesses. Peak season counts were conducted in July, and reflect conditions with high tourist and recreational activity. Peak season counts were conducted on a typical weekday, as well as during two regular summer Saturday events held in downtown Kingston—the Farmer's Market and evening concert. Table 2 summarizes the utilization of public parking in the study area during each of these periods.

⁹ Washington State Department of Transportation (WSDOT), 2014. Washington State Freight Mobility Plan, October.

¹⁰ City of Seattle, Department of Planning Development, May 12, 2011.

Figure 7. Existing Parking Characteristics



Source: MacLeod Reckord, Heffron Transportation, 2015.

Table 2. Existing Public Parking Supply and Utilization

Location	Capacity (spaces)	Off-peak Season Weekday Midday ¹		Peak Season Weekday Midday ²		Saturday Farmer's Market ²		Saturday Evening Concert ²	
		Vehicles Parked	Utilization	Vehicles Parked	Utilization	Vehicles Parked	Utilization	Vehicles Parked	Utilization
Public Pay Lots									
WSDOT 1 st Avenue Lot	72	25	35%	34	47%	45	63%	42	58%
Port of Kingston Lot	328	122	37%	166	51%	301	92%	275	84%
Washington Blvd Lot	32	19	59%	22	69%	20	63%	19	59%
On-Street Parking									
Time restricted	204	27	13%	20	10%	31	15%	36	18%
Unrestricted	90	27	30%	30	33%	27	30%	27	30%
Illegally parked	---	1	---	0	---	0	---	0	---
Total	726	221	30%	272	37%	424	58%	399	55%

Source: Heffron Transportation, 2015

1. Off-peak season weekday count was conducted on Thursday, February 12, 2015.
2. Peak season Saturday counts were conducted on July 18, 2015, and the weekday count on Thursday, July 23, 2015

The table shows that overall, utilization in the study area ranged from 30% during off-peak season midday to 58% during the Saturday Farmer's Market. Overall, increases in demand were reflected to the greatest degree in the Port of Kingston lot, which more than doubled during the Farmers' Market, when utilization at the Port's lot reached 92%. This is to be expected since this lot, which is the largest in the area, is also located near Mike Wallace Park where the events are held. However, the parking utilization results also are consistent with the observations of local business owners that higher levels of activity along the waterfront do not necessarily translate to higher levels of activity in the downtown area. The parking utilization results indicated that on-street parking was 43% utilized during both off-peak and peak season weekday conditions, increasing slightly to 48% during the Saturday events. However, it is also noted that utilization of the WSDOT lot, located across downtown from Mike Wallace Park, also showed increased use during the Saturday events. Utilization of the Washington Boulevard lot did not change significantly during the Saturday events.

The parking study results show that while demand does increase during events and periods of high activity, there is excess parking capacity that is not being used even during the high demand periods, particularly on-street. These results indicate that there is capacity available for potential complete streets improvements that would remove some on-street parking without significantly affecting overall parking conditions in the area. Additionally, improved pedestrian connections between the waterfront area and the downtown core could help make downtown parking feel more accessible for participants in waterfront events.

4.5. Parking

Buildout of all project elements is expected to remove parking spaces, and/or require parking reconfiguration, at the following locations:

- Up to 5 spaces would be affected at the north end of Bannister Street NE, on the west side.
- Up to 15 spaces would be affected at the south side of SR 104, between Bannister Street NE and NE 2nd Street.
- Up to 21 spaces would be affected along Iowa Avenue NE.

This totals up to 41 parking spaces potentially removed from the study area as a result of buildout of the Complete Streets Project, reducing the total capacity in the study area. It is expected that drivers who would park in these spaces would instead park in other available spaces nearby. Table 7 shows the effect that this would have on parking utilization during the highest demand period observed in the parking study. The table shows that with removal of these parking spaces, peak parking utilization would increase from 58% to 62%, but the area would still have an excess capacity of more than 250 spaces. Parking efficiency could potentially be improved through provision of signage that clearly identifies long-term and short-term parking options in the area. There may also be opportunity to lower the total number of parking spaces removed through reconfiguration of other parking near the affected areas.

Table 7. Public Parking Supply and Peak Utilization – With and Without Project

Location	Without Project (Existing)			With Project Buildout		
	Capacity (spaces)	Peak Demand ¹	Utilization	Capacity (spaces)	Peak Demand	Utilization
Public Pay Lots						
WSDOT 1 st Avenue Lot	72	45	63%	72	45	63%
Port of Kingston Lot	328	301	92%	328	301	92%
Washington Blvd Lot	32	20	63%	32 ¹	20	63%
On-Street Parking						
Time restricted	204	31	15%	183	31	17%
Unrestricted	90	27	30%	70	27	39%
Total	726	424	58%	685	424	62%

Source: Heffron Transportation, MacLeod Reckord, 2016.

1. Capacity at Washington Boulevard could slightly increase or decrease with the proposed reconfiguration, but is not expected to substantially change public parking capacity.

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Retail sales projections

Washington State Department of Revenue (DOR) collects retail sales taxes on all retail sales conducted in Washington State. DOR correlates retail sales by the North American Industrial Classification System (NAICS) defining sales by 6 digit industry classifications able to distinguish sales in convenience stores, for example, from supermarkets, meat markets, fish and seafood markets, fruit and vegetable markets, baked good stores, and all other specialty food stores. DOR collects but does not report sales per store classification when the number of stores is so low that reporting would disclose the sales for any particular store.

DOR collects retail sales data by municipal jurisdiction since the retail sales tax is shared between state and local jurisdictions. In Kitsap County, DOR data is available for 2014 retail sales for Bainbridge Island, Poulsbo, Bremerton, Port Orchard, and by inference unincorporated Kitsap County.

Urban Land Institute's (ULI) publication Dollars & Cents of Shopping Centers correlates retail sales per square foot of different types of stores located in strip, specialty, neighborhood, community, and regional shopping centers and districts. Except for some specialty centers, the stores are franchises located in newer buildings with efficient stocking and manageable checkout counters. Older buildings occupied by non-franchise stores will likely not perform at as high square foot sales as ULI's data indicates.

Kingston estimates - were derived for Kingston's CCD for 2013 and 2025 by correlating ACS population totals and projections with the retail sales per capita estimates for Bainbridge Island (the closest comparable developed retail area) and the retail sales per square foot for specialty and neighborhood shopping centers (the closest store type comparable).

The projected 2013 sales per store were subtracted from the 2025 projected retail sales to derive the additional square footage

sales potential that population growth will create between the time periods.

Retail square footage available

Convenience	2013	2025	13-25
Supermarket	31,959	35,283	3,324
Hardware	18,618	20,555	1,936
Nursery/garden	17,925	19,789	1,864
Snack/beverage	6,935	7,656	721
Drug/pharmacy	6,544	7,224	681
Pet/supplies	5,085	5,614	529
Comparable	2013	2025	13-25
Sporting goods	11,435	12,624	1,183
Family clothing	11,379	12,562	1,183
Furniture	10,540	11,637	1,096
Shoe stores	5,289	5,840	550
Book stores	4,924	5,436	512
Sewing/needle	4,804	5,303	500
Women clothes	4,269	4,713	444
Specialty foods	3,845	4,245	400
Office supplies	3,694	4,078	384
Hobby/toy	2,222	2,454	231
Florists	2,207	2,437	230
Specialty	2013	2025	13-25
Restaurants - full service	39,202	43,279	4,077
Restaurants - limited service	15,436	17,042	1,605
Art dealers	3,056	3,373	318

Sources: DOR, OFM, ULI

- **Convenience goods stores** - (supermarket, hardware, nursery, drug) generate the largest potential square footage currently and by 2025 with the potential square footage increases to compensate for local resident consumer population growth.

A significant portion of this potential, however, has been claimed with the Albertsons, Sav-On and Rite Aid Pharmacy developments west of Kingston at the intersection of State Highway 104 and Hansville Road, and Thriftway Center in Kingston proper.

- **Comparable goods stores** - (family and women's clothing, furniture, shoes, office supplies) generate significant potential square footage now and through 2025.

Very little of this market has been recruited currently within Kingston proper or the Hansville intersection for local consumer populations.

- **Specialty or destination goods stores** - (restaurants, sporting goods, drinking places, pets, books, sewing, art, hobby) generate notable square footage potential particularly if these stores are not currently resident within the local market.

Kingston proper has recruited a significant number of limited and full-service restaurants and drinking establishments oriented to local, commuter, and tourist consumers. While this segment may be close to current saturation it is a constantly changing, fluid market.

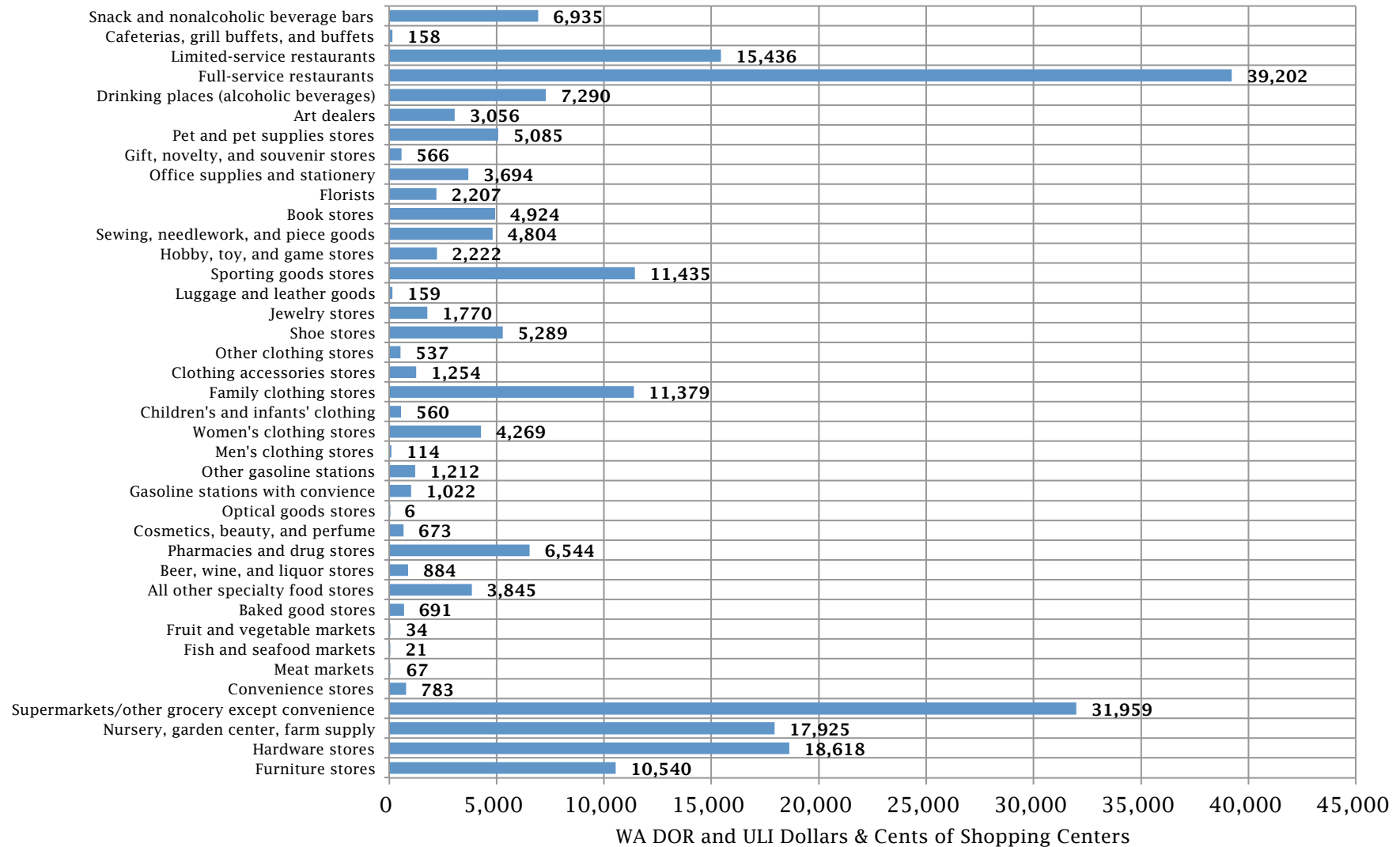
Kingston has not recruited, however, other residual specialty stores including sporting goods, pets, books, sewing, art, and hobby of local resident, commuter, and tourist consumer potential.

Note - the above comments assume existing stores have captured or serve the current market well with good products, service, prices, displays, operating hours, invested ownership and management, and other features.

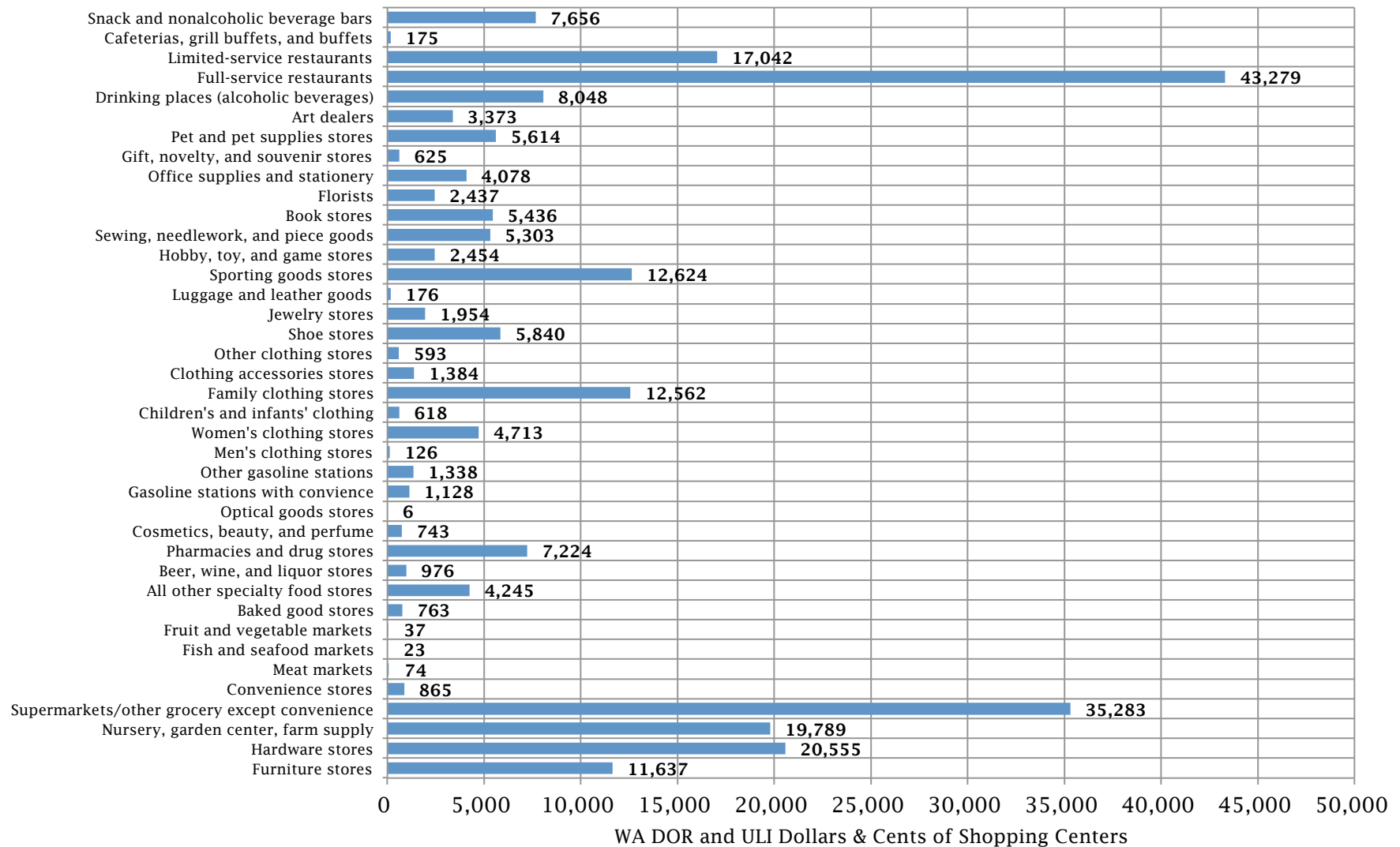
In actuality, retail businesses have a high turnover rate where these characteristics may not be well provided, market conditions and preferences change as due economic impacts on consumer behavior, and ownership and managements evolve. The sales and square footage estimates represent a benchmark to be maintained through the natural business evolutions which affect all retail centers.

Also note - the per capita sales averages for Bainbridge Island include sales to local residents as well as commuters and tourists. These averages should be typical of most Kingston sales to local residents, commuters, and tourists as well for most of the store lines indicated. However, the projections do not include potential hotel, motel, bed-and-breakfast consumption nor sales related to weddings, corporate retreats, or conferences.

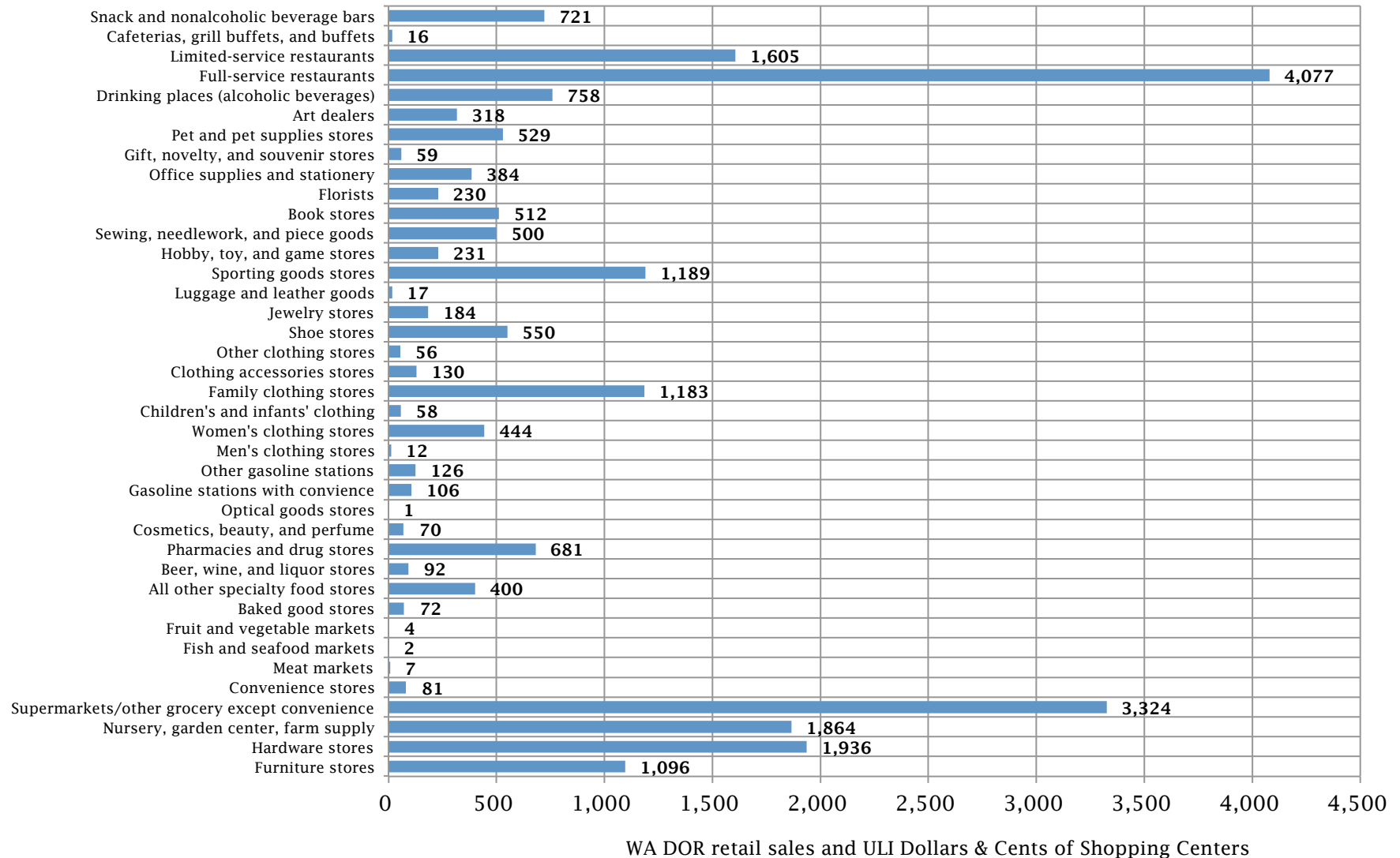
Retail square footage available in Kingston CCD 2013



Retail square footage available in Kingston CCD 2025



Additional sq footage generated Kingston CDD 2025



Kingston retail sales potentials 2013-2025 by NAICS Industry

NAICS	Per capita retail sales				Retail sales Kingston CCD		Retail sf available			2025 net sf available	
	Washington State	Unincorporated Kitsap	Bainbridge Island	Kingston	2013 12,483	2025 13,781	Sales sq ft	2013 12,483	2025 31,292		
442110	Furniture stores	\$161.56	\$83.54	\$124.42	\$124.42	\$1,553,165.65	\$1,714,695	\$147.35	10,540	11,637	1,096
444130	Hardware stores	\$104.95	\$15.02	\$213.73	\$213.73	\$2,668,020.82	\$2,945,495	\$143.30	18,618	20,555	1,936
444220	Nursery, garden center, farm supply	\$59.97	\$27.25	\$205.77	\$205.77	\$2,568,649.36	\$2,835,789	\$143.30	17,925	19,789	1,864
445110	Supermarkets/other grocery except convenience	\$401.58	\$188.93	\$888.65	\$888.65	\$11,093,008.53	\$12,246,681	\$347.10	31,959	35,283	3,324
445120	Convenience stores	\$66.36	\$21.78	d	\$21.78	\$271,879.74	\$300,155	\$347.10	783	865	81
445210	Meat markets	\$1.86	d	d	\$1.86	\$23,218.38	\$25,633	\$347.10	67	74	7
445220	Fish and seafood markets	\$0.59	d	d	\$0.59	\$7,364.97	\$8,131	\$347.10	21	23	2
445230	Fruit and vegetable markets	\$1.51	\$0.12	\$0.94	\$0.94	\$11,734.77	\$12,955	\$347.10	34	37	4
445291	Baked good stores	\$10.49	\$19.22	d	\$19.22	\$239,923.26	\$264,875	\$347.10	691	763	72
445299	All other specialty food stores	\$8.13	\$0.17	\$32.65	\$32.65	\$407,582.06	\$449,971	\$106.00	3,845	4,245	400
445310	Beer, wine, and liquor stores	\$24.58	\$12.44	\$17.99	\$17.99	\$224,629.94	\$247,991	\$254.10	884	976	92
446110	Pharmacies and drug stores	\$129.77	\$61.84	\$214.08	\$214.08	\$2,672,372.91	\$2,950,300	\$408.40	6,544	7,224	681
446120	Cosmetics, beauty, and perfume	\$34.82	\$17.95	\$14.82	\$14.82	\$184,998.71	\$204,239	\$275.00	673	743	70
446130	Optical goods stores	\$2.61	\$1.79	\$0.07	\$0.07	\$882.80	\$975	\$159.18	6	6	1
447110	Gasoline stations with convenience	\$186.33	\$106.66	d	\$106.60	\$1,330,687.80	\$1,469,079	\$1,302.00	1,022	1,128	106
447190	Other gasoline stations	\$44.57	\$47.55	\$126.42	\$126.42	\$1,578,137.57	\$1,742,264	\$1,302.00	1,212	1,338	126
448110	Men's clothing stores	\$13.36	\$12.67	\$1.54	\$1.54	\$19,201.43	\$21,198	\$167.96	114	126	12
448120	Women's clothing stores	\$49.23	\$23.86	\$57.44	\$57.44	\$716,963.45	\$791,528	\$167.96	4,269	4,713	444
448130	Children's and infants' clothing	\$12.40	\$6.32	\$7.53	\$7.53	\$94,006.84	\$103,784	\$167.96	560	618	58
448140	Family clothing stores	\$329.19	\$224.68	\$153.11	\$153.11	\$1,911,216.20	\$2,109,983	\$167.96	11,379	12,562	1,183
448150	Clothing accessories stores	\$23.48	\$13.73	\$16.87	\$16.87	\$210,564.90	\$232,464	\$167.96	1,254	1,384	130
448190	Other clothing stores	\$39.40	\$23.48	\$7.23	\$7.23	\$90,269.48	\$99,658	\$167.96	537	593	56
448210	Shoe stores	\$50.23	\$37.63	\$70.08	\$70.08	\$874,816.61	\$965,798	\$165.39	5,289	5,840	550
448310	Jewelry stores	\$56.89	\$38.97	\$39.72	\$39.72	\$495,830.79	\$547,397	\$280.09	1,770	1,954	184
448320	Luggage and leather goods	\$14.92	\$1.27	\$2.03	\$2.03	\$25,336.35	\$27,971	\$159.18	159	176	17
451110	Sporting goods stores	\$191.50	\$123.46	\$174.05	\$174.05	\$2,172,687.95	\$2,398,647	\$190.00	11,435	12,624	1,189
451120	Hobby, toy, and game stores	\$49.94	\$64.60	\$29.05	\$29.05	\$362,598.05	\$400,308	\$163.15	2,222	2,454	231
451130	Sewing, needlework, and piece goods	\$20.23	\$33.02	\$55.41	\$55.41	\$691,720.23	\$763,659	\$144.00	4,804	5,303	500
451211	Book stores	\$43.24	\$43.91	\$56.80	\$56.80	\$708,985.42	\$782,720	\$144.00	4,924	5,436	512
453110	Florists	\$12.00	\$4.84	\$25.46	\$25.46	\$317,817.50	\$350,871	\$144.00	2,207	2,437	230
453210	Office supplies and stationery	\$92.40	\$39.98	\$42.61	\$42.61	\$531,901.76	\$587,220	\$144.00	3,694	4,078	384
453220	Gift, novelty, and souvenir stores	\$32.41	\$20.66	\$8.45	\$8.45	\$105,462.78	\$116,431	\$186.32	566	625	59
453910	Pet and pet supplies stores	\$60.91	\$52.58	\$58.66	\$58.66	\$732,239.65	\$808,393	\$144.00	5,085	5,614	529
453920	Art dealers	\$7.08	\$2.65	\$38.96	\$38.96	\$486,389.68	\$536,974	\$159.18	3,056	3,373	318
722410	Drinking places (alcoholic beverages)	\$104.34	\$23.98	\$130.98	\$130.98	\$1,634,969.94	\$1,805,007	\$224.28	7,290	8,048	758
722511	Full-service restaurants	\$741.09	\$338.63	\$704.34	\$704.34	\$8,792,220.45	\$9,706,611	\$224.28	39,202	43,279	4,077
722513	Limited-service restaurants	\$461.81	\$271.59	\$277.34	\$277.34	\$3,462,059.12	\$3,822,113	\$224.28	15,436	17,042	1,605
722514	Cafeterias, grill buffets, and buffets	\$10.96	\$2.84	d	\$2.84	\$35,451.72	\$39,139	\$224.28	158	175	16
722515	Snack and nonalcoholic beverage bars	\$165.35	\$75.33	\$124.60	\$124.60	\$1,555,324.20	\$1,717,078	\$224.28	6,935	7,656	721

Sources: Retail sales WA Department of Revenue (DOR), Population Office of Financial Management (OFM), Sq ft sales ULI Dollars & Cents of Shopping Centers

2013 population Washington State = 6,882,400, Unincorporated Kitsap County = 170,505, Bainbridge Island = 23,190.

d - records not disclosable due to confidentiality requirements.

PLANNING FOR
Whole Communities
TOOLKIT



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Parking Management



Background

Definition

Every automobile trip starts and ends at a parking space. Parking is provided in a variety of contexts: on-street public parking, off-street public parking, private pay lots, and on-site parking for the residents, customers, and employees of private development. In all its forms, parking is a dominant land use in most neighborhoods. There are many good reasons for this. Cars remain the primary mode of transportation in the U.S. and businesses and residents alike rely on parking availability at the start and finish of daily trips of all kinds. However, there are many downsides to the way in which parking supply, and particularly oversupply, has been shaped by public policy.

Health, equity, and sustainability considerations

Parking lots and structured parking contribute to a built environment that discourages walking, biking, and transit use. Rigid and excessive parking requirements for new development can result in inefficient use of urban land, reduced densities, and increased costs that drive up the cost of housing and commercial space and may even render compact development financially infeasible. Public policies that seek to ensure a ready supply of free parking create an incentive for single-occupant auto travel, with negative impacts on air quality, congestion, and public health. Finally, surface parking increases the square footage of impervious surfaces, leading to increased polluted runoff and higher stormwater volumes.

Most new developments provide
50% more parking than is needed.

According to [Parking Evaluation](#) by the Victoria Transport Policy Institute, a typical off-street parking space uses 300-400 square feet of land, whether in a surface lot or parking structure. On-street parking requires 140-160 square feet per space. As a result, up to half or more of the land in many U.S. cities is devoted to parking.

Research from the [Victoria Transport Policy Institute](#) also shows that construction costs for structured parking are estimated to total \$20,000 for above ground and \$40,000 for below ground parking per stall. These figures do not include the lost opportunity cost of land or development capacity for space that could have been occupied by additional housing or commercial space. Structured parking costs add an estimated 12.5% to the cost of housing for each stall required.

Local governments can address the need for automobile parking while mitigating many of its negative impacts by using a range of innovative parking management tools to ensure a tighter fit of parking supply to actual demand. The tools described in this overview of parking management encompass a range of approaches, from flexible regulations to pricing strategies to district-wide management of parking supply.

Program and Policy Examples

Program examples

Most local governments set minimum parking requirements for every land use while at the same time providing free on-street parking. These requirements typically ensure that developers will provide enough spaces to satisfy the peak demand for free parking. Some estimates suggest that 99% of all parking is provided at no cost to the user. The result is that parking is highly subsidized by the public, developers, consumers, and households through increased rents. There are examples, however, of fresh approaches to regulating and managing parking that achieve more balanced and sustainable outcomes.

King County Metro's [Right Size Parking](#) project focuses the best available data and innovative tools to inform the management of parking supply for multifamily residential projects. The project aims to reduce the oversupply of residential parking that compromises the ability of local communities to achieve sustainable, healthy, and transit-supportive outcomes.

The project has produced [guidance](#) on local best practices and for parking policies in multifamily residential development, including a summary table showing parking tools used by communities and a review of market-based and other innovative approaches to parking management as an alternative to mandatory minimums.

Right Size Parking carried out original research on parking utilization in multifamily projects throughout King County. Using a robust set of factors, including transit service, housing prices, and demographics, the project developed a model for predicting parking needs for different types of multifamily projects in a variety of urban locations. The model is at the heart of an online calculator that can be used to inform local policy discussions.

The City of Ithaca, New York, has [eliminated minimum parking requirements](#) in selected residential zones, and has established a committee to evaluate zoning assumptions about parking minimums for new developments (as well as off-street parking pricing). Additionally, the city hired its first Director of Parking to oversee implementation of changes to parking minimums and monitor pricing of public parking.

The City of Santa Monica, California, adopted policy that created an alternative parking provision, rather than eliminating minimum parking requirements. In 1986, the Santa Monica City Council approved a business assessment district to fund improvements for the downtown Promenade area. Part of that program included [this critical piece](#): it gave developers the ability to opt out of providing the required on-site parking by paying an annual fee of \$1.50 per square foot of floor area added for which there was no parking provided. This new policy allows small-scale developers and entrepreneurs to find and implement the most successful uses for those properties without having to worry about whether meeting the expensive minimum parking requirements was practical or cost-effective.

The City of Pasadena implemented higher prices per hour and longer metered times for on-street parking in the Old Town Pasadena commercial district. The parking revenue went directly to Old Town Pasadena to pay for building and sidewalk improvements and maintenance. The higher prices have led to a better balance between the supply and demand for parking, with more vacant parking spots now available, thus

reducing vehicles circling the block. The meter prices discourage many residents and visitors from driving and have led to an increase in other modes of transit to avoid paying for parking.

Another example of innovative on-street pricing for parking is the [SFPark initiative](#) in San Francisco, California. The program uses variable pricing to set rates based on demand as measured in real time by on-street sensors. The aim of this system is to achieve an optimal balance of supply and demand that results in approximately 85% of the parking spaces occupied at any one time.

Development regulations and model ordinances

As part of a larger reform of land-use regulations in 2012, Seattle's Ordinance No. 123939 ([Seattle Municipal Code 23.54.015, Table B](#)) reduced minimum parking requirements by 50 percent for new developments in multifamily and commercial zones with access to frequent transit service. Additionally, new or redeveloping office and manufacturing sites can lower parking minimums 40 percent if the worksite provides transportation alternatives to mitigate demand for single-occupancy travel. The ordinance also removed parking minimums altogether for residential development in urban centers, urban villages, or station overlays, allowing developers to calculate parking provision based on market demand.

The City of Berkeley enacted a Parking Requirement Reduction ([Berkeley Municipal Code Section 23E.28.140](#)) in coordination with its Transit-Oriented Development efforts. The city ordinance reduced the amount of needed off-street parking for new development within 1/3 mile of the Bay Area Rapid Transit (BART). Enacting the reduced parking requirements in close proximity to alternate modes of transit supported the new regulations by providing an alternative to travel by automobile for area residents and visitors alike.

Finally, King County's Right Size Parking project produced an extensive [guide](#) for local jurisdictions that highlighted model code language to enact a range of innovative parking tools.

Performance evaluation

Several aspects of performance management can help to support a parking management system. New technologies exist to track the utilization of on-street parking. Such data can be used as a basis for setting meter rates to match actual demand.

Studies of on-site parking utilization are crucial data sources for calibrating parking requirements (minimums or maximums) to actual needs. The research conducted by King County's program is an excellent model of how to approach this kind of evaluation.

Performance evaluation can and should go beyond actual parking outcomes, but also assess what difference parking reforms have made on the built environment and affordable housing. A study of parking deregulation in Los Angeles showed that removing parking requirements for even a subset of downtown buildings led to a greater number and variety of housing units, including more affordable housing and redevelopment in underused neighborhoods. (See Michael Manville, "[Parking requirements as barrier to housing development: regulation and reform in Los Angeles](#)," Lewis Center for Regional Policy Studies, University of California Los Angeles, 2010).

Implementation

Developing policy language

The Metropolitan Transportation Commission (greater San Francisco) published a [useful guide](#) for parking policy reform aimed at their region's smart growth goals. The guide provides resources for a variety of community types – from regional center to rural town – and transit access, and provides a table of potential strategies for each. Each policy strategy is defined and provided alongside best practices from the San Francisco metropolitan region.

The American Planning Association (APA) published a comprehensive [best practices guide](#) that outlines alternative parking management strategies that establish more accessible land-use patterns; reduce congestion, pollution, and accidents; and enhance mobility for non-drivers.

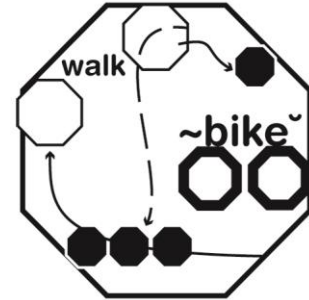
Considerations for local implementation

There are numerous local development incentives that influence parking requirements. According to the PSRC's [Housing Innovations Program](#), reducing minimum parking requirements is most applicable in areas or districts that have good transit accessibility and offer amenities within walking/biking distance. In smaller jurisdictions, reduced parking requirements may be more appropriate in downtown locations or business districts where space is at a premium, congestion is most severe, more transit options are available, and the community wants to encourage a lively pedestrian atmosphere.

Challenges to implementation

- *Concerns about parking spillover.* Residents of districts where reducing or eliminating off-street parking minimums has been proposed will often oppose such reforms out of a concern that developers will not provide sufficient parking, and demand will spill over to take up parking space on surrounding streets. Reduced parking does not mean that a new development will not have parking; tools exist for local governments to address parking demand with better data and developers have strong market incentives to provide sufficient parking in their residential and commercial projects. With supportive regulations, parking may be accommodated with different forms, including tandem stalls, shared spaces, or first-come access parking. In addition, new developments with reduced parking requirements are most appropriately implemented in dense areas with more transit options. Finally, local governments can incorporate residential parking permit programs to ensure residents can access on-street parking.
- *Concerns about parking availability for small businesses.* Business owners may oppose the adoption of new or increased on-street parking charges for fear that it will drive away customers. However, if properly managed to achieve optimal occupancy (about 85%), fees charged for on-street parking can result in an increase in parking turnover and thus increased accessibility to nearby businesses.
- *Education.* Overcoming resistance to changes to existing and long-standing parking regulations and free or low-cost on-street parking can be furthered by educating policy makers, residents, and business owners about the many other policy goals that are affected and perhaps thwarted by policies that result in an oversupply of parking. Furthermore, the case for parking reforms can be strengthened by linking parking charges to tangible local improvements and by coupling parking reductions to enhanced transit service.

Pedestrian-Oriented Design



Background

Definition

Walkable communities are places where people can easily and safely walk to access goods, services and local amenities. They are places that have a variety of transportation options and where pedestrian activity is encouraged. Pedestrian-oriented design encourages a dense mix of land uses including compact residential and commercial areas, smaller block sizes, design features that prioritize pedestrian safety, and local amenities such as parks, street trees and public art. Pedestrian-oriented design also helps to make places more walking friendly by providing a range of transportation options. These can include clear, comfortable pedestrian pathways, bicycle connections, bicycle parking, access trails and walkways, and transit options and access to bus stops.

Health, equity, and sustainability considerations

More than [20% of trips](#) in the region are less than one mile. For these short trips, walking can be the most efficient way to travel. Furthermore, 14% of trips less than one mile are completed by driving alone, while 18% of these trips are completed by two people driving together. In order to encourage people to take more walking trips, an emphasis on safety, walkable communities, and connectivity should be a priority.

Pedestrian infrastructure can yield up to a 200% increase in walking trips.

One way jurisdictions can help increase the number of people walking is to market the many benefits such as improved personal health, lower greenhouse gas emissions, and cost savings (parking fees, gas, etc.).

Walking is a practical way to increase physical activity and improve health. The [Centers for Disease Control and Prevention](#) has drawn connections between active transportation, including walking, and a reduction in obesity, diabetes, osteoporosis, pulmonary and cardiac diseases, and even cancer.

Increased pedestrian activity reduces the reliance on driving and therefore reduces emissions from automobiles. A [5%](#) increase in neighborhood walkability is associated with 6.5% fewer vehicle miles traveled (VMT) per capita. Fewer vehicle miles traveled results in a reduction in fossil fuel consumption and the resulting greenhouse gas emissions. Projects that support walking are often designed in ways that have environmental benefits, such as green landscaping, street trees and in some cases, the use of permeable surfaces.

Making it easier for residents to walk to their destinations can also stimulate the local economy. Providing quality pedestrian access can reduce costs associated with traffic congestion and parking. An influx of foot traffic can also boost sales at local businesses. Walking saves money on transportation costs. This [green dividend](#) can be spent on things other than transportation, such as restaurants and retail purchases, which helps to keep money in the local economy.

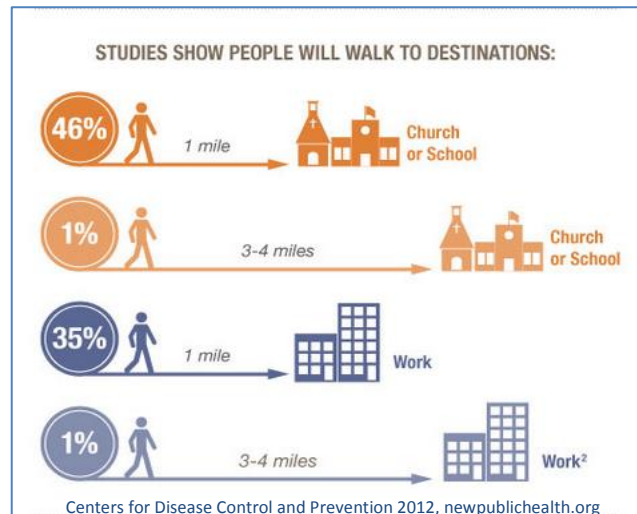
Program and Policy Examples

Program Examples—How is it used locally?

Cities can play an important role in providing walkable communities. By implementing pedestrian-oriented design strategies, local communities achieve economic and health benefits, and create a sense of place. Pedestrian-oriented communities are often best located within centers, near transit, schools, retail areas and other places where people may access goods and services within walkable distances. The following outlines specific strategies for implementing pedestrian-oriented design.

Small Block Sizes and Dense Mix of Land Uses

Walking distance to amenities is one factor related to “walkability.” Land use strategies can help to facilitate this by providing a dense mix of land uses, including compact residential and commercial areas, with smaller block sizes, which are more manageable on foot. Block size is a good indicator of pedestrian-scale development and overall walkability. A small average block size reflects multiple access points to the activities located on that block, and a fine network of streets.



For a high degree of walkability, block lengths of [300 feet](#), more or less, are desirable, although blocks of 400-500 feet still function to support pedestrian-oriented environments. These are typical in older, urban areas. Block sizes that are more scaled to the automobile (more than 600-800 feet) can be made more pedestrian friendly with mid-block crossings as well as pedestrian pathways between buildings, through alleys and along easements to allow for access to amenities within a walking distance.

Connectivity of Walkways

The connectivity and contiguity of sidewalks and pedestrian pathways facilitates walkability and is critical for safety and to accommodate people of all ages and abilities. Connected walkways can also help break up large block sizes into more manageable walking distances. Connection to regional trails and shared use paths can help to improve this connectivity. The quality of pavement is critical for the safety of all users. Wider sidewalks than the recommended five feet may be installed in busier areas with high concentrations of pedestrians.

Prioritizing pedestrian infrastructure can be a challenge for jurisdictions that lack pedestrian amenities. Local jurisdictions can begin to evaluate high priority areas by including sidewalks and pathways in the inventory of the transportation system, developing pedestrian networks for incorporation into comprehensive plans and assessing areas that are most appropriate for pedestrian-oriented design such as within local centers, retail and activity centers, near schools or parks, transit hubs, or in areas that have historically lacked investment such as low-income areas that can benefit from infrastructure that provides lighting and encourages safety and visibility. Safe Routes to School programs are a great initiative to encourage walking and network planning in and around schools.

Local communities should then consider the walking distances and existing infrastructure from these destinations when assessing pedestrian routes. An industry standard for an average pedestrian trip is about one-half of a mile, or about a ten-minute walk. Considering the walking distances, conditions and routes to access these destinations is the first step in developing a pedestrian network.

Sidewalks are opportunities for social engagement as well as for taking walking trips. In small centers and rural main streets, the sidewalk becomes an integral part of community character. Community outreach is one of the key elements to assessing pedestrian networks. Jurisdictions are encouraged to engage with community members to identify needs.

In 2013, the City of Olympia created the [Neighborhood Pathways Program](#) to increase neighborhood walkability and to involve residents in the creation of pedestrian and bicycle paths in their neighborhood. The program works to construct non motorized routes that connect to parks, streets, schools and other services. Local neighborhood associations are the key driver behind the proposals for the program. The majority of construction is to be completed by community volunteers. The [Olympia Bicycle and Pedestrian Advisory Committee](#) gives about \$150,000 to the program every year. Funding comes from a private utility tax approved by voters in 2004.

The [City of Tukwila](#) assessed walking distances in order to prioritize investments. The city assessed walking distances one-fourth and one-half mile from priority destinations such as schools, shopping centers and employment hubs. This information overlays the existing transportation system, including sidewalks, paved shoulders, and existing, and future separated shared use paths.

Access to Walkable Places

As jurisdictions prioritize pedestrian improvements, a range of travel options also should be considered. Bicycle networks that connect to pedestrian zones and a range of transit options help facilitate access to walkable communities. Access to transit stops within walkable communities as well as adequate bicycle parking helps to relieve congestion and parking pressure within areas where pedestrian activity is encouraged.

Engineering Solutions for Safety

Safe crossings are also critical to supporting pedestrian-oriented design. Crossing treatments at bus stops, intersections and mid-block crossings within reasonable walking distances help to prevent dangerous jaywalking in areas with high concentrations of pedestrians and transit users. Improving visibility at crossings, refuge islands and increased crossing times that accommodate people with slower mobility can improve safety in walking environments.

Curb extensions (also called bulb-outs) extend the sidewalk into the parking lane in order to narrow the roadway, shortening crossing distances, slowing traffic speeds and providing additional pedestrian space and visibility. It is critical that public works engineers are highly trained in pedestrian design issues and kept up to date on best practices.

Pedestrian Priority Zones

Pedestrian priority zones help communities identify places that may attract high numbers of pedestrians and provide for vibrant streetscapes that create a high quality of life. These zones are often located within

local or regional centers and near transit and bicycle infrastructure. Signage, art, wayfinding and safe infrastructure help to facilitate a pedestrian zone.

Reduce speed limits

In 2013, the Washington State Legislature passed the [Neighborhood Safe Streets](#) bill, which allows more flexibility for local communities to reduce speed limits to 20 miles per hour. This provides communities another option for creating safer environments for all users.

Jurisdictions can also improve pedestrian safety by improving lighting and visibility on walkways. This “eyes on the street” strategy can help to improve real and perceived safety along pedestrian networks. The [Crime Prevention Through Environmental Design](#) resource guide provides more information on design strategies to promote safe spaces.

Implementation

Opportunities for funding

The Washington State [Department of Commerce](#) offers several grants that provide funding for pedestrian-oriented design efforts, including the Washington State Community Development Block Grant and funding from the Community Economic Revitalization Board.

Local jurisdictions also have the opportunity to pass an ordinance or tax levy to create an ongoing funding source for pedestrian improvements. In 2004, voters in the City of Olympia approved a [3% increase in the utility tax](#) to fund improvements to parks, sidewalks, and open spaces.

Additionally, many pedestrian-oriented design projects may be eligible for funding from complete streets funding programs. See the Complete Streets resource guide for more information.

Considerations for local implementation

[Transportation 2040](#) calls for the development of local and regional pedestrian networks. Identifying networks can help direct resources to those areas with the greatest likelihood to result in increased walking.

Centers (both regional and locally designated) and transit station areas are ideal locations for investments that support and encourage more walking. Further, following the direction of [VISION 2040](#) and encouraging compact development patterns near transit should result in places that are more walkable.

Municipal code and improvement districts can support the development and maintenance of pedestrian infrastructure. The [City of Lacey’s municipal code](#) (16.25) outlines sidewalk requirements and maintenance for the city’s central business district. These requirements include planter strips separating sidewalks from the street curb and a local grant program to fund sidewalk improvement.

Challenges to implementation

One explanation for why some people choose not to walk more is because of real or perceived issues of safety. Pedestrians are much more vulnerable to incidents involving motor vehicles than almost all other modes of transportation. Safety issues stem not only from conflicts with motor vehicles, but also from places that may pose a crime risk such as poorly lit areas. Enforcement strategies—aimed at specific locations or at specific behaviors that put pedestrians at risk—are crucial to overcoming these barriers.

Resources

The City of Seattle's [Pedestrian Toolbox](#) (2014)

Federal Highway Administration's [A Resident's Guide for Creating Safe and Walkable Communities](#) (2008)

Feet First's [Walking Audits](#) (2014)

Puget Sound Regional Council's [Active Transportation Plan](#) (2014)

Tacoma-Pierce County Health Department's Healthy Community Planning Toolbox—Policy Intervention Tool: [Placemaking](#) (2013)

University of British Columbia's [The Walkability Index](#) (2013)

U.S. Department of Transportation's [Walkability Checklist](#)

U.S. EPA's Technical Assistance for Sustainable Communities: [Walkability Workshop Report](#) (2011)

[Walkshed tool](#) (2010)

Outcome Matrix

Resource Guide	Health			Equity		Sustainability		
	Obesity Prevention	Reduced Exposure to Harmful Pollutants	Improved Mental Health & Well Being	Community Engagement & Political Access	Access to Jobs & Regional Prosperity	Multimodal Mobility & Connectivity	Mixed Use & Compact Development	Protect & Restore Natural Systems
Active Travel Choice Programs						*	*	*
Affordable Housing		*	*		*		*	
Brownfield Redevelopment	*	*					*	*
Community Engagement Tools				*				
Community Gardens & Urban Agriculture	*		*	*				*
Complete Streets	*		*			*	*	*
Crime Prevention Through Environmental Design			*	*				
Design for Aging in Place	*		*		*	*	*	
Green Stormwater Infrastructure		*						*
Greenhouse Gas Emission Reduction Strategies		*						*
Green Waste Management								*
Health Impact Assessment								
Healthy Food Retail	*				*			
Inclusive Contracting & Business Development				*	*			
Joint Use Agreements	*		*	*				
Multimodal Concurrency						*		*
Opportunity Mapping								
Parking Management						*	*	
Pedestrian-Oriented Design	*					*	*	
Recognition Programs				*				
Safe Routes to School	*		*			*		*
Special Needs Transportation					*			
Sustainable Parks and Open Spaces	*							*
Tobacco-Free Parks	*	*						
Transit-Oriented Development					*	*	*	

Kingston UGA Capacity-to-Target Estimate			
	2016 Baseline	2016 Corrected	2019 Estimate
Population	-2.5%	-3.2%	-1.3%
Employment	14.7%	20.3%	14.7%

Countywide UGA Capacity-to-Target Estimate			
	2016 Baseline	2016 Corrected	2019 Estimate
Population	-5.1%	-5.2%	-5.0%
Employment	1.4%	1.6%	1.4%

Kingston UGA Population Capacity Estimate			
	2016 Baseline	2016 Corrected	2019 Estimate
Kingston UGA Target	2,926	2,926	2,926
2016 Plan Capacity	2,854	2,831	2,831
UVC Zone			58
Revised Capacity			2,889
Variance from Target (#)	(72)	(95)	(37)

Countywide Population Capacity Estimate			
	2016 Baseline	2016 Corrected	2019 Estimate
Countywide UGA Target	33,551	33,551	33,551
2016 Plan Capacity	31,837	31,814	31,814
UVC Zone			58
Revised Capacity			31,872
Variance from Target (#)	(1,714)	(1,737)	(1,679)

Kingston UGA Employment Capacity Estimate			
	2016 Baseline	2016 Corrected	2019 Estimate
Kingston UGA Target	597	597	597
2016 Plan Capacity	685	718	718
UVC Zone			(33)
Revised Capacity			685
Variance from Target (#)	88	121	88

Countywide Employment Capacity Estimate			
	2016 Baseline	2016 Corrected	2019 Estimate
Countywide UGA Target	14,007	14,007	14,007
2016 Plan Capacity	14,200	14,233	14,233
UVC Zone			(33)
Revised Capacity			14,200
Variance from Target (#)	193	226	193

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UNDERUTILIZED LANDS	2016	2016 Corrected	2019 Estimate
Gross Residential Acres (Developed Parcels only)			
Total Gross Acres	26.83	26.83	26.83
Multi - Family Dwelling (-)	1.36	1.36	1.36
Parcels < 0.50 Acres (-)	15.34	15.34	15.34
Public/Current Use/Utilities (-)	6.44	6.44	6.44
Shoreline Parcel < 1.00 Acre (-)	0.00	0.00	0.00
Subtotal	3.69	3.69	3.69
Redevelopable Acres			
<i>Median Value \$143,497</i>			
Density < 2.5 (-)	0.00	0.00	0.00
Density >= 2.5 - < 4.00 Value > (-)	0.00	0.00	0.00
Density >= 4.0 - < 5.00 Value > (-)	0.00	0.00	0.00
Density >= 5.0 - < 10.00 Value > (-)	0.00	0.00	0.00
Subtotal	0.00	0.00	0.00
Remove 100% of Platted lots (acres)	0.00	0.00	0.00
Critical Areas			
Total Redevelopable Acres	0.00	0.00	0.00
Unencumbered Acres	0.00	0.00	0.00
Acres within Critical Areas	0.00	0.00	0.00
Critical Areas reduction 75% (-)	0.00	0.00	0.00
Acres within Area of Concern	0.00	0.00	0.00
Area of Concern reduction 50% (-)	0.00	0.00	0.00
Subtotal	0.00	0.00	0.00
Roads/Right-of-Way (Future)			
20% (-)	0.00	0.00	0.00
Public Facility (Future)			
20% (-)	0.00	0.00	0.00
Unavailable Lands			
15% (-)	0.00	0.00	0.00
Commercial Split			
Old 50%; New 70% (-)	1.85	1.85	2.58
Platted Lots			
<i>50% (-)</i>	<i>50% (-)</i>	<i>50% (-)</i>	
Underutilized Platted Lots 25% (+)	0.00	0.00	0.00
<i>Platted Lot existing dwelling unit (count)</i>	3	3	3
Dwelling Unit count (-75%)	0	0	0
Existing Dwelling Units			
<i>Existing Dwelling Units (non platted lots) (-)</i>	0	0	0
Total Dwelling Units (-)	2	2	2
TOTALS			
Net Developable Acres	1.85	1.85	2.58
	<i>12 DU/AC</i>	<i>12 DU/AC</i>	<i>16 DU/AC</i>
Dwelling Unit Capacity	21	22	41
	<i>1.8 pph</i>	<i>1.8 pph</i>	<i>1.8 pph</i>
Population Capacity	37	40	74

VACANT LAND	2016	2016 Corrected	2019 Estimate
Gross Residential Zoned Acres			
Total Gross Vacant Acres	2.53	2.53	2.53
Subtotal	2.53	2.53	2.53
Redevelopable Acres			
Subtotal	2.53	2.53	2.53
Vacant Platted Lots (-)	2.53	2.53	2.53
Critical Areas			
Total Redevelopable Acres	0.00	0.00	0.00
Unencumbered Acres	0.00	0.00	0.00
Acres within Critical Areas	0.00	0.00	0.00
Critical Areas reduction 75% (-)	0.00	0.00	0.00
Acres within Area of Concern	0.00	0.00	0.00
Area of Concern reduction 50% (-)	0.00	0.00	0.00
Subtotal	0.00	0.00	0.00
Roads/Right-of-Way (Future)			
20% (-)	0.00	0.00	0.00
Public Facility (Future)			
20% (-)	0.00	0.00	0.00
Unavailable Lands			
5% (-)	0.00	0.00	0.00
Commercial Split			
Old 50%; New 70% (-)	1.27	1.27	1.77
Subtotal	0.00	0.00	0.00
Platted Lots			
Vacant Platted Lots	14	14	14
	0	0	0
Net Developable Acres	1.27	1.27	1.77
	<i>12 DU/AC</i>	<i>12 DU/AC</i>	<i>16 DU/AC</i>
Dwelling Unit Capacity	29	15	28
	<i>1.8 pph</i>	<i>1.8 pph</i>	<i>1.8 pph</i>
Population Capacity	53	27	51

Total Net Developable Acres	3.11	3.11	4.35
	<i>12 DU/AC</i>	<i>12 DU/AC</i>	<i>16 DU/AC</i>
Total Dwelling Unit Capacity	50	37	70
	<i>1.8 pph</i>	<i>1.8 pph</i>	<i>1.8 pph</i>
Total Population Capacity	90	67	125

Check: Total (unsplit) Acres 6.22 6.22

UNDERUTILIZED LANDS	2016	2016 Corrected	2019 Estimate
Gross Acres (Developed Parcels only)	See Residential Sheet for split	See Residential Sheet for split	See Residential Sheet for split
Total Gross Acres	0.00	0.00	0.00
Multi - Family Dwelling (-)	0.00	0.00	0.00
Public/Current Use/Utilities (-)	0.00	0.00	0.00
Subtotal	0.00	0.00	0.00
Remove 100% Planned Lots	0.00	0.00	0.00
Remove 100% Platted	0.00	0.00	0.00
Subtotal	0.00	0.00	0.00
Critical Areas			
Total Redevelopable Acres	0.00	0.00	0.00
Unencumbered Acres	0.00	0.00	0.00
Acres within Critical Areas	0.00	0.00	0.00
Critical Areas reduction 75% (-)	0.00	0.00	0.00
Acres within Area of Concern	0.00	0.00	0.00
Area of Concern reduction 50% (-)	0.00	0.00	0.00
Subtotal	0.00	0.00	0.00
Roads/Right-of-Way (Future)			
20% (-)	0.00	0.00	0.00
Public Facility (Future)			
20% (-)	0.00	0.00	0.00
Unavailable Lands			
25% (-)	0.00	0.00	0.00
Underutilized Acres	1.85	1.85	1.11
VACANT LAND	URBAN VILLAGE CENTER	URBAN VILLAGE CENTER	URBAN VILLAGE CENTER
Gross Acres	See Residential Sheet for split	See Residential Sheet for split	See Residential Sheet for split
Total Gross Acres	0.00	0.00	0.00
Remove 100% Planned Development	0.00	0.00	0.00
Remove 100% Platted Lots	0.00	0.00	0.00
Subtotal	0.00	0.00	0.00
Critical Areas			
Total Redevelopable Acres	0.00	0.00	0.00
Unencumbered Acres	0.00	0.00	0.00
Acres within Critical Areas	0.00	0.00	0.00
Critical Areas reduction 75% (-)	0.00	0.00	0.00
Acres within Area of Concern	0.00	0.00	0.00
Area of Concern reduction 50% (-)	0.00	0.00	0.00
Subtotal	0.00	0.00	0.00
Roads/Right-of-Way (Future)			
20% (-)	0.00	0.00	0.00
Public Facility (Future)			
20% (-)	0.00	0.00	0.00
Unavailable Lands			
20% (-)	0.00	0.00	0.00
Vacant Acres	0.00	1.27	0.76
	32%	32%	32%
Total Net Developable Acres	1.85	3.11	1.87
Net Developable w/ Applied FAR			
Total Square Footage Planned Developments	0	0	0

Split: Old 50%, New 30%

Split: Old 50%, New 30%

	12/31/2012	2012 Corrected	2019 Estimate
Net Developable Acres	1.85	3.11	1.87
Total Net Developable Acres	1.85	3.11	1.87
Conversion to Square Footage	80586	135472	81283
Existing Floor Area (-)			
Total Square Footage	80586	135472	81283
Industrial Square Footage			
Industrial Planned lots			
Total			
Commercial Square Footage	25788	43351	26011
Commercial Planned lots	0	0	0
Total	25788	43351	26011
Total Building Square Footage	25788	43351	26011

	12/31/2012	2012 Corrected	2019 Estimate
Net Developable Acres	1.85	3.11	1.87
Building Area Estimates (Square Footage)	25,788	43,351	26,011
Vacancy Rate Adjustment (5%)	24,498	41,183	24,710
Employment Rate - Square Feet Per Employee	500	500	500
Resulting Employment Capacity - Jobs	49	82	49

Kingston UVC Achieved Density Assessment

Historically Achieved Density = Permitted Development (Consistent with Revised Regulations)

Permit #	Project	2012 BLR/LCA Status	Year Issued	Built	Zone	HCT Station Area	Type	Demo Units	New Units	Gross Acres	Gross Achieved Density	Max Density (KCC)	Net Acres	Net Achieved Density	Demo Commercial SF	New Commercial SF
Unknown	Townhouses (Iowa/2nd)	Developed	~1999	Yes	UVC	Yes	MF-Townhouse	Unk	5	0.37	14	N/A	0.25	20	0	0
09-87029	Tanner Building (2nd)	Developed	2011	Yes	UVC	Yes	Commercial	1	0	0.22	0	18	0.22	0	0	5375
14-03694	Sterner SFR Replacement (Ohio)	Developed	2014	Yes	UVC	Yes	SFR-Detached	1	1	0.22	5	18	0.22	5	0	0
16-05735	Hinoki Terrace (Ohio-Iowa)	Split Vacant/Developed	2016	No	UVC	Yes	SFR-Attached	2	6	0.48	13	18	0.26	23	0	0
16-05615	Washington Park (Washington)	Vacant	2018	Yes	UVC	Yes	Park	0	0	0.36	0	18	0.36	0	0	0
Average													0.26	10		

Older project; valid to include because it represents smallest lot size for one SFR/townhouse at minimum density - probable on some small lots due to unconsolidated ownership patterns in the UVC zone
2-stories; allowed maximum height was 35'; valid to include because some lots will likely not include residential
Replacement of existing SFR; valid to include because there are many lots like this in the UVC zone
Short plat subdivision just outside of the Kingston design districts; 3-stories; allowed maximum height was 45'; most parking is underbuilding
Valid to include since this lot was vacant in BLR/LCA; was former restaurant site that burned down in 2005

Probable Future Achieved Density = Above Projects + Permitted (or Slightly Modified) Local Development Examples that are Reflective of Revised Regulations

11-102969	Martha & Mary Senior Housing (Dulay)	Developed	2012	Yes	NC	Yes	MF-Apartment (senior assisted living)	N/A	35	1.15	30	30	1.15	30	0	0
N/A	Modified Belmont (Washington)	Developed	Est	N/A	UVC	Yes	Mixed-use	0	8	0.22	36	N/A	0.22	36	0	4648
Combined Average													0.38	16		

Senior assisted living; 3-stories in height from main entrance; 4-stories facing street; allowed maximum height was 45'; subject to design standards; surface parking; less parking needed for this specific use; ~40% reduction from proposed parking requirements for normal MF-apartment; valid to include since it met design standards and is needed use in this community

4-stories; modified by converting 4 offices on 2nd floor (of 2006 design) to 4 dwelling units; Underground, underbuilding, and on-street parking; valid to include since it reflects an expected development model on some larger lots.

Residential Split 70%
Commercial Split 16% Includes mixed-use
Park Split? 13%

Ruled Out = All Other Permitted Developments since 2010 that are not Reflective of Revised Regulations or UVC

10-93015	Maine St Building (Maine)	Vacant	2010	No	UVC	Yes	Mixed-use	Unk	Unk	0.20		18	0.20	Unknown	Unknown	Unknown
11-100741	Kobuki MH (2nd)	Underutilized	2011	Yes	UVC	Yes	SFR	0	1	0.21	5	18	0.21	5	0	0
12-104423	Phillips SFR Demo (Ohio)	Vacant	2012	N/A	UVC	Yes	Demo	1	0	0.21		18	n/a			
14-01055	Talmage SFR Demo (Iowa)	Developed	2014	N/A	UVC	Yes	Demo	1	0	0.15		18	n/a			
15-02900	Sanderson MH Demo (Illinois)	Developed	2015	N/A	UVC	Yes	Demo	1	0	0.32		18	n/a			
18-01960	Chumbley SFR Demo (1st)	Developed	2018	N/A	UVC	Yes	Demo	1	0	0.11		18	n/a			
17-04643	Seaside Kingston (Lindvog)	Split Vacant/Underutilized	2018	Under Construction	C	No	MF-Townhouse	1	140	7.08	20	30	2.84	49	0	0

Online application information was limited and did not specify adequate information to evaluate
Not valid to include because this should have had to meet minimum density of 10 DU/Ac since lot was undeveloped at time of permitting
Demo only
Demo only
Demo only
Demo only

Subdivision in C-zone but subject to design standards; 3-stories; allowed maximum height was 35'; outside HCTSA, but granted 15% parking reduction; not valid to include because lots this size do not existing in the UVC zone and development assumptions would be quite different on smaller lots