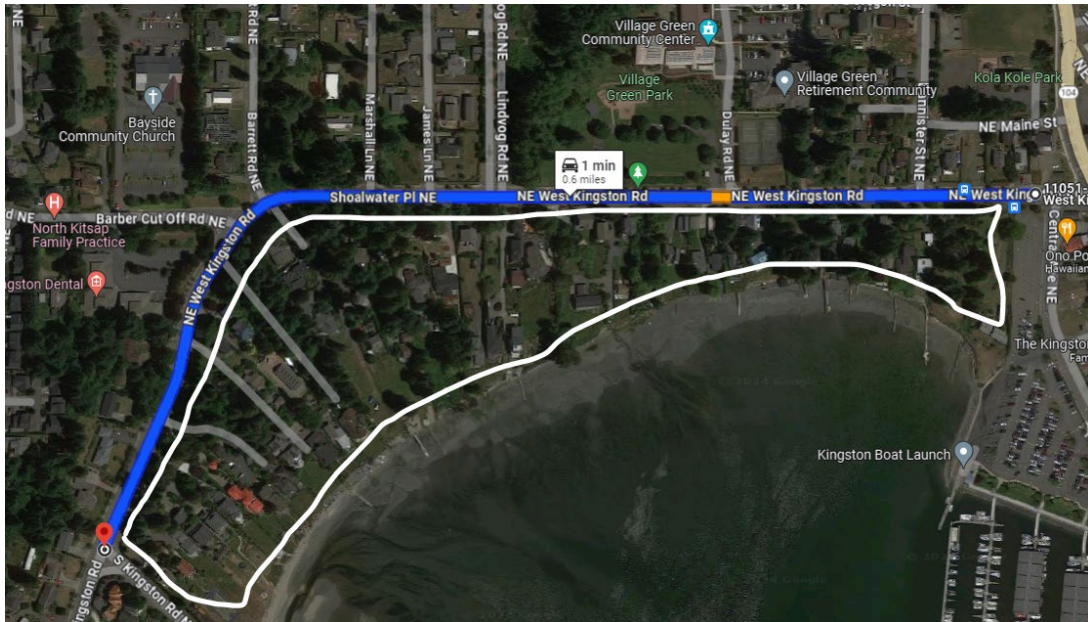


The Kingston Community Advisory Council would like to ask your support for our intent to submit two Transportation Improvement Projects (TIP's) to Kitsap Public Works. Two recommended projects involve Kingston's Elementary Schools and the complexity of intersecting rural Kitsap County roads with State Route 104's 45+ MPH traffic. Our third submission asks KPW to address resident concerns regarding safe pedestrian crossing of N.E. West Kingston Rd near the Village Green Community Center.

1. The intersection of Barber Cut Off Rd NE/Parcells Rd NE and SR 104. This junction has numerous issues that warrant improvement.
 - The county roads connect at an angle to SR 104, which widens the distance to cross the highway from one county road to the other.
 - Getting from Barber Cut Off Rd to Parcells Rd NE involves crossing, from a stop sign, SR104 with a 45 MPH limit, a limit that appears to be generously exceeded by most drivers.
 - Kingston's WSF port is 1.5 miles away and handles over 2MM cars annually, the vast majority of which pass this intersection.
 - This intersection will see an increase in turns from both Barber Cut Off NE and Parcells Rd Ne on to west bound SR104 with the installation and activation of the forthcoming Active Traffic Management System (ATMS).
 - When in operation, most Kingston residents bound for the ferry, that live west of this intersection, will use Barber Cut Off Rd NE to route them to the ATMS at Lindvog Rd NE.
 - Local residents are already vocal about the need for traffic control improvement at this intersection. The need for improved traffic control will only be accentuated with the opening of the ATMS.
 - Kingston residents driving to board the ferry who live North of SR104 and East of this intersection, will be tempted to save time by making illegal and dangerous U-Turns at or near this intersection. If a roundabout were to be chosen as the solution, these residents could safely navigate a legal U-Turn here.
 - There are currently no pedestrian crossing safety features.
2. The second intersection in need of improved traffic control is Highland Rd NE and SR 104 at Kingston's Wolfle Elementary School.
 - As Highland Rd NE is the lone entrance and departure road for Wolfle Elementary school, the challenge here is simply to move student drop-off and pick-up traffic safely and efficiently between SR104 and Highland Rd NE.

- With a student body of some 400 and a staff of 65, cars and school buses are forced to wait two or more minutes each to turn onto 50 MPH traffic on SR 104 in the morning and afternoon. This creates lengthy delays and parent frustration.
- Traffic dangerously backs up on the state route waiting to turn left into the school.
- School district staff and parents have been sharing these concerns with WSDOT for years with little progress.
- Through these efforts, WSDOT conducted a traffic study that showed the intersection was rated very low in safety and efficiency.
- WSDOT agreed to install, in 2024, a yellow light that will flash during the am arrival and pm departure times that would attempt to slow traffic to 35mph in this zone. WSDOT's representative told Principal Lahaie that this solution is only a "Band-Aid to the problem". Please note that the school would need to pay for the equipment!
- Highland Rd NE from the school to SR104 is two-lane road that has no right turn lane or shoulder at SR 104. As such, every vehicle turning right onto SR104 has to wait behind vehicles waiting to turn left onto the state highway, significantly increasing the wait times and frustration of parents.
- For those turning left onto SR104 from Highland Rd NE, there is no center lane in the highway that would allow traffic to cross eastbound traffic and merge into westbound traffic.
- This left turn delay forces school buses that intend to go west on SR104, to turn right and take a detour through a strip mall at Georges Corner, to put them on Miller Bay Rd, then turn left back on westbound SR104! A two plus mile detour that adds several minutes to the bus trips, burns additional fuel, and adds carbon to the atmosphere.

3. There is a need for a safe place for pedestrians to cross from the south side of N.E. West Kingston Rd. to the north side. N.E. West Kingston Rd is one of Kingston's busiest roads.



- There are more than 50 homes on the south side of N.E. West Kingston Rd between S. Kingston Rd N.E. and Central Ave N.E. None of these homes on this .6-mile route are serviced by a sidewalk to cross to the north side.
- The north side of N.E. West Kingston Rd. along this same distance does have a sidewalk.
- If a pedestrian wishes to cross this street using a sidewalk, they would need to walk to S. Kingston Rd N.E., or to Central Ave N.E.
- Local residents have expressed concern over the lack of anywhere to safely cross. There are numerous senior citizens living on the south side of this road, and as Kingston's very popular Village Green Community Center is located on the north side of N.E. West Kingston Rd. many cross the road regularly. Resident Mike Reed has witnessed numerous "close calls" for both seniors and children.
- The intersection of N.E. West Kingston Rd and Lindvog Rd N.E. or at the entrance to the Village Green Community Center, Duley Rd N.E. would appear to offer sensible locations to offer some type of passive or active pedestrian crossing.
- Kitsap Public works has said that since there is no sidewalk on the south side of N.E. West Kingston Rd, there is nowhere to connect a crosswalk to the north side. Yet, as near as .3 mile away, there is a similar intersection, Barber Cut Off Rd N.E. and N.E. Carpenter Lane. At this intersection, near Cooper Elementary

School, there is a pedestrian activated flashing light crosswalk. This intersection has no crosswalk on either side of the street.



- Kitsap Public Works has also stated that the Bike Lane on the south side of N.E. West Kingston Rd presents an obstacle to walking along that side of the road. Again, a short distance away, this challenge appears to be managed with a single traffic sign that alerts drivers that both pedestrians and bikes may be present on Barber Cut Off Rd.



In conclusion, the KCAC is recommending that TIPs be added to Kitsap Public Works plans to build safe and efficient solutions at both of these high traffic intersections and uncontrolled pedestrian crossing areas.

Thank you, Kingston Community Advisory Council