

Comment 2.5

P. O. BOX 310, SILVERDALE, WA 98383
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portofsilverdale@wavecable.com

January 17, 2020

Kitsap County Commissioners
614 Division St. MS - 4
Port Orchard, WA 98366

RE: Annual Comprehensive Plan Amendment
Dickey Pit Specific Application (CPA 18-00495)

Dear Commissioners:

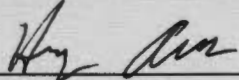
At the Port of Silverdale commissioners meeting on January 16, 2020 we considered the Dickey Pit Specific Application (CPA 18-00495) for an annual comprehensive plan amendment to convert an area between Dicky Road and Willamette Meridian Road from industrial use to residential and neighborhood commercial.

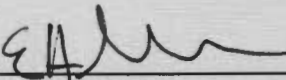
The Dickey Pit area exceeds 138 acres and includes the only significant Industrial zoned area within the boundaries of the Port of Silverdale District, which includes much of Central Kitsap and the Silverdale UGA. As a Port District concerned about the economic welfare of our area, we share these thoughts with you:


- This proposal would reduce the industrial zoned land within the Silverdale UGA by over 50%, and this industrial land may be needed for future economic growth in the Silverdale area – we note that the county’s Land Use Goal 10 is to “Maintain sufficient industrial land area in the Urban Growth Areas for future industrial use;”
- If the Silverdale UGA may someday become the city of Silverdale, it would be imprudent to deprive it of this industrial land as a significant economic resource;
- There does not seem to be a need to create higher density residential uses west of Dicky Road, as there are adequate undeveloped areas zoned for residential use remaining in the Silverdale UGA;
- This area is within the flight pattern of planes using Apex general aviation airport, and to allow high density residential housing would invite an inevitable conflict with noise and safety issues, which is why state law requires the county comprehensive plan to discourage the siting of incompatible uses adjacent to general aviation airports;
- To create residential and commercial uses on some of the land now zoned industrial, while retaining industrial use for the remainder and neighboring land, would invite a clash of incompatible uses due to noise, dust, traffic, smells, vibration, etc., and diminish the attraction of economically beneficial industries to the area due to such potential conflicts.

In summary, we agree with the staff recommendation to deny the proposed amendment, as it is not in the public interest or consistent with the Kitsap County Comprehensive Plan. A denial would ensure the best flexibility for optimum economic develop of the Silverdale community.

Sincerely,
Port of Silverdale Commissioners:


Henry Aus


Ed Scholfield


Caleb Reese

January 21, 2020

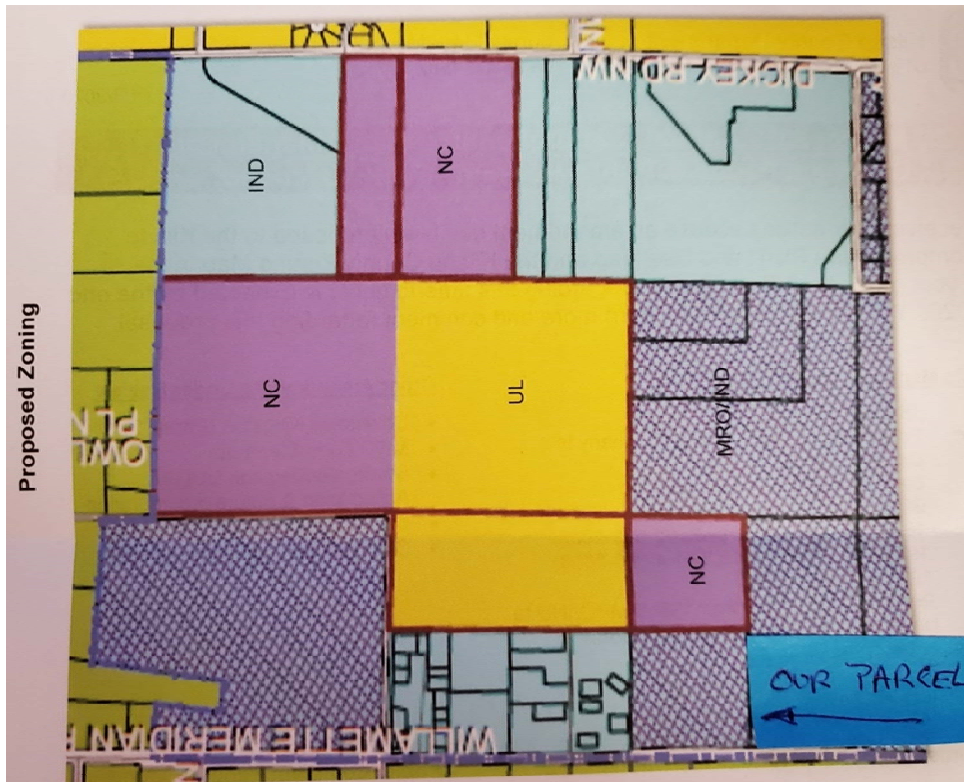
Peter Best, Senior Planner
Planning and Environmental Programs
Kitsap County Department of Community Development
619 Division Street MS-36
Port Orchard, WA 98366-4682

Subject: 2019 Annual Comprehensive Plan Amendment - Rezone

Dear Peter,

This letter is to provide our comments regarding the Site-Specific map amendment proposed in our area. The Permit number referenced in the county notice dated 1/3/2020 is 18-00495. I have also provided a second letter in response to "Other Amendments Under Review" (as identified in the same county notice).

We own 10 acres abutting part of the proposed rezone, tax id number: 192501-2-005-2006, which has the same industrial zoning as most of the property being considered for rezone. Our North, South, and East property lines currently abut industrial zoned property and, as such, we are not required to have a buffer to isolate our property from the neighboring property.



If the property is allowed to be rezoned, we want to make sure that the buffer requirements are placed on the rezoned property and are not forced upon our property. This would require a 50 foot native vegetation buffer around the Dickey Wood LLC property along our common property line in lieu of a 50' buffer on our property.

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As an alternative to buffer restrictions being placed on the current proposed property (and if these parcels are deemed suitable by Kitsap County for 5-9 DU/Ac and 10-30 DU/Ac zoning), I would recommend one of those same zoning classifications also be placed on our parcel. In addition, this alternative may also solve another apparent problem; it could provide an option for an additional residential road access (for the rezoned residential property to Willamette Meridian).

Our hope is that your office reviews the best options for neighboring properties as well as the proposed rezoned property and that additional restrictions not be placed upon the neighboring properties for the sole benefit of the rezoned property.

Sincerely,

Dave Wixson

Owner/Manager
DCRW Properties LLC

Michael Costello
Pyramid Materials
P.O. Box 3337
Renton WA 98056
425-757-0762
mcostello@pyramidmaterials.com

Kitsap County Planning Commission
614 Division St ms-4
Port Orchard, WA 98366

RE: Annual Comprehensive Plan Amendment
Dickey Pit Specific application (CPA 18-00495)

Dear Kitsap County Planning Commission:

I am writing to express concerns regarding the site specific application regarding Dickey Pit and changing the zoning from Industrial with MRO to NC (10-30 DU/AC) and UL (5-9 DU/AC). I am the managing member of the adjoining 120 acre properties consisting of an active mine site, asphalt plant, and industrial processing yard. The changing of the adjoining property to residential would have a significant impact on our business, reduce the access of the mineral resource , and create a significant risk to public safety. Specific concerns are as follows:

- Mining operations with current and future expansions will directly border the parcels in question, creating the requirement to modify our current mine plan and increase setbacks which would greatly reduce the ability to extract the mineral resource.
- Mine operations create slopes of over 100 foot in height. Public access is prohibited by regulation, and controls are put in place to limit access. With restrictions in place you still occasionally have illegal access to the properties and dispatch of local police to intervein. An increase in residential development bordering the property would create a potentially dangerous situation.
- Mine and asphalt plant operations generate fairly significant noise levels. Development of residential would limit the industrial and mining activities and place the burden on the owner. Currently the location of the mine and asphalt plant with bordering industrial parcels keeps the residential segments at a distance where noise is not a major issue. This would change with rezoning.
- Existing stormwater ponds on the parcels in question in the application have been used for many years to collect runoff from the adjoining mine site and allow infiltration without discharge off site. The northern pond was constructed by the original owner many years ago and set up with a bio swell to allow infiltration of water from the asphalt plant. The

Kitsap County Planning Commission

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approved design with the dept. of Ecology collects the water from the asphalt plant yard in catch basins and diverts it to tanks where it is monitored for compliance. Water is then discharged onto the property in the application where it passes through a bio swell and then infiltrates through the pond. A change in the parcels to residential would create a significant problem with needing to completely redesign the water retention system that was put in place by the original property owners.

A change to residential would be incompatible with existing mining and heavy industrial uses, would significantly limit the viability in the future for continued industrial use, limit the access to more than 50 years remaining mineral resources, and place the public immediately adjacent to a burdensome and potentially dangerous location it was never intended to be. It is my opinion that the application to change the parcels to anything other than industrial with MRO at this time would not in anyone's best interest.

Sincerely,

M. Costello

Michael Costello
Pyramid Materials



LAW OFFICE OF
RICHARD B. SHATTUCK
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SILVERDALE, WA 98383

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FAX: (360) 698-5569

January 22, 2020

VIA E-MAIL

Department of Community Development
Kitsap County
Planning and Environmental Programs
619 Division Street
Port Orchard, Washington 98366
Attn: Planning Commission

RE: **Dickey Pit**
CPA 18-00495

Dear Planning Commissioners:

I want to thank you and the Department of Community Development for the opportunity to comment on the 2019 Annual Comprehensive Plan Amendment proposals. I specifically wish to address the Staff recommendation to deny the Comprehensive Plan amendment proposal for the Dickey Pit Site-Specific request, CPA 18-00495.

I was frankly surprised at the denial recommendation. The Staff analysis, particularly as it relates to the need for industrial lands within the Silverdale UGA, is very confusing. On the one hand, the Staff analysis postulates that the proposal would remove up to 50% of the vacant industrial land located within the UGA as designated in the 2016 Plan. On the other hand, Staff notes that the 2016 plan considered this property as "developed," and accordingly it was not "vacant land" within the earlier Comprehensive Plan. The proper analysis would appear to be that this site-specific proposal would not have any impact on the availability of vacant industrial lands inventoried under the 2016 Comprehensive Plan.

Similarly, the employment and housing discussion last evening during the public hearing is difficult to reconcile with the Staff analysis. For example, the Staff Report shows that the 2016 comprehensive plan employment capacity for the UGA was 3.8% below the adopted employment target. Adding this Comprehensive Plan Amendment would take it, by Staff's analysis, to 1% above the adopted employment target. In other words, the proposed amendment would lead to a better fit with employment targets. In any case, the analysis also finds that the adopted amendment will keep target goals within the County's 5% margin of error.

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Most disturbing is the clear absence of any market analysis in the Staff's recommendations. The Staff seems to be setting Silverdale up for a follow-on planning disaster to what occurred in the West Hill area of Silverdale following Comprehensive Plan changes in the mid-2000's. You may recall that the area was designated to be a "Little Ballard" and accordingly was zoned for six story buildings. There was no market for this zoning. Instead, the area fell into urban decay. This gateway to Silverdale was littered with boarded up houses and a property with a collapsed roof. As a member of the Central Kitsap Community Council, I worked for years to try to clean up this mess – a mess that would have been cleared up by responsible development in this corridor with appropriate zoning.


The Staff recommendation is taking us down this same path. As I noted during my testimony last evening, I have attempted to market some of the property at issue for years. Despite available industrial infrastructure on the property and its location abutting Dickey Road, there were no takers. How likely is it that, given the lack of a market for this "shovel ready" industrial property, we are going to have an industrial developer pay for the cost of reclaiming the mine?

I also note that I raised my three children for 15 years within a few lots of the Dickey Pit mining area. During this time we explored the incredible beauty of Little Anderson Creek near its headwaters on this property. These lands are very important environmental areas. Developing this property as a neighborhood will help protect this area relative to the environmental challenges of either industrial use or a fallow mine.

Finally, I would note that our house on Andrea Lane was directly in the flight path of Apex Airport. For the 15 years we lived on the property, we frankly enjoyed the opportunity watch the occasional hobby pilot take off from Apex Airport. The "noise" concern from this hobby airport is inconsistent with the facts that I observed while I lived in this area.

Kitsap County has an opportunity to convert an abandoned mine into a neighborhood community treasure. I ask the Planning Commission to recommend approval to the County Commissioners. I hope we can avoid the mistakes experienced in planning for the West Hill area of Silverdale, and take into account market forces by adopting this amendment.

Very truly yours,



RICHARD B. SHATTUCK

RBS/jef

Peter Best

From: Steve Segó <steve@watermanmp.com>
Sent: Wednesday, January 22, 2020 10:04 PM
To: Amanda Walston; Peter Best
Cc: Levi Holmes
Subject: Site-Specific Comprehensive Plan Amendment application CPA-18-00495
Attachments: PastedGraphic-5.tiff

I'm writing to extend my support of the proposed Site-Specific Comprehensive Plan Amendment application for the "Dickey Pit" site. I spoke at the Public Hearing yesterday and wanted to follow-up with written testimony to make certain that the Planning Commission members in attendance are able to fully consider my strong recommendation of support for this proposed Amendment.

As I shared, I have been engaged in wetland and riparian habitat restoration and consolidated mitigation projects for nearly 20 years, having partnered in the establishment of the first privately sponsored Mitigation Bank in Washington in 2004. Subsequently I helped design, finance, construct and permit large-scale mitigation banks and projects throughout the Pacific NW and Alaska. Having relocated to my native home here in Kitsap nearly ten years ago, I began to identify prospective wetland and habitat restoration sites that could support a regional mitigation bank project, which is just now in the pre-permitting draft Prospectus stage.

Identifying prospective sites in Kitsap is challenging, considering the constraints of the Kitsap peninsula due to short stream systems and rapid development. While exploring these options I learned about the Dickey site which include the headwaters of two salmonid streams, Little Anderson Creek which drains into the Hood Canal and Strawberry Creek which drains into Dyes Inlet. Much attention is paid to the mouth and lower reaches of vital salmon bearing stream systems, although the upper reaches and headwater sources for these streams are of potentially greater significance and value to the riparian habitat than downstream improvements. Healthy and functioning headwater sources, properly restored with habitat and providing hydrologic storage and shading to reduce water temperature, are critical to the sustenance of a functioning fish bearing stream.

The current conditions of the Dickey site, although conforming to regulatory provisions and compliant with the permitted mining activities, lack the potential benefits and components that would support a healthy stream system. Should the proponents of this Amendment be permitted to develop a residential project on this site, updated provisions for buffer setbacks, and the opportunity to enhance the headwaters feature with habitat that supports shading and improved stream functions are possible.

As other speakers confirmed, there has been no demand for industrial development for this site for decades, including the landowner who invested in infrastructure to provide a turn-key project for a prospective user. This site was designated for industrial use decades ago, and although planners may have imagined that demand for this use would materialize, the conclusion is clear that this is very unlikely. Without some land use action that would trigger some development activity that could initiate the restoration of these vital headwaters functions all of the effort and investment downstream in both streams will suffer and fall short of efforts to restore salmon habitat and measured seasonal drainage through hydrologic functions upstream.

I've been monitoring the process of this proposed Comprehensive Plan Amendment for the last 18 months, expecting full staff support recommending approval. All of the issues have been addressed by the applicants, and staff conveyed general support when I inquired. The very unexpected turnaround within the last two weeks resulting in the staff member assigned to this review is confounding and, considering the excessive narrative to support the recommendation of denial which is well beyond the purview of this review process, of concern.

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I'm confident that, should the members of the Planning Commission carefully review the incongruous arguments provided by staff relative to the detailed data provided by the applicant and the substantiated comments of community members and experts in long-term planning and land use, this proposed amendment will find much support and a recommendation for approval.

In the end, the Planning Commission's decision will be founded on the provisions of the existing Comprehensive Plan and the necessary balancing of land use that supports the best interests of Kitsap County as relates to residential growth, economic vitality and responsible planning for our local communities. Environmental benefits and opportunities for habitat protection and improvements may not be the primary priority for land use planning but, should this proposed amendment be reasonable and supportable per the applicants submittal, the added value of the potential ecological benefits will provide much to our local ecosystem for generations to come.

Best regards,

Steve Sego

(206)661-2401

www.watermanmp.com





January 22, 2020

Kitsap County Planning Commission
Attention: Amanda Walston, Clerk
619 Division Street
Port Orchard, WA 98366

RE: Critical Area Testimony for the Site-Specific Comprehensive Plan Amendment application CPA-18-00495 for the Port Orchard Sand and Gravel property (commonly known as Dickey Pit).

I attended and spoke at the public hearing on January 21, 2020 to provide testimony in support of the proposed Dickey Pit rezone. My testimony specifically addressed the onsite critical areas and how the rezone will be a benefit to the critical areas. I created a synopsis of my testimony to beef it up and to include additional comments. I have worked as a wetland biologist for nearly 30 years including 23 years working for Bob Wiltermood. Most of my work has been conducted in Kitsap County and my experience and knowledge of the county critical areas lead to my employment with Ecological Land Services, Inc (ELS) beginning in May 2013. ELS conducted the wetland reconnaissance and delineation of the onsite wetlands in 2019.

- The critical areas include four wetlands located at the north end (Wetland A), near the southeast corner (Wetlands B and C), and on the eastern portion of the pit (Wetland D). There are three streams that represent the headwaters for Strawberry Creek (southeast corner), for a small tributary to Strawberry Creek (north end), and for Little Anderson Creek (southwest corner). Wetland A is associated with the small tributary beginning at the north end.
- These critical areas are adjacent to areas that were historically part of the active pit and in some cases, they were impacted by unintended erosion of steep slopes or clearing of native vegetation. The buffer areas are composed of non-native plant species that do not provide adequate protection for the critical areas (Wetlands A, B, and C).
- The pit is not currently mined but the mining activities at the south end of Dickey Pit continue to utilize the sediment ponds through the middle and at the southwest corner. The permits that allow the use of these ponds are updated regularly so that there is continued, uninterrupted use. Therefore, if the rezone is not approved, the pit will remain in its current condition and reclamation will be put off for many years. The current condition of the buffers will not change and provide low buffer function for the identified wetlands.
- Developing a site residentially requires specific buffers for critical areas that range from 40 to 300 for moderate and high intensity land uses. By rezoning the Dickey Pit to residential, buffers can be applied to each of the wetlands and provide greater protection than is currently provided by the non-native vegetation.

Comment 2.26

- Rezoning Dickey Pit will facilitate the reclamation of the pit, which will include enhancement and restoration of the wetlands and buffers. There will be significant beneficial affects downstream of the Dickey Pit through the enhancement of the areas that drain into Strawberry and Little Anderson Creeks both of which are considered fish bearing streams. Additional habitat improvement onsite is also expected and maintenance of corridors between habitat areas will also be a result of the reclamation.

Thank you for your consideration in this matter. Please approve the rezone of Dickey Pit because it will improve conditions for the on and offsite critical areas.

Sincerely,

A handwritten signature in blue ink that reads "Joanne Bartlett". The signature is written in a cursive style.

Joanne Bartlett
Wetland Biologist

cc: Peter Best, Kitsap County DCD
The JWJ Group-Levi Holmes/John Johnson

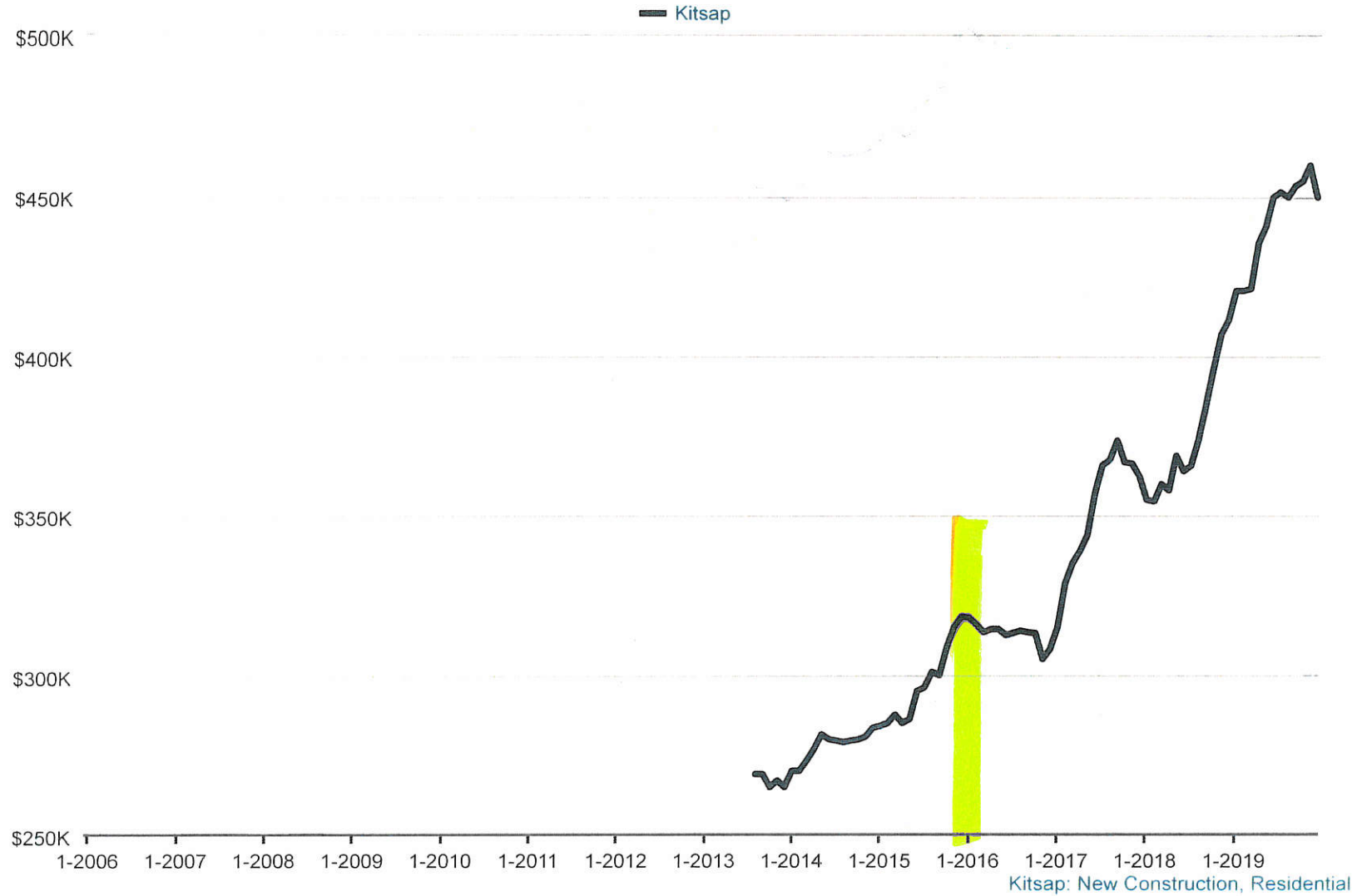
Comment 2.28

Nathan Johnson
G Anderson Group LLC

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nate.johnson@waubayholdings.com
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Median Sales Price



Each data point is 12 months of activity. Data is from January 19, 2020.

All data from Northwest Multiple Listing Service®. InfoSparks © 2020 ShowingTime.

Nathan Johnson
G Anderson Group LLC

Comment 2.28

Office: (360) 373-2343

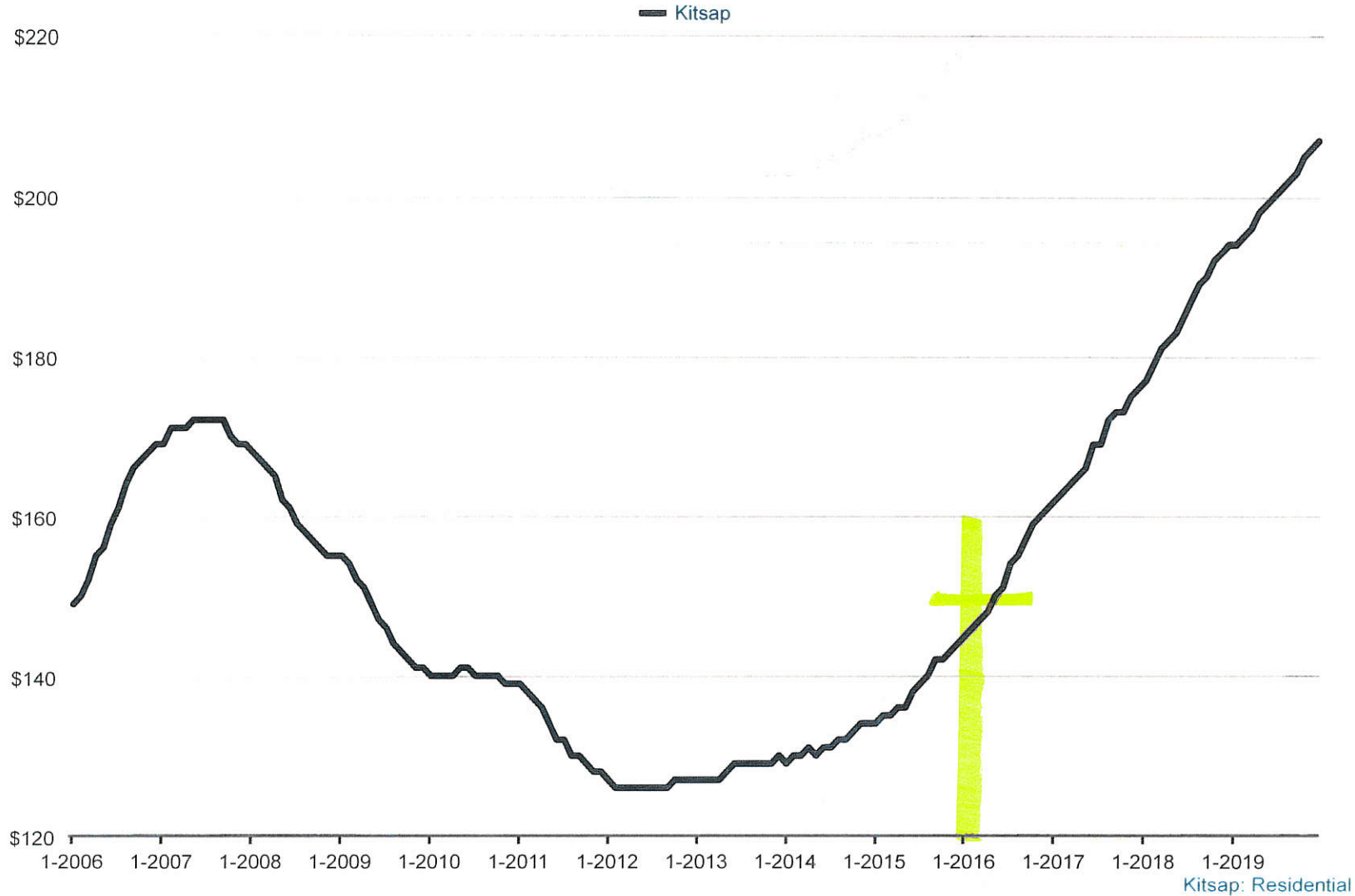
Cell: (360) 710-0480

nate.johnson@waubayholdings.com

www.gandersongroup.com



Median Price Per Square Foot



Each data point is 12 months of activity. Data is from January 19, 2020.

All data from Northwest Multiple Listing Service®. InfoSparks © 2020 ShowingTime.

January 22, 2020

Kitsap County Planning Commission

Delivered via email to:

Amanda Walston, Clerk of the Planning Commission at awalston@co.kitsap.wa.us

Peter Best, Senior Planner at pbest@co.kitsap.wa.us

CompPlan@co.kitsap.wa.us

Re: Annual Comprehensive Plan Amendment Process for 2019: Dickey Pit Site-Specific Application (CPA 18-00495)

Honorable Members of the Planning Commission:

I am writing on behalf of the Applicant, JWJ Group, LLC, in support of its application for a Site-Specific Comprehensive Plan and Zoning Amendment (CPA-18-00495) for the 138-acre Port Orchard Sand and Gravel property, commonly known as Dickey Pit (“the Amendment”). The proposed Amendment would redesignate and rezone the property from an Urban Industrial designation (with a Mineral Resource Overlay) to an Urban Low-Intensity Commercial and Urban Low-Density Residential designation.

On January 10, 2020, the Department of Community Development issued a Staff Report and Recommendation recommending denial of the Amendment. Such a recommendation of denial is inconsistent with the evidence in the record and applicable law and should be rejected by the Planning Commission.

As a preliminary matter, the Applicant has prepared and submitted a thorough and detailed matrix that responds to the Staff Report’s recommendation of denial. We ask that the Planning Commission review this matrix as it demonstrates why the Staff Report and evidence in the record do not support denial of the Amendment. In my opinion, based on experience in handling Growth Management Hearings Board appeals of comprehensive plan and zoning amendments, and more than 30 years of experience as a land use attorney, I think the Growth Management Hearings Board would likely find that the proposed Amendment is in compliance with the Growth Management Act and the provisions of the Kitsap County Code in Chapter 21.08 governing such amendments, in the unlikely event of an appeal.

In terms of specific errors in the Staff Report’s recommendation of denial, two stand out:

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First, contrary to the Staff Report and Recommendation, the proposed Amendment meets the criteria in KCC 21.08.070(A)(1) and (2) that require demonstration of (a) how “circumstances related to the proposed amendment and/or the area in which the property affected by the proposed amendment is located have substantially changed since the adoption of the Comprehensive Plan or applicable development regulations” and (b) how “assumptions upon which the Comprehensive Plan is based are no longer valid or there is new information which was not considered during the adoption.” Consistent with these criteria, and as demonstrated by the evidence, the circumstances and assumptions justifying the Amendment have substantially changed since adoption of the 2016 Comprehensive Plan. They include the following:

- (1) The mine on the property has been depleted and it is now ready for reclamation and redevelopment. In the 2016 Comprehensive Plan, this mine was already considered “developed” and thus was not counted in the Land Capacity Analysis as available industrial land. It now is no longer considered “developed” and thus needs to be added to the available inventory and reconsidered. It is appropriate to do so now, as part of the proposed Amendment. This is a significant change in circumstances since 2016.
- (2) Kitsap County and the Region are experiencing a housing crisis. As the evidence demonstrates, there is a great need and demand for housing. This also is a substantially changed condition since 2016.
- (3) The industrial demand for this area is minimal to non-existent.

These and other substantially changed conditions since the 2016 Comprehensive Plan are supported by evidence in the record and justify the proposed Amendment.

As further justification for its recommendation of denial, the Staff Report also alleges that the proposed Amendment would be incompatible with surrounding uses—in particular, the airport, mines and industrial uses. This is not a legitimate basis for denial of the proposed non-project Amendment. Not only is there no evidence to support a claim of incompatibility with surrounding uses at the non-project level, these concerns can and should be addressed and mitigated during project level SEPA review. In fact, Staff concerns regarding compatibility, utilities, analysis of population, jobs and density, the environment, capital facilities, noise, transportation, and public services, among other potential concerns, were all addressed during SEPA review for the proposed Amendment and none were considered significant enough to require mitigation in the DNS issued for the proposed non-project Amendment.

Thus, these issues of compatibility and any other concerns do not support denial of the non-project Amendment but should instead be addressed during project review, including any

January 22, 2020


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project-level SEPA review. It was thus error for the Staff Report to justify denial based on such concerns.

On behalf of the Applicant, we ask that the Planning Commission reject the recommendation of denial and instead recommend approval of the Amendment, as proposed.

Thank you for your consideration of these comments.

Law Office of Charles Maduell, PLLC

A handwritten signature in blue ink, appearing to read 'C. Maduell', with a long, wavy horizontal line extending to the right.

Charles Maduell



PHONE (360) 598-3311
Fax (360) 598-6295
<http://www.suquamish.nsn.us>

THE SUQUAMISH TRIBE

PO Box 498 Suquamish, WA 98392-0498

January 22, 2020

Peter Best, Long Range Planner
614 Division Street, Department of Community Development
Port Orchard, WA 98366

Re: Annual Comprehensive Plan Amendment Process for 2019 Dickey Pit Site-Specific Application (CPA 18-00495)

Mr. Best,

This letter transmits the Suquamish Tribe's (Tribe) comments on the Annual Comprehensive Plan Amendment Process for 2019 Dickey Pit Site-Specific Application (CPA 18-00495).

The property is approximately 138 acres currently zoned industrial with a Mineral Resource Overlay. The applicant is requesting to rezone the property to approximately 78 acres of Neighborhood Commercial and approximately 60 acres of Urban Low Residential. Surrounding land use is primarily industrial.

The Tribe asserts that Kitsap County has not fully evaluated or field verified the environmental constraints on this property before proposing to make a significant zoning change. Although paper documentation may state that one or more streams onsite are non-fish bearing, there is no field verification to support this conclusion. The Tribe requests that all streams typing be verified by field observation by WDFW and the Tribe. Applicants (and others) commonly rely on the Department of Natural Resources (DNR) hydro layer and the National Wetlands Inventory (NWI) Maps. These resources are only to be used as preliminary information and should always be field verified before making regulatory decisions.

To prevent continued degradation of wetlands and streams, wetland function needs to be maintained throughout the Anderson and Strawberry Creek watershed that includes prevention of additional wetland filling and adding impervious surfaces associated with the residential and commercial development. It is equally important to ensure that the most up to date stormwater protection is implemented on any proposed development within the watershed to eliminate further impacts to wetlands and streams. Several of the wetlands onsite are headwater wetlands. Wetlands in headwater areas provide sediment control, pollutant control, and floodwater control. In addition to maintaining base flows, headwater systems provide cool water and essential nutrient input to enhance fish and wildlife survival downstream. Properly functioning headwaters also provide essential food supply for salmon including both living organisms like insects and decaying organic matter. The Tribe requests that with any proposed development onsite protect these wetland and headwater systems (with adequate buffers) and remain connected to downstream habitat. Wetland and stream impacts should be avoided altogether.

Mr. Best
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The County issued a Determination of Non Significance on November 15, 2019. The Tribe disagrees with the SEPA Determination of Non-Significance. An urban development of 78 acres of 10-30DU/Ac and 60 acres of 5-9 DU/Ac will result in significant urbanization of the upper Little Anderson Creek and Strawberry Creek watersheds. Little Anderson Creek flows into Hood Canal and contains ESA listed Hood Canal Summer Chum and steelhead and Strawberry Creek flows to Dyes Inlet and contains ESA listed steelhead. The Washington Supreme Court has concluded that SEPA mandates preparation of an EIS when significant adverse impacts on the environment are “ ‘probable’, not when they are ‘inevitable’”. The absence of specific development plans should not be conclusive of whether adverse environmental impact is likely.”¹ In addition, the court held that UGA designations under the Growth Management Act (GMA) will result in intensive urban level development and potential adverse environmental impacts are presumed.² The impacts resulting from large scale urban development need to be thoroughly investigated prior to permit issuance.

The Tribe, however, concurs with the Kitsap County Department of Community Development staff report, dated 1/10/2020, rational that supports *denial* of the request for the following reasons:

1. 2016 Comprehensive Plan, Countywide Planning Policies, and Multi-County Planning Policies seek to absorb growth into designated centers. The proposed rezone redirects growth out of the Silverdale Regional Growth Center.
2. The proposed rezone will potentially locate a large residential development adjacent to a general aviation airport, an active surface mine and other industrial zoned properties with uses that are incompatible.
3. Population and employment assumptions have not substantially changed since the adoption of the 2016 Comprehensive Plan amendment. Any adjustments need to be considered during the Buildable Lands Report and Land Capacity analysis associated with the next 8-yr Comprehensive Plan review update. To do so without appropriate need would violate the Growth Management Act.

It is also the Tribe’s understanding that this amendment was proposed without notification to at least one of the adjacent property owners affected (see comments provided by Brett Caswell provided on January 6, 2020). For these reasons stated above, we request that the proposed amendment be denied and removed from the docket.

Thank you for the opportunity to comment on the above referenced material. Please keep us informed of project status and any relevant project related actions. If you have questions regarding the comments stated above please don’t hesitate to contact me at 360-394-8447.

Sincerely,



Alison O’Sullivan
Senior Biologist, Environmental Program

¹ RCW 43.21C.031; *King County v. Wash. State Boundary Review Bd.*, 122 Wn.2d 648, 664, 860 P.2d 1024 (1993)

² *King County*, 122 Wn.2d at 665-667

Site-Specific Comprehensive Plan Amendment application CPA-18-00495

Members of the Planning Commission,

I would like to take the opportunity to address conditions of approval, respond to concerns raised by members of the public, clarify misleading information in the staff report, and correct some items that are untrue.

All parcels should be rezoned as proposed by the applicant with the following condition:

1. The Mineral Resource Overlay (MRO) on the subject parcels shall be removed from the Kitsap County Land Use Map and the Kitsap County Zoning Map immediately after the Department of Natural Resources closes the surface mining permit for the applicable parcels associated with this amendment due to either (1) the completion of mine reclamation consistent with the active permit(s) or (2) the approval of a new reclamation plan consistent with KCC 17.170.065 and transfer of jurisdiction over reclamation from the Washington State Department of Natural Resources to Kitsap County under RCW 78.44.390.

The Following Staff proposed conditions of approval are not appropriate:

1. Condition 1 maintains two of the parcels as industrial zoning. This is unnecessary as the compatibility concern is irrelevant as mentioned later in this letter.
2. Condition 2
 - a) is not relevant as Central Kitsap Fire and Rescue District (CKFR) offers service agreements for parcels outside their district and is already in the process of annexing this property into the district. In the unlikely event the annexation is unsuccessful, then the parcel can continue to utilize an outside service area agreement.
 - b) Addresses project specific requirements and should not be a condition of approval for a non-project action. It is unreasonable and onerous to have an applicant prepare all of the documents necessary for a project specific submittal with no guarantee the appropriate zoning will remain in effect.

Traffic:

1. **The SEPA determination addressed concerns with capital facilities (including traffic) at a broad level and deemed mitigation unnecessary for a non-project action.** If this rezone is approved then a project specific application will be submitted. At that time, a traffic study will be completed, reviewed by the county, and necessary improvements will be conditioned on the project.
2. **In addition to potential offsite improvements conditioned in the application, the applicant will be required to pay [impact fees](#) for every unit of housing or square foot**

of commercial. The traffic portion of these fees are used to make off site improvements in the surrounding area.

3. **Improvements are already scheduled for the area.** The Six-year TIP includes improvements to the following roads near the proposed site:
 - Intersection of Anderson Hill Road, Provost Road, and Old Frontier Road
 - Intersection of Greaves Way NW and Old Frontier Road NW
 - Intersection of Anderson hill Road and Apex Airport Road

A list of all traffic projects currently funded and scheduled can be found on the [Public Works website](#).

4. **Any development of the site, regardless of zoning, will create new impacts that are addressed and mitigated (if necessary) at the project specific level and I would encourage anyone with concerns to engage in the process at that time.**
5. **The Capital Facility providers, including Public Works, deemed any potential concerns can be addressed at the project specific level.**

Density:

1. **Application review must be consistent with the assumptions of the Comprehensive Plan.** The Comprehensive Plan (CP) and Land Capacity Analysis (LCA) utilize calculations for residential and commercial density. These calculations must be consistently used with any change to the CP otherwise it will be impossible to determine if the application complies with the CP. For instance, although residential uses are allowed in the Neighborhood Commercial zone, the LCA does not attribute any density. This is also the case for the Commercial zone which allows for 10-30 units per acre.
2. **The Staff report is misleading when it states the maximum density of the site could be 2904 units.** Maximum density is calculated based on gross acreage however projects are never developed to their max density for instance:
 - The Silverdale Regional Center is approximately 660 acres with a max density of 60 units per acre. Technically that means it could be developed at **39,600 units**.
 - Silverthorn is a Urban Low zoned (5-9 units per acre) residential project located off of Dickey Road with little to no development obstacles such as steep slopes, critical areas, or major power easements. The max density allowed is 9 units per acre (90 units) however it was developed at 4.7 units per acre (47 units).
 - **Sterling Hills Estates** is an Urban Low zoned (5-9 units per acre) residential project located off of Apex Airport road with large critical areas. The max density allowed is 9 units per acre (333 units) however it was developed at **2 units per acre (76 units)**.
 - The Land Capacity Analysis calculates approximately **56 total net developable acres**. This is due to steep slopes, critical areas, power

easements, etc. Due to these site conditions, it is infeasible to develop the site at max densities.

- Using assumptions that the site is built out at **max density does not account for any commercial uses** which is a large portion of the proposal.

Access to Willamette Meridian

The application proposes to connect to Willamette Meridian for emergency vehicle access only. The application has always proposed access for this purpose. The connecting road would be gated at the property line, remain locked, and be keyed appropriately as deemed necessary by the fire marshal's office. I understand the County may want potential public road connections through the site however it cannot condition this project's approval based on dedicating right away across adjacent parcels that the applicant does not own.

Compatibility

1. The neighboring mine to the south has voiced concerns over compatibility especially as it relates to their plans for future expansion of 40 acres to the west. The following items should be considered:
 - **The 40 acre expansion is bordered entirely by Urban Low zoning on the south and rural residential on the west.** This proposal would only border approximately 50% of the expansion's north boundary. Furthermore, the **Kitsap County Critical Area Map, Washington Department of Fish and Wildlife map, and Salomon Scape Map identify a fish barring stream, tributaries, and headwaters on the majority of the north half of the 40 acre expansion** (see attachment). The required buffers for these features will provide a large buffer from the proposed rezone.
 - **The proposed 40 acre expansion does not appear to have been issued any permits, completed any public meetings, or provided any studies regarding the proposed expansion.** It is entirely possible that the plans for future expansion are over optimistic at best and likely infeasible.
2. **Apex Airport is a limited use airport, mostly for hobby use, and is unlikely to expand** for the following reasons:
 - It is a private/public airport. The landing strip is public but there is no access for the public to public roads, transportation, or amenities.
 - There are not services available at the airport such as fuel, food, or supplies.
 - The zoning surrounding the airport is all Rural Residential with the exception of the Industrial zoned. Rural residential only allows the following commercial uses which do not support the airport as an economic center:
 - i. Home business
 - ii. Day Care
 - iii. Kennel
 - iv. Nursery
 - v. Temporary Office
 - vi. Cemetery

- vii. Contractor storage yard
- viii. Storage
- ix. Top soil production

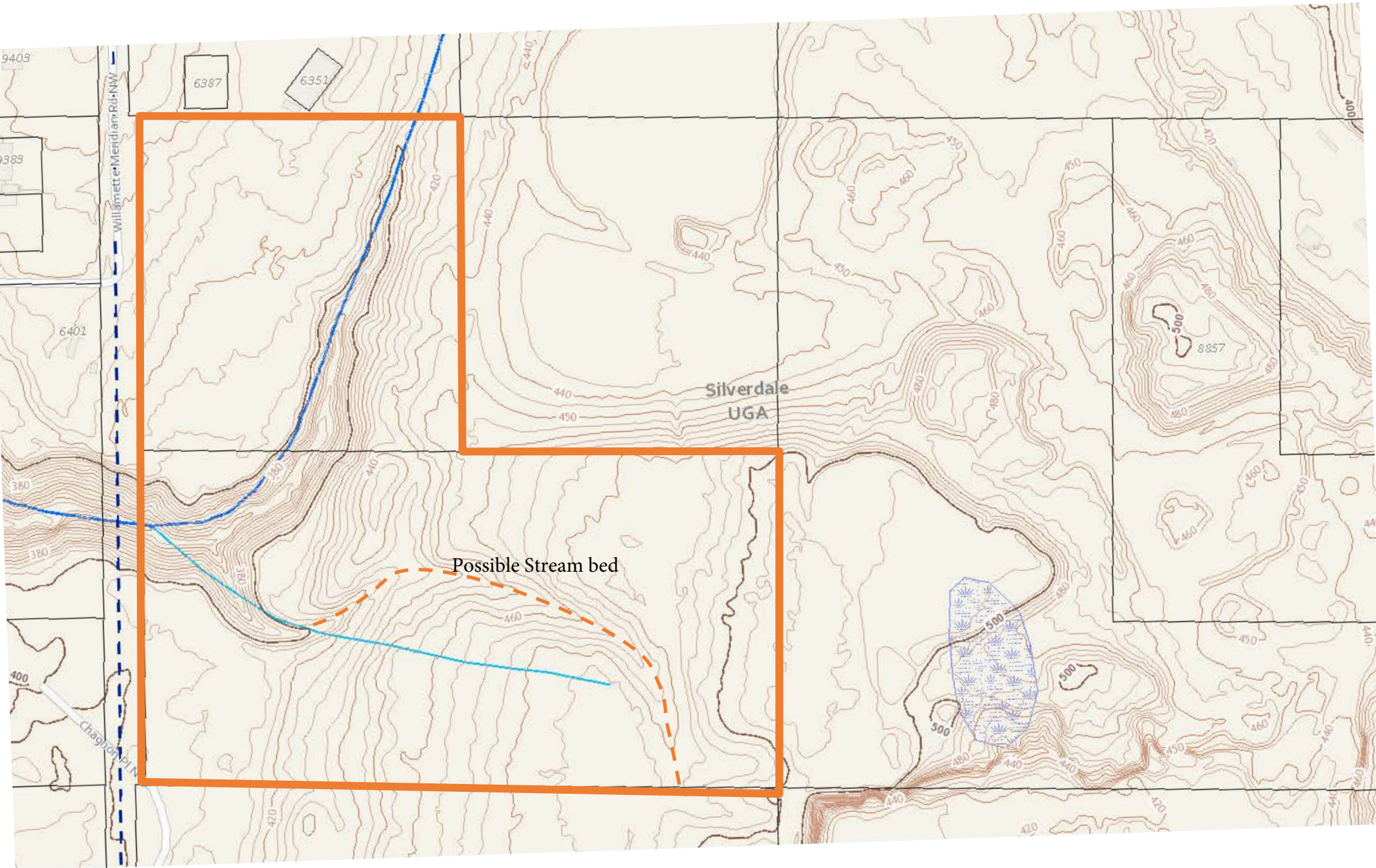
3. Compatibility concerns, if any, can be addressed at the project specific level.

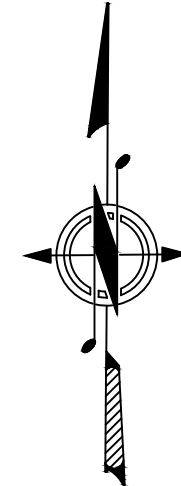
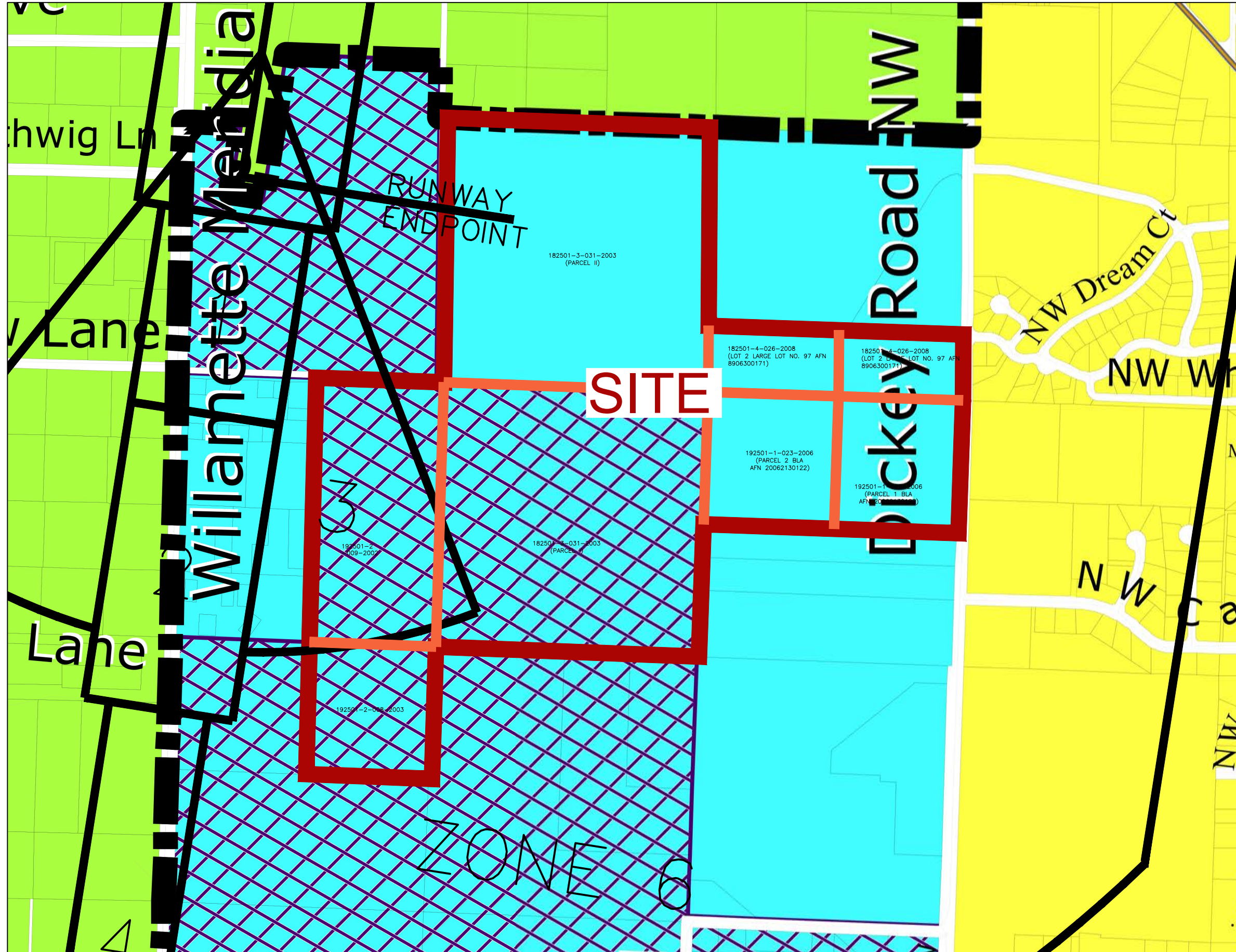
- A large majority of the bordering industrial parcels have critical areas that will naturally buffer them from the proposed rezone.
- Kitsap County Code (KCC) has existing provisions in place to mitigate surrounding incompatible uses. Mitigation requirements fall as much on the development of the subject parcels as they do the potential expansion of neighboring uses.
 - i. KCC 17.500.025 requires that a minimum of 15% of the site be landscaped.
 - ii. KCC 17.500.027 directs buffer sizes based on neighboring uses and zoning.
 - iii. A standard subdivision would be required to provide a Solid Screening Buffer (KCC 17.500.027.B) which requires a minimum width of 50-feet when residential zoning abuts industrial zoning or uses.
 - iv. The Director has the authority to increase this requirement depending on the proposed use of the site and adjacent zones and/or uses (KCC 17.500.027).

Thank you for your consideration of this proposal.

Best Regards,

Levi Holmes, Project Applicant





SCALE: 1"=500'



Scale in Feet



Kitsap County
Annual Comprehensive Plan Amendment Process for 2018



Site-Specific Amendment Application
Review Criteria Narrative

Instructions: This document must be completed and submitted with your site-specific Comprehensive Plan amendment application form.

Introduction

Each proposed amendment to the Comprehensive Plan must demonstrate how the review criteria from Kitsap County Code (KCC 21.08.070) have been met. These criteria are used by the Department of Community Development in developing its recommendation, the Planning Commission in reaching its recommendation, and the Board of County Commissioners in making its decision. The following are the review criteria applicable to site-specific amendments rephrased in the form of questions.

Review Criteria: General

All applicants must answer the questions in this section.

1. How have the circumstances related to the proposed amendment and/or the area in which the property affected by the proposed amendment is located substantially changed since the adoption of the Comprehensive Plan or applicable development regulations?

The historical use of the site has been a gravel and sand mining operation and the resource has been depleted. The site reclamation process is not yet complete, and the underutilized property will be available for redevelopment upon the completion of this process. The reclamation of the mine into productive and useable parcels within the Urban Growth Area will be coordinated with the final grading as provided for in Kitsap County Code Section 17.170.065. The proposal was derived in coordination with input from DCD staff meetings and suggestions and includes the former mining property and adjacent industrial properties that provide direct access to Dicky Road NW.

Apart from the mining activities, this specific industrial area has seen minimal Industrial development in the past several decades. The demand for industrial development in the Silverdale Urban Growth Area has been far below the projected needs and a significant growth factor for industrial development is being directed to other parts of the County and to the local Cities.

Presently, Kitsap County is experiencing a shortage of housing supply to be able to meet the needs of the population growth and the local economy. Commercial and residential demand will continue to increase with the relocation of Harrison Hospital and the ancillary medical support business that will be occupying the Silverdale UGA in the near future. None of this medical driven demand was calculated as part of the previous UGA study and has significantly altered the makeup of economic demand factors and may explain the waning need for additional industrial capacity within the Silverdale UGA.

Reclamation of this former mining land and conversion to residential and commercial uses would best serve the growing needs of the Silverdale Urban Growth area and the community.

2. How are the assumptions upon which the Comprehensive Plan is based no longer valid, or is there new information available which was not considered during the adoption of, or during the last annual amendment to, the Comprehensive Plan or development regulations?

The primary assumption that has changed is that the gravel mine has depleted the resource and can now be redeveloped. Much of the subject property has been an active gravel mine since before the Growth Management Act. In all of the past comprehensive plan updates the majority of the property was assumed to be an active mine and therefore it was treated as developed property. Now that the mining activity is complete, an appropriate use for the property must be considered. Furthermore, market conditions for industrial property have slowed considerably and residential units are in short supply. The addition of the fast ferry system is also bringing increased population to Kitsap County which in turn creates additional demands for residential units as well as complimentary commercial services.

3. How is the requested amendment in the public interest and the proposal consistent with the Kitsap County Comprehensive Plan?

The proposal is consistent with the Goals and Policies of the Kitsap County Comprehensive Plan. The designation of former under-utilized Mineral Resource Land to Residential Urban Low and Neighborhood Commercial would not significantly impact the employment growth capacity of the Silverdale UGA or Countywide UGA as both remain within +/-5% of the targets.

The designation of the additional residential land within the UGA would accomplish the goal to direct new residential units to Urban Growth Areas and would further minimize the rural/urban residential split.

Current residentially zoned property is in short supply and many of the sites available have significant development challenges. Kitsap County continues to have less than two months' supply of housing units which is considered a seller's market and leads to increased housing costs. By adding the residential aspect of this proposal, it will help add to the housing supply which in turn balance the market.

The Neighborhood Commercial zoning will allow for a walkable mixed-use neighborhood with amenities nearby for both the residents of the subject property as well as the numerous residential units located in the vicinity. Currently there is a limited supply of commercial property able to serve the residents who reside west of Silverdale. This commercial property will allow opportunity to serve residents without the need to travel long distances.

Additional Review Criteria: All Site-specific Amendments

All applicants must answer the questions in this section.

4. How will the proposed amendment meet concurrency requirements for transportation, sewer and water, and not result in significant adverse impacts on adopted level of service standards for other public facilities and services, such as police, fire and emergency medical services, park services, and general government services? Explain or attach documentation.

The proposed amendment will continue to meet the goal of being within +/-5% of the population and employment targets for both the Silverdale UGA and Countywide UGA. This will be a minimal change that the capital facilities providers will be able to account for. Furthermore, the applicant has correspondence with Kitsap County Public Works Sewer and Roads division, Silverdale Water District, Kitsap Transit, Central Kitsap School District, and Central Kitsap Fire District in which all providers have confirmed this proposed amendment will not create any adverse impacts that cannot be mitigated at the time of a project specific application.

5. How is the proposed amendment consistent with the balance of the goals, policies and objectives of the Kitsap County Comprehensive Plan and reflect the local circumstances of the county?

As previously discussed, job capacity will still be in excess of the planning requirements. The redesignation and rezone to Urban Low Residential will allow construction of housing that will comply with the Comprehensive plan goals and policies for provision of housing for residents of Kitsap County. There is a strong need for residential housing in the County, especially in the Silverdale UGA. Other residential developments in this neighborhood have experienced high sales rates indicating the desirability of residential housing in the area.

The Comprehensive Plan is considered balanced if the growth capacity is within +/-5% of the target. This proposal will maintain that balance and therefore will continue to be consistent with the goals, policies, and objectives of the Kitsap County Comprehensive Plan.

Furthermore, the proposed amendment meets the following specific goals, policies, and objectives:

- Land Use Goal 1. Focus current and future planning on infill and redevelopment of existing Urban Growth Areas
- Land use policy 2. Support innovative, high quality infill development and redevelopment in existing developed areas within the Urban Growth Areas
- Land Use Policy 7. Facilitate mixed use development in commercial designations
- Land Use Goal 3. Support more dense residential areas with access to transportation, urban amenities, goods and services, physical activity and healthy foods.
- Land Use Policy 16. Promote housing preservation and development in areas that are already well-served by schools, public transportation and commercial facilities, and have adequate infrastructure to support alternative modes of transportation.
- Land Use Policy 29. Through application of Growth Management Act goals, increase density in urban areas and limit sprawl in rural lands.
- Economic Development Policy 9. Encourage mixed use developments within commercial districts that will enhance the visual, economic, and environmental quality of these areas and improve the transition between commercial and residential districts.
- Transportation Policy 1. Coordinate a “safe routes to schools” program with local school districts and prioritize non-motorized improvements and connectivity around schools.
- Transportation Policy 3. Continue to require sidewalks on roads when development occurs within Urban Growth Areas.
- Silverdale Policy 1. Monitor land supply over time to ensure a continued adequate supply of residential, commercial and industrial designated land to meet Silverdale’s population and employment targets and to meet the needs of unexpected growth.

- Silverdale Policy 14. Encourage full use and development of designated commercial and industrial areas prior to expanding those areas. Promote revitalization within existing developed areas to take advantage of the investment in existing buildings and infrastructure.

6. How is the subject parcel(s) suitable for the requested land use designation based upon, but not limited to, access, provision of utilities, and consistency with existing and planned uses, environmental constraints and compatibility with the neighborhood?

The underutilized former mining site is suitable for much needed residential development with acceptable topography, access, utilities, and an existing disturbed land and soil features that will preserve undisturbed natural environments elsewhere within the Urban Growth Area.

The site possesses many amenities that will be desirable for a residential development. The site is served by some utilities at the present time and as new development is built the appropriate utility extensions will be constructed. Access to the site is available to Willamette Meridian Road and Dickey Road and the approval of the residential designation would facilitate much needed emergency services east-west cross connections between those major north-south corridors.

Existing industrial use nearby is minor in use and the negative impacts of any possible adjacency of any residential zone is evaluated and mitigated as part of the land use subdivision development and entitlement process. Numerous examples of residential development adjacent to industrially zoned parcels have been approved within Kitsap County and exist as a model of the land use entitlement process.

Within Table KCC 17.420.060 the Footnotes for Tables, Footnote #27 was created to address this very issue and has successfully mitigated the potential adjacency issues. Any new development would equally be required to comply with Footnote #27:

KCC 17.420.060 Footnote #27. As approved by the director, wherever an industrial zone abuts a residential zone, a fifty-foot screening buffer area shall be provided. This screening buffer is intended to reduce impacts to abutting residential uses such as noise, light, odors, dust and structure bulk. No structures, open storage, or parking shall be allowed within this area. The director shall only approve screening buffers that improve the compatibility between the proposed use and the residential zone. The director may reduce this buffer to a minimum of twenty-five-foot width only when based upon a site-specific determination that topography, berming or other screening features will effectively screen industrial activities from the residential zone. Conversely, based upon a similar site-specific determination, the director may increase the buffer width from fifty feet to ensure adequate buffering and compatibility between uses Apex Airport is nearby and other Industrial Zoned areas exist adjacent the site on the other boundaries. Residential uses of the site can be compatible with the adjoining Industrial Zone uses by provision of buffers that occur naturally due to topography or critical area buffers.

7. How does the proposed amendment not materially affect the land uses and growth projections which are the basis for the Comprehensive Plan, and reflect local circumstances in the county?

The job capacity requirements of the Silverdale UGA have been discussed and shown to remain adequate for the plan after the redesignation of these parcels. The residential development potential of the site is not large in comparison to the overall area provided and required for residential zones and will not materially alter the balance of land uses in the County. The Comprehensive Plan will remain in balance based upon the growth to target ratio of +/-5%.

8. How does the proposed amendment not materially affect the adequacy or availability of urban facilities and services to the immediate area or the overall area of the urban growth area?

As discussed previously, there are no capital facility impacts that cannot be addressed at the time of a project specific application.

Furthermore, the future development of the site would stimulate extensions of additional urban services to smaller or intermediate parcels that may be adjacent or within “the last mile” of service. Extension of urban services to unserved areas within the UGA usually requires a substantial investment that can be cost prohibitive for smaller parcels. It takes a large assemblage of parcels to facilitate the necessary economies of scale to provide these services unless the County decides to bear the cost of the extensions through capital improvement projects.

9. How is the proposed amendment consistent with the Growth Management Act ([RCW 36.70A](#)), [Kitsap County-wide Planning Policies](#), state and local laws and other applicable inter-jurisdictional policies or agreements?

The redesignation request complies with Kitsap County Comprehensive Plan goals and Policies as previously discussed, and therefore will comply with the requirements of the Growth Management Act. The site is not affiliated with any City jurisdiction so no inter-jurisdictional agreements will be impacted by this request. Development of the site in conformance with Kitsap County Code requirements will ensure compatibility with state and local laws and surrounding uses and developments. This site does not add any area to the current UGA boundary and therefore is already consistent with GMA.

Additional Review Criteria: Site-Specific Amendments within an Urban Growth Area (UGA)

Only applicants submitting proposals within [Urban Growth Areas](#) must answer the questions in this section.

Urban Growth Area (UGA)	Affiliated Jurisdiction
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East Bremerton UGA	City of Bremerton
West Bremerton UGA	City of Bremerton
Gorst UGA	City of Bremerton
Puget Sound Industrial Center UGA	City of Bremerton
ULID No. 6/McCormick UGA	City of Port Orchard
South Kitsap/Port Orchard UGA	City of Port Orchard
Silverdale UGA	Kitsap County (not currently associated with a city)
Kingston UGA	Kitsap County (not currently associated with a city)
Central Kitsap UGA	Kitsap County (not currently associated with a city)

10. Does the jurisdiction affiliated with the UGA have the capability and capacity to provide urban level services to the area subject to this proposal? Explain or attach documentation.

Urban services include those public services and public facilities at an intensity historically and typically provided in cities, specifically including storm and sanitary sewer systems, domestic water systems, street cleaning services, fire and police protection services, public transit services, and other public utilities associated with urban areas and normally not associated with rural areas [RCW 36.70A.030(20)].

Not Applicable as the Silverdale UGA is not affiliated with another City jurisdiction.

11. How is this proposal consistent with the Comprehensive Plan of the jurisdiction affiliated with the UGA?

Not Applicable as the Silverdale UGA is not affiliated with another City jurisdiction.

12. How does this proposal meet the transportation standards of the jurisdiction affiliated with the UGA? Explain or attach documentation.

Not Applicable as the Silverdale UGA is not affiliated with another City jurisdiction.



Kitsap County
Annual Comprehensive Plan Amendment Process for 2018



Site-Specific Amendment Application
Review Criteria Narrative

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Reclamation of this former mining land and conversion to residential and commercial uses would best serve the growing needs of the Silverdale Urban Growth area and the community.

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3. How is the requested amendment in the public interest and the proposal consistent with the Kitsap County Comprehensive Plan?

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Current residentially zoned property is in short supply and many of the sites available have significant development challenges. Kitsap County continues to have less than two months' supply of housing units which is considered a seller's market and leads to increased housing costs. By adding the residential aspect of this proposal, it will help add to the housing supply which in turn balance the market.

The Neighborhood Commercial zoning will allow for a walkable mixed-use neighborhood with amenities nearby for both the residents of the subject property as well as the numerous residential units located in the vicinity. Currently there is a limited supply of commercial property able to serve the residents who reside west of Silverdale. This commercial property will allow opportunity to serve residents without the need to travel long distances.

Additional Review Criteria: All Site-specific Amendments

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4. How will the proposed amendment meet concurrency requirements for transportation, sewer and water, and not result in significant adverse impacts on adopted level of service standards for other public facilities and services, such as police, fire and emergency medical services, park services, and general government services? Explain or attach documentation.

The proposed amendment will continue to meet the goal of being within +/-5% of the population and employment targets for both the Silverdale UGA and Countywide UGA. This will be a minimal change that the capital facilities providers will be able to account for. Furthermore, the applicant has been in discussions with Kitsap County Public Works Sewer and Roads division, Silverdale Water District, Kitsap Transit, Central Kitsap School District, and Central Kitsap Fire District. The majority of the providers have confirmed they can address any concerns at the time of a future project specific application through analysis and potential mitigation.

5. How is the proposed amendment consistent with the balance of the goals, policies and objectives of the Kitsap County Comprehensive Plan and reflect the local circumstances of the county?

As previously discussed, job capacity will still be in excess of the planning requirements. The redesignation and rezone to Urban Low Residential will allow construction of housing that will comply with the Comprehensive plan goals and policies for provision of housing for residents of Kitsap County. There is a strong need for residential housing in the County, especially in the Silverdale UGA. Other residential developments in this neighborhood have experienced high sales rates indicating the desirability of residential housing in the area.

The Comprehensive Plan is considered balanced if the growth capacity is within +/-5% of the target. This proposal will maintain that balance and therefore will continue to be consistent with the goals, policies, and objectives of the Kitsap County Comprehensive Plan.

Furthermore, the proposed amendment meets the following specific goals, policies, and objectives:

- Land Use Goal 1. Focus current and future planning on infill and redevelopment of existing Urban Growth Areas
- Land use policy 2. Support innovative, high quality infill development and redevelopment in existing developed areas within the Urban Growth Areas
- Land Use Policy 7. Facilitate mixed use development in commercial designations
- Land Use Goal 3. Support more dense residential areas with access to transportation, urban amenities, goods and services, physical activity and healthy foods.
- Land Use Policy 16. Promote housing preservation and development in areas that are already well-served by schools, public transportation and commercial facilities, and have adequate infrastructure to support alternative modes of transportation.
- Land Use Policy 29. Through application of Growth Management Act goals, increase density in urban areas and limit sprawl in rural lands.
- Economic Development Policy 9. Encourage mixed use developments within commercial districts that will enhance the visual, economic, and environmental quality of these areas and improve the transition between commercial and residential districts.
- Transportation Policy 1. Coordinate a “safe routes to schools” program with local school districts and prioritize non-motorized improvements and connectivity around schools.
- Transportation Policy 3. Continue to require sidewalks on roads when development occurs within Urban Growth Areas.
- Silverdale Policy 1. Monitor land supply over time to ensure a continued adequate supply of residential, commercial and industrial designated land to meet Silverdale’s population and employment targets and to meet the needs of unexpected growth.

- Silverdale Policy 14. Encourage full use and development of designated commercial and industrial areas prior to expanding those areas. Promote revitalization within existing developed areas to take advantage of the investment in existing buildings and infrastructure.

6. How is the subject parcel(s) suitable for the requested land use designation based upon, but not limited to, access, provision of utilities, and consistency with existing and planned uses, environmental constraints and compatibility with the neighborhood?

The underutilized former mining site is suitable for much needed residential development with acceptable topography, access, utilities, and an existing disturbed land and soil features that will preserve undisturbed natural environments elsewhere within the Urban Growth Area.

The site possesses many amenities that will be desirable for a residential development. The site is served by some utilities at the present time and as new development is built the appropriate utility extensions will be constructed. Access to the site is available to Willamette Meridian Road and Dickey Road and the approval of the residential designation would facilitate much needed emergency services east-west cross connections between those major north-south corridors.

Existing industrial use nearby is minor in use and the negative impacts of any possible adjacency of any residential zone is evaluated and mitigated as part of the land use subdivision development and entitlement process. Numerous examples of residential development adjacent to industrially zoned parcels have been approved within Kitsap County and exist as a model of the land use entitlement process.

Within Table KCC 17.420.060 the Footnotes for Tables, Footnote #27 was created to address this very issue and has successfully mitigated the potential adjacency issues. Any new development would equally be required to comply with Footnote #27:

KCC 17.420.060 Footnote #27. As approved by the director, wherever an industrial zone abuts a residential zone, a fifty-foot screening buffer area shall be provided. This screening buffer is intended to reduce impacts to abutting residential uses such as noise, light, odors, dust and structure bulk. No structures, open storage, or parking shall be allowed within this area. The director shall only approve screening buffers that improve the compatibility between the proposed use and the residential zone. The director may reduce this buffer to a minimum of twenty-five-foot width only when based upon a site-specific determination that topography, berming or other screening features will effectively screen industrial activities from the residential zone. Conversely, based upon a similar site-specific determination, the director may increase the buffer width from fifty feet to ensure adequate buffering and compatibility between uses Apex Airport is nearby and other Industrial Zoned areas exist adjacent the site on the other boundaries. Residential uses of the site can be compatible with the adjoining Industrial Zone uses by provision of buffers that occur naturally due to topography or critical area buffers.

7. How does the proposed amendment not materially affect the land uses and growth projections which are the basis for the Comprehensive Plan, and reflect local circumstances in the county?

The job capacity requirements of the Silverdale UGA have been discussed and shown to remain adequate for the plan after the redesignation of these parcels. The residential development potential of the site is not large in comparison to the overall area provided and required for residential zones and will not materially alter the balance of land uses in the County. The Comprehensive Plan will remain in balance based upon the growth to target ratio of +/-5%.

8. How does the proposed amendment not materially affect the adequacy or availability of urban facilities and services to the immediate area or the overall area of the urban growth area?

As discussed previously, there are no capital facility impacts that cannot be addressed at the time of a project specific application.

Furthermore, the future development of the site would stimulate extensions of additional urban services to smaller or intermediate parcels that may be adjacent or within “the last mile” of service. Extension of urban services to unserved areas within the UGA usually requires a substantial investment that can be cost prohibitive for smaller parcels. It takes a large assemblage of parcels to facilitate the necessary economies of scale to provide these services unless the County decides to bear the cost of the extensions through capital improvement projects.

9. How is the proposed amendment consistent with the Growth Management Act ([RCW 36.70A](#)), [Kitsap County-wide Planning Policies](#), state and local laws and other applicable inter-jurisdictional policies or agreements?

The redesignation request complies with Kitsap County Comprehensive Plan goals and Policies as previously discussed, and therefore will comply with the requirements of the Growth Management Act. The site is not affiliated with any City jurisdiction so no inter-jurisdictional agreements will be impacted by this request. Development of the site in conformance with Kitsap County Code requirements will ensure compatibility with state and local laws and surrounding uses and developments. This site does not add any area to the current UGA boundary and therefore is already consistent with GMA.

Additional Review Criteria: Site-Specific Amendments within an Urban Growth Area (UGA)

Only applicants submitting proposals within [Urban Growth Areas](#) must answer the questions in this section.

Urban Growth Area (UGA)	Affiliated Jurisdiction
Poulsbo UGA	City of Poulsbo
East Bremerton UGA	City of Bremerton
West Bremerton UGA	City of Bremerton
Gorst UGA	City of Bremerton
Puget Sound Industrial Center UGA	City of Bremerton
ULID No. 6/McCormick UGA	City of Port Orchard
South Kitsap/Port Orchard UGA	City of Port Orchard
Silverdale UGA	Kitsap County (not currently associated with a city)
Kingston UGA	Kitsap County (not currently associated with a city)
Central Kitsap UGA	Kitsap County (not currently associated with a city)

10. Does the jurisdiction affiliated with the UGA have the capability and capacity to provide urban level services to the area subject to this proposal? Explain or attach documentation.

Urban services include those public services and public facilities at an intensity historically and typically provided in cities, specifically including storm and sanitary sewer systems, domestic water systems, street cleaning services, fire and police protection services, public transit services, and other public utilities associated with urban areas and normally not associated with rural areas [RCW 36.70A.030(20)].

The proposed site is currently already located in the UGA and is served by or adjacent to urban level services. The applicant has had initial discussions with providers regarding capacity however site specific impacts will be determined and mitigated at the time of a project specific application.

11. How is this proposal consistent with the Comprehensive Plan of the jurisdiction affiliated with the UGA?

The Kitsap County Comprehensive Plan is considered balanced if the growth capacity is within +/-5% of the target. This proposal will maintain that balance and therefore will continue to be consistent with the goals, policies, and objectives of the Kitsap County Comprehensive Plan.

Furthermore, the proposed amendment meets the following specific goals, policies, and objectives:

- Land Use Goal 1. Focus current and future planning on infill and redevelopment of existing Urban Growth Areas
- Land use policy 2. Support innovative, high quality infill development and redevelopment in existing developed areas within the Urban Growth Areas
- Land Use Policy 7. Facilitate mixed use development in commercial designations
- Land Use Goal 3. Support more dense residential areas with access to transportation, urban amenities, goods and services, physical activity and healthy foods.

- Land Use Policy 16. Promote housing preservation and development in areas that are already well-served by schools, public transportation and commercial facilities, and have adequate infrastructure to support alternative modes of transportation.
- Land Use Policy 29. Through application of Growth Management Act goals, increase density in urban areas and limit sprawl in rural lands.
- Economic Development Policy 9. Encourage mixed use developments within commercial districts that will enhance the visual, economic, and environmental quality of these areas and improve the transition between commercial and residential districts.
- Transportation Policy 1. Coordinate a “safe routes to schools” program with local school districts and prioritize non-motorized improvements and connectivity around schools.
- Transportation Policy 3. Continue to require sidewalks on roads when development occurs within Urban Growth Areas.
- Silverdale Policy 1. Monitor land supply over time to ensure a continued adequate supply of residential, commercial and industrial designated land to meet Silverdale’s population and employment targets and to meet the needs of unexpected growth.
- Silverdale Policy 14. Encourage full use and development of designated commercial and industrial areas prior to expanding those areas. Promote revitalization within existing developed areas to take advantage of the investment in existing buildings and infrastructure.

12. How does this proposal meet the transportation standards of the jurisdiction affiliated with the UGA? Explain or attach documentation.

The proposed site is currently already located in the UGA and is served by or adjacent to urban level services. The applicant has had initial discussions with Jon Brand, the County Engineer for the Public Works Road Division, regarding capacity however site specific impacts will be determined and mitigated at the time of a project specific application.

SEPA ENVIRONMENTAL CHECKLIST

Purpose of checklist:

Governmental agencies use this checklist to help determine whether the environmental impacts of your proposal are significant. This information is also helpful to determine if available avoidance, minimization or compensatory mitigation measures will address the probable significant impacts or if an environmental impact statement will be prepared to further analyze the proposal.

Instructions for applicants:

This environmental checklist asks you to describe some basic information about your proposal. Please answer each question accurately and carefully, to the best of your knowledge. You may need to consult with an agency specialist or private consultant for some questions. You may use "not applicable" or "does not apply" only when you can explain why it does not apply and not when the answer is unknown. You may also attach or incorporate by reference additional studies reports. Complete and accurate answers to these questions often avoid delays with the SEPA process as well as later in the decision-making process.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will help describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

Instructions for Lead Agencies:

Please adjust the format of this template as needed. Additional information may be necessary to evaluate the existing environment, all interrelated aspects of the proposal and an analysis of adverse impacts. The checklist is considered the first but not necessarily the only source of information needed to make an adequate threshold determination. Once a threshold determination is made, the lead agency is responsible for the completeness and accuracy of the checklist and other supporting documents.

Use of checklist for nonproject proposals:

For nonproject proposals (such as ordinances, regulations, plans and programs), complete the applicable parts of sections A and B plus the [SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS \(part D\)](#). Please completely answer all questions that apply and note that the words "project," "applicant," and "property or site" should be read as "proposal," "proponent," and "affected geographic area," respectively. The lead agency may exclude (for non-projects) questions in Part B - Environmental Elements –that do not contribute meaningfully to the analysis of the proposal.

A. Background [\[HELP\]](#)

1. Name of proposed project, if applicable:

CPA 18-00495 Site Specific Comp Plan Amendment / Port Orchard Sand and Gravel

2. Name of applicant: AGENT: Levi Holmes

3. Address and phone number of applicant and contact person:

Levi Holmes

The JWJ Group, LLC

3599 NW Carlton St, Ste 201

Silverdale, WA 98383

PH: 360-626-1146

Levi@JWJGroup.com

4. Date checklist prepared: Revised 10-3-2019

5. Agency requesting checklist: Kitsap County Department of Community Development

6. Proposed timing or schedule (including phasing, if applicable):

Site Specific Comprehensive Plan Approval expected by the end of 2019. Reclamation of the property and close out of the mining permit are expected to happen at some point in the future concurrently at the time of Kitsap County Site Development Permits. Currently it is unknown when project specific land use and development permits will be submitted for.

7. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain.

Mine Reclamation Permit pursuant to KCC 17.060.065 and Subdivision Permit Processing upon Comprehensive Site Specific Amendment approval.

8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.

Geological analysis provided, see geotechnical memorandum provided by N.L. Olson and Associates (Exhibit 1)

9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain.

None Known

10. List any government approvals or permits that will be needed for your proposal, if known.

The current proposal will require removal of the mineral resource layer as well as a zoning change and amendment to the comprehensive plan. Future development of the site will require reclamation of the mining activity as well as development and land use permits which may include the following:

- Site development permit consistent with Kitsap County Code (KCC) Title 12
- Land use permits consistent with KCC Title 16, 17, and 21,
- Building permits consistent with KCC title 14

11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this

page. (Lead agencies may modify this form to include additional specific information on project description.)

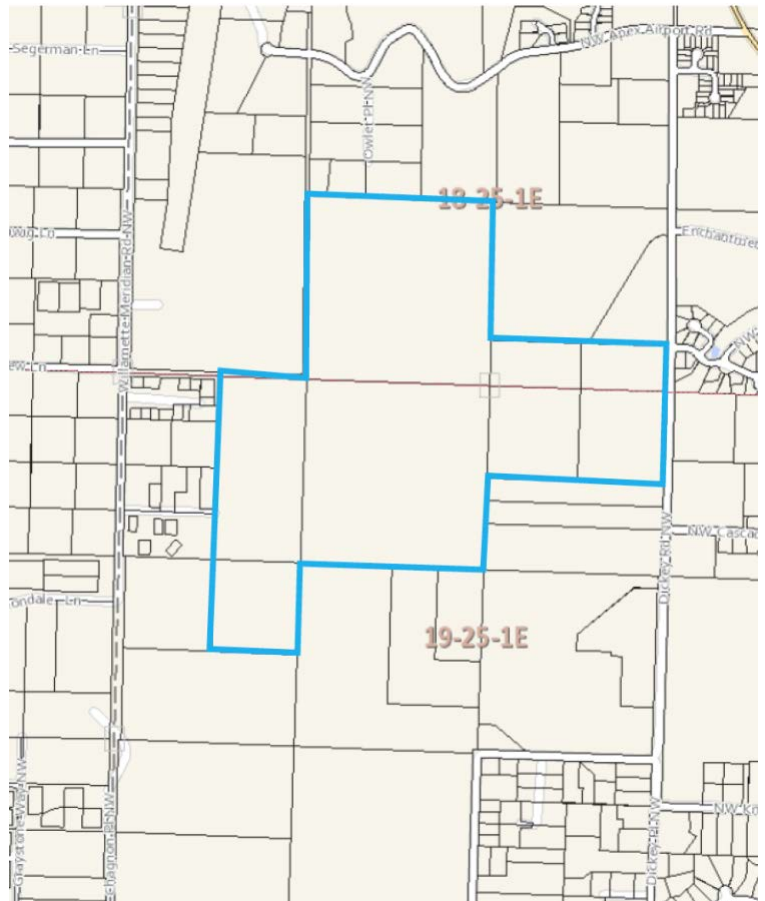
The property is approximately 138 acres currently zoned Industrial with a Mineral Resource Overlay. The applicant is requesting to rezone the property to approx. 78 acres of Neighborhood Commercial and approx. 60 acres of Urban Low Residential.

12. Location of the proposal. Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township, and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit applications related to this checklist.

The site is comprised of 5 parcels totaling approximately 138.45 acres within a Mineral Resource Area/Industrial zoned area of the Silverdale UGA that is generally bordered by Newberry Hill Road NW, Dickey Road NW, Apex Airport Road NW, and Willamette-Meridian Road NW. The majority of the subject properties were a sand and gravel mine, which has now exhausted the resource and is awaiting reclamation.

Adjacent uses include residential, commercial, and industrial. Specifically to the north is rural residential, a private/public general aviation airport (Apex airpark airport), and vacant industrial land. To the west is rural single family residential, and various industrial uses. To the south is surface mining, a public solid-waste transfer center, an animal shelter as well as undeveloped land. To the east is urban low residential as well as an elementary school, and bus barn.

For more information on the site location reference Exhibit 2.



B. Environmental Elements [HELP]

1. Earth [help]

a. General description of the site:

(circle one): Flat, rolling hilly, steep slopes, mountainous, other _____

b. What is the steepest slope on the site (approximate percent slope)?

According to the Puget Sound Lidar maps available on the Kitsap County GIS website the site is rolling with flat areas. The general declivity of the site is east to west with slopes onsite varying from 0% to 40%.

- c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any agricultural land of long-term commercial significance and whether the proposal results in removing any of these soils.

See attached geotechnical memorandum provided by N.L. Olson and Associates (Exhibit 1).

- d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe.

Kitsap County Seismic Hazard Map identifies areas of liquefaction however that will not be an issue with the proposed site. See attached geotechnical memorandum provided by N.L. Olson and Associates. This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

- e. Describe the purpose, type, total area, and approximate quantities and total affected area of any filling, excavation, and grading proposed. Indicate source of fill.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated in accordance with KCC Title 12 at the time of a project specific application. The topography of the site will allow for future commercial and/or residential development which will require grading of the site. Future grading volumes are unknown. Redevelopment will require reclamation and result in previously disturbed areas being potentially regraded and amended to mimic more natural conditions.

- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Erosion from future development activities will be controlled through KCC Title 12 and the Site Development Activity Permit (SDAP) process which includes a Temporary Erosion and Sediment Control Plan in accordance with the Kitsap County Stormwater Management Manual.

- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)?

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Potential impervious surfaces will be limited based on KCC Title 17.420 (85% max for Neighborhood Commercial), as well as Title KCC 17.500 Landscaping, and Title 16 Land Division and Development which have requirements for open space. KCC Title 12 will ensure all impervious surfaces properly mitigate water

quality and quantity. KCC Title 19, Critical Area Ordinance, will require additional buffers and impervious surface setbacks from critical areas including steep slopes, streams, wetlands, etc.

- h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any:
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Currently the site has temporary erosion control measures as part of the mining activity. A Temporary Erosion and Sediment Control plan will be submitted to Kitsap County prior to any future development of the site. The plan will be in accordance with the Kitsap County Stormwater Management Manual. The plan will utilize Best Management Practices throughout construction. This could include vegetation retention, earth covering, filter fabric fences, stabilized construction entrances, sediment traps/ponds, stormwater ponds, rip-rap, hydroseeding, low impact development (LID) and other best management practices BMP's as necessary. Furthermore, redevelopment will require reclamation of the prior mining activities.

2. Air [\[help\]](#)

- a. What types of emissions to the air would result from the proposal during construction, operation, and maintenance when the project is completed? If any, generally describe and give approximate quantities if known.
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. During future development there will be some dust and emissions from construction equipment. Upon project completion the normal emissions from traffic by residents, customers and guests can be expected. Furthermore, the change from Industrial Zoning will reduce the potential for high intensity industrial uses which may create ongoing emissions as part of the business operations.
- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe.
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. The adjacent gravel mine and Apex Airport have limited sources of emissions and/or odor however they currently border residential areas.
- c. Proposed measures to reduce or control emissions or other impacts to air, if any:
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. The reduction in industrial zoning will reduce potential emissions. Watering may be used to control dust during future construction. Pervious surface areas may be landscaped after construction.

3. **Water** [\[help\]](#)

a. Surface Water: [\[help\]](#)

- 1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.

Currently the site has large sediment ponds as part of the mining permit. The Kitsap County Critical Areas Map identifies potential fish and non-fish bearing streams. There are potential wetlands on site as well. Reference attached wetland exhibit 3.

- 2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated in accordance with KCC Title 19 at the time of a project specific application.

- 3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.

No dredge or fill is anticipated however this is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

- 4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.

No withdrawals or diversions are anticipated however this is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

- 5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan.

No, no part of the proposal lies within a 100-year flood plain

- 6) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.

None known however this is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

b. Ground Water: [\[help\]](#)

- 1) Will groundwater be withdrawn from a well for drinking water or other purposes? If so, give a general description of the well, proposed uses and approximate quantities withdrawn from the well. Will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known.

None anticipated, however this is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Any existing wells on the property are anticipated to be decommissioned when connecting to public water.

- 2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals. . . ; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Sanitary sewer from future development will be discharged to the sanitary sewer systems. Any existing septic systems are anticipated to be decommissioned when connecting to public sewer services.

c. Water runoff (including stormwater):

- 1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Runoff from future development of the site will receive quantity control and quality enhancement in accordance with KCC Title 12 Stormwater Drainage.

- 2) Could waste materials enter ground or surface waters? If so, generally describe.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Future development has the potential for accidental spills during construction or accidental breakage of sewer lines. This same potential exists under the current zoning. Residential and commercial zonings are less likely to have large quantities of harmful chemicals stored on site.

- 3) Does the proposal alter or otherwise affect drainage patterns in the vicinity of the site? If so, describe.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated in accordance with KCC Title 12 at the time of a project specific application.

d. Proposed measures to reduce or control surface, ground, and runoff water, and drainage pattern impacts, if any:

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated in accordance with KCC Title 12 at the time of a project specific application.

4. **Plants** [\[help\]](#)

a. Check the types of vegetation found on the site:

- deciduous tree: **alder, maple**, aspen, other
- evergreen tree: **fir, cedar**, pine, other
- shrubs
- grass
- pasture
- crop or grain
- Orchards, vineyards or other permanent crops.
- wet soil plants: cattail, **buttercup**, bullrush, skunk cabbage, other
- water plants: water lily, eelgrass, milfoil, other
- other types of vegetation

b. What kind and amount of vegetation will be removed or altered?

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated in accordance with KCC Title 12 and 19 at the time of a project specific application. Currently the majority of the site is stripped of vegetation however the redevelopment of the site will result in reclamation including replanting and amended soils.

c. List threatened and endangered species known to be on or near the site.

None known however this is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Kitsap County Code will require future development of the site to include landscaping consistent with KCC Title 17.

e. List all noxious weeds and invasive species known to be on or near the site.

Scotch Broom and Blackberry.

5. **Animals** [\[help\]](#)

a. List any birds and other animals which have been observed on or near the site or are known to be on or near the site.

Examples include:

- birds: **hawk**, heron, **eagle, songbirds, other:**
- mammals: **deer**, bear, elk, beaver, **other:**
- fish: bass, salmon, trout, herring, shellfish, other _____

- b. List any threatened and endangered species known to be on or near the site.
None known however this is a comprehensive plan amendment; site specific impacts will be determined and mitigated in accordance with KCC Title 19 at the time of a project specific application.
- c. Is the site part of a migration route? If so, explain.
None known however this is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.
- d. Proposed measures to preserve or enhance wildlife, if any:
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.
- e. List any invasive animal species known to be on or near the site.
None known however this is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

6. Energy and Natural Resources [\[help\]](#)

- a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc.
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.
Future development may include electricity and natural gas for heating, lighting and other household use. It is anticipated that the energy demand for residential/commercial uses may be less than the demands for industrial uses.
- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe.
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.
- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any:
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.
Future buildings will be constructed in accordance with KCC Title 14 Buildings and Construction and will meet Washington State Energy Conservation codes.

7. Environmental Health [\[help\]](#)

a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste, that could occur as a result of this proposal? If so, describe.

1) Describe any known or possible contamination at the site from present or past uses.

None known however this is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

2) Describe existing hazardous chemicals/conditions that might affect project development and design. This includes underground hazardous liquid and gas transmission pipelines located within the project area and in the vicinity.

None known however this is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

3) Describe any toxic or hazardous chemicals that might be stored, used, or produced during the project's development or construction, or at any time during the operating life of the project.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Future construction may use and store small amounts of petroleum products for vehicle operations. Residential and Commercial zoning is less likely than Industrial zoning to have large quantities of harmful chemicals stored on site.

4) Describe special emergency services that might be required.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Increased police and fire services may be required as is necessary for any development regardless of zoning. Future development of the site may allow for potential east/west emergency vehicle connections with Dickey Rd and Willamette Meridian Rd.

5) Proposed measures to reduce or control environmental health hazards, if any:

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

b. Noise

1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. The adjacent gravel mine and Apex Airport generate noise, and the adjacent school generates heavy traffic. Currently all of them are located near residential areas.

2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Future development of the site may have construction noise during normal business hours on a short-term basis during construction, increased traffic and people noise on a long-term basis after site development. Residential and Commercial zonings have stricter noise restrictions than Industrial uses, thus the proposed amendment may result in a reduction of potential short/long term noise generation.

3) Proposed measures to reduce or control noise impacts, if any:

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Future construction will be limited to normal business hours.

8. Land and Shoreline Use [\[help\]](#)

a. What is the current use of the site and adjacent properties? Will the proposal affect current land uses on nearby or adjacent properties? If so, describe.

The site is vacant and undeveloped with a former gravel pit located on site. The properties to the north are residential, the properties to the northwest include an airfield, Apex Airport. The properties to the west and east include industrial properties, most of which are undeveloped. The properties to the south include industrial property with a Mineral Resource Overlay and an operational gravel and sand mining operation. This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

b. Has the project site been used as working farmlands or working forest lands? If so, describe. How much agricultural or forest land of long-term commercial significance will be converted to other uses as a result of the proposal, if any? If resource lands have not been designated, how many acres in farmland or forest land tax status will be converted to nonfarm or nonforest use?

Unknown, however, this is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

1) Will the proposal affect or be affected by surrounding working farm or forest land normal business operations, such as oversize equipment access, the application of pesticides, tilling, and harvesting? If so, how:

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

c. Describe any structures on the site.

None Known

- d. Will any structures be demolished? If so, what?
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.
- e. What is the current zoning classification of the site?
Industrial
- f. What is the current comprehensive plan designation of the site?
Mineral Resource Overlay / Industrial
- g. If applicable, what is the current shoreline master program designation of the site?
N/A No Shoreline
- h. Has any part of the site been classified as a critical area by the city or county? If so, specify.
Yes, geological hazards with steep slopes (please reference attached geological report). The site lies within the urban growth boundary and has mapped fish/non-fish streams, and Category 1 and Category 2 Critical Aquifer designated areas. A large electrical easement bisects a significant portion of the eastern property boundary. This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.
- i. Approximately how many people would reside or work in the completed project?
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Urban Low Residential zoning allows for 5-9 dwelling units per acre and Neighborhood Commercial allows for 10-30 units per acre however the site specific Land Capacity Analysis (LCA reference Exhibit 4) provided by Kitsap County calculates 151 new residential units (377 people) and 446 new jobs (reference Exhibit 5) The LCA is based on similar assumptions used for the currently adopted Comprehensive Plan. Furthermore the LCA calculates the property will consist of 25.16 net developable acres of Urban Low Residential and 31.04 net developable acres of Neighborhood Commercial.
- j. Approximately how many people would the completed project displace?
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. It is unlikely the future project would displace any people.
- k. Proposed measures to avoid or reduce displacement impacts, if any:
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

- L. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any:

The site is within the Silverdale Urban Growth Area for Kitsap County and is adjacent to residential development to the north. The Neighborhood Commercial provides a buffer/transition to adjacent land uses. Industrial uses are planned for other surrounding properties and compatibility will be provided by naturally occurring and man made buffers. KCC Title 17 addresses setbacks and buffer requirements for adjacent uses. Kitsap County currently has many areas where Industrial zoning is located near commercial and residential zoning (reference Exhibit 6).

- m. Proposed measures to reduce or control impacts to agricultural and forest lands of long-term commercial significance, if any:

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

9. Housing [\[help\]](#)

- a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Urban Low Residential zoning allows for 5-9 dwelling units per acre and Neighborhood Commercial allows for 10-30 units per acre however the site specific Land Capacity Analysis (LCA reference Exhibit 4) provided by Kitsap County calculates 151 new residential units (377 people) and 446 new jobs (reference Exhibit 5). The LCA is based on similar assumptions used for the currently adopted Comprehensive Plan. Furthermore the LCA calculates the property will consist of 25.16 net developable acres of Urban Low Residential and 31.04 net developable acres of Neighborhood Commercial.

- b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing.

No housing units will be removed as part of this Comprehensive Plan Amendment request.

- c. Proposed measures to reduce or control housing impacts, if any:

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

10. **Aesthetics** [\[help\]](#)

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed?
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.
Future development will comply with KCC 17.420 which allows for a maximum building height of 35' in Urban Low and up to 45' in Neighborhood Commercial.
- b. What views in the immediate vicinity would be altered or obstructed?
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.
The site is visible to the immediate neighbors. The reclamation will increase the aesthetics for surrounding properties including revegetation of the site.
- b. Proposed measures to reduce or control aesthetic impacts, if any:
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

11. **Light and Glare** [\[help\]](#)

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur?
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.
Light from future buildings and vehicles would be noticed mainly at night.
Industrial uses allow for much more intense light and glare.
- b. Could light or glare from the finished project be a safety hazard or interfere with views?
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.
- c. What existing off-site sources of light or glare may affect your proposal?
None known, however, this is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.
- d. Proposed measures to reduce or control light and glare impacts, if any:
This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

12. **Recreation** [\[help\]](#)

- a. What designated and informal recreational opportunities are in the immediate vicinity?
Klahowya Secondary School and Silverdale Elementary are located nearby.
Other recreation opportunities in the area are located at the Fairgrounds in Silverdale. The Bonneville power easement is an informal hiking trail.
- b. Would the proposed project displace any existing recreational uses? If so, describe.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any:

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

13. Historic and cultural preservation [\[help\]](#)

- a. Are there any buildings, structures, or sites, located on or near the site that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers? If so, specifically describe.

None known, however, this is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

- b. Are there any landmarks, features, or other evidence of Indian or historic use or occupation? This may include human burials or old cemeteries. Are there any material evidence, artifacts, or areas of cultural importance on or near the site? Please list any professional studies conducted at the site to identify such resources.

None known, however, this is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

- c. Describe the methods used to assess the potential impacts to cultural and historic resources on or near the project site. Examples include consultation with tribes and the department of archeology and historic preservation, archaeological surveys, historic maps, GIS data, etc.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

Prior gravel mine is likely to have encountered any potential historical or cultural points of interest. Kitsap County Code requires contractors to contact local jurisdictions if any potential cultural or historical points of interest are encountered during development.

- d. Proposed measures to avoid, minimize, or compensate for loss, changes to, and disturbance to resources. Please include plans for the above and any permits that may be required.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

14. Transportation [\[help\]](#)

- a. Identify public streets and highways serving the site or affected geographic area and describe proposed access to the existing street system. Show on site plans, if any.

The site is accessed from Dickey Road NW and Willamette Meridian Road. Anderson Hill Road to the north and Newberry Hill Road to the south allows connectivity to State Highway 3 to the east.

- b. Is the site or affected geographic area currently served by public transit? If so, generally describe. If not, what is the approximate distance to the nearest transit stop?

Kitsap Transit does not serve the site. The nearest bus route is at the intersection of Anderson Hill Road and Old Frontier Place about 1 mile east of the site. Kitsap transit will have an opportunity to work with the applicant on possible routes and transit stops during a project specific application.

- c. How many additional parking spaces would the completed project or non-project proposal have? How many would the project or proposal eliminate?

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Future development will include parking guidelines consistent with the design guidelines of Kitsap County Code including Title 17.490.

- d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities, not including driveways? If so, generally describe (indicate whether public or private).

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. The future development of the site may include construction of new streets onsite as well as frontage improvements. The project may need to contribute towards mitigation improvements for any roads that this project creates deficiencies on. The future project will also be required to pay traffic impact fees.

- e. Will the project or proposal use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. The project is located in the vicinity of Apex Airport.

- f. How many vehicular trips per day would be generated by the completed project or proposal? If known, indicate when peak volumes would occur and what percentage of the volume would be trucks (such as commercial and nonpassenger vehicles). What data or transportation models were used to make these estimates?

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application. Future development of the site will result in additional trips which will be calculated at the time of a project specific application.

- g. Will the proposal interfere with, affect or be affected by the movement of agricultural and forest products on roads or streets in the area? If so, generally describe.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

h. Proposed measures to reduce or control transportation impacts, if any:

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

Future development may require mitigation based on a traffic impact analysis and review by Kitsap County Public Works.

15. Public Services [\[help\]](#)

a. Would the project result in an increased need for public services (for example: fire protection, police protection, public transit, health care, schools, other)? If so, generally describe.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

Future development of the site will require an increase in all of the above as is normal for residential and commercial developments. Impact fees will be paid to account for the increased need. Additional deficiencies may require additional mitigation at a project specific level.

b. Proposed measures to reduce or control direct impacts on public services, if any.

This is a comprehensive plan amendment; site specific impacts will be determined and mitigated at the time of a project specific application.

Future development will require impact fees which may include parks, schools and traffic.

16. Utilities [\[help\]](#)

a. Circle utilities currently available at the site:

electricity, natural gas, water, refuse service, telephone, sanitary sewer septic system, other _____

c. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed.

Electricity:	Puget Sound Energy Services
Water:	Silverdale Water District #16
Sanitary Sewer:	Kitsap County
Refuse Service:	Waste Management
Telephone:	U.S. West

C. Signature [\[HELP\]](#)

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision.

Signature:  _____

Name of signee Levi Holmes

Position and Agency/Organization Manager, The JWJ Group LLC

Date Submitted: 10-4-19

D. Supplemental sheet for nonproject actions [\[HELP\]](#)

(IT IS NOT NECESSARY to use this sheet for project actions)

Because these questions are very general, it may be helpful to read them in conjunction with the list of the elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage, or release of toxic or hazardous substances; or production of noise?

The redesignation to Urban Low Residential will allow additional dwelling units to be constructed beyond that allowed by the current designation/zone. A residential development is less likely to increase discharge to water, emissions to air production, storage, or release of toxic or hazardous substances and noise production beyond what is typical for industrial uses. Development of the site as a residential project most likely have impacts less than that of a site fully developed as Industrial. The inclusion of localized Neighborhood Commercial nodes provides the ability to have walkable neighborhoods and reduction in transportation related discharges.

Proposed measures to avoid or reduce such increases are:

The future development proposal will comply with Kitsap County development standards and requirements.

2. How would the proposal be likely to affect plants, animals, fish, or marine life?

The redesignation to Urban Low Residential will allow residential dwelling units to be constructed rather than an industrial development. The inclusion of localized Neighborhood Commercial nodes in the Alternatives provides the ability to have walkable neighborhoods. While both forms of development will remove existing vegetation and thereby alter habitat on the site, residential uses should have less impacts than industrial uses to plants and animals in the area.

Proposed measures to protect or conserve plants, animals, fish, or marine life are:

The future development proposal will comply with Kitsap County development standards and requirements.

3. How would the proposal be likely to deplete energy or natural resources?

The Redesignation to Urban Low Residential will allow additional dwelling units to be constructed on the site. The inclusion of localized Neighborhood Commercial provides the ability to have walkable neighborhoods and may allow

an additional amount of limited commercial serving the local community. This construction and ongoing residential uses will use energy in the future however the demand is anticipated to be less than a fully developed Industrial property. The site has been mined and the resource has been depleted.

Proposed measures to protect or conserve energy and natural resources are:

The future development proposal will comply with Washington State Energy code and Kitsap County development standards and requirements.

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection; such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, floodplains, or prime farmlands?

The redesignation to Urban Low Residential will allow additional dwelling units to be constructed on the site. The inclusion of localized Neighborhood Commercial provides the ability to have walkable neighborhoods and may allow an additional amount of limited commercial serving the local community. The site does not contain any areas designated or considered by government for parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, flood plains or prime farmlands. Environmentally sensitive areas will be protected by Kitsap County Critical area Ordinance and other Kitsap County Code provisions. Furthermore the development will require open space and park amenities.

Proposed measures to protect such resources or to avoid or reduce impacts are:

The future development proposal will comply with Kitsap County development standards and requirements.

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans?

The site is not in the Shoreline Management Area and this redesignation and future development of the site is not expected to affect shoreline uses.

Proposed measures to avoid or reduce shoreline and land use impacts are:

The future development proposal will comply with Kitsap County development standards and requirements.

6. How would the proposal be likely to increase demands on transportation or public services and utilities?

The redesignation to Urban Low Residential will allow additional dwelling units to be constructed. The inclusion of localized Neighborhood Commercial provides the ability to have walkable neighborhoods and may allow an additional amount of limited commercial serving the local community. This higher density may increase demands on all of the above.

Proposed measures to reduce or respond to such demand(s) are:

Traffic mitigation may be required for capacity deficiencies. Silverdale Water District has storage and capacity to serve the site. The future development will require Impact Fees to be paid to Kitsap County to address the increased demand for these urban services.

7. Identify, if possible, whether the proposal may conflict with local, state, or federal laws or requirements for the protection of the environment.

The redesignation to Urban Low Residential with Neighborhood Commercial provides the ability to have walkable neighborhoods and may allow an additional amount of limited commercial to serve the local community and will not conflict with local, state, or federal laws or requirements to protect the environment.



MEMORANDUM

PROJECT: Dickey Road Sand Gravel Pit (Feasibility Study)

TO: Levi Holmes
JWJ Group
3599 Carlton Street, Suite 201
Silverdale, WA 98383
PH (360) 626-1146

FROM: Wesley R. Johnson, P.E. *WRL*
10/21/2019

SUBJ: LIQUEFACTION POTENTIAL

DATE: May 3, 2019

Mr. Holmes,

This report summarizes our subsurface exploration program that involved test pits and borings. During our initial investigation that occurred in September 2018, N.L. Olson & Associates, Inc (NLO) verified fill at several test pits locations that extended below 22 feet. After review of the initial subsurface exploration program, NLO recommended that drilling operations should be implemented that occurred in January 2019.

The drilling operations were recommended to determine actual fill depths, ground water levels, soil types, relative soil density, and determine zones within the fill susceptible to liquefaction.

PURPOSE AND SCOPE OF SERVICE

NLO's scope of work included a site reconnaissance, review of available geologic site information, a subsurface exploration program that consisted of the excavation of four (4) borings and 27 test pits. Engineering analysis, and the preparation of this report that has provided design and construction recommendations for site development criterion for grading and structural fill placement, assessment for both liquefaction with mitigation measures, and general construction criteria. Our scope of services was performed in general accordance with NLO's Professional Services Agreement (PSA) and fee estimate that included the following:

- A field investigation consisting of excavating 27 test pits at the project site to evaluate the subsurface soil and groundwater conditions. The depths of the exploratory test pits ranged from 7 feet to 22 feet below existing site grades.
- A field investigation consisting of drilling four (4) exploratory borings at the project site and sampling to evaluate the subsurface soil and subsurface water conditions. The depths of the exploratory borings ranged from 20.5 feet to 46.5 feet below existing site grades.

- Performing laboratory tests on representative soil samples obtained from the borings to evaluate the physical and index properties of the subsurface soils.
- Preparation of this report summarizing the results, conclusions, recommendations, and findings of our investigation.

NLO did not provide recommendations for placement of reinforced concrete retaining walls, rock walls or mechanically stabilized earth walls. In addition, our scope of work did not include chemical testing of the soil or water or to perform slope stability analysis of the gradual slope that resides around the perimeter of the subject property.

SITE LOCATION

The subject property is located southeast of Apex Airport and between Willamette Meridian and Dickey Road approximately one (1) mile west of Silverdale, WA. The site is situated in sections 18 & 19, T25N, Range 1 East in Kitsap County Washington. The location of the property is shown on the Vicinity Map, Figure 1.

EXISTING CONDITIONS

The project area is roughly 100 acres and bounded to the west by Apex Property Owners IMP Association, airport, commercial development to the east by a undeveloped property and beyond by Dickey Road NW, to the north by a mix of commercial and residential development and to the south by Pyramid Material Holdings LLC. The subject area presently consists of a reclaimed pit area. The site has two (2) pond areas with the northern pond being roughly eight (8) acres and the south pond about four (4) acres. A large soil stock pile area that had a vertical height of about 20 feet and covered an area of about 4 acres. The screening pile/piles were observed roughly centered along the east side of the north pond. The site's existing conditions have been illustrated on the Site Plan, Figure 2.

PROPOSED

As presently conceived, the proposed reclaimed pit area has been proposed to be developed with residential development. The proposed development is pending the results of this investigation to determine future site improvements and grading plan.

FIELD INVESTIGATION

The site subsurface soil conditions were explored during September 17, 2018 to September 19, 2018 with 27 test pits and drilling four (4) borings on January 9 and 10, 2019. Advanced Drill Exploration, Inc., advanced the borings down to a maximum depth of 46.5 feet below ground surface (bgs) with the drilling operations performed with a D50 track mounted Drill Rig. Piezometers were placed in each of the borings. The test pits were excavated by Vet Industrial with a case CX240B excavator. The approximate boring and test pit locations have been shown on the attached Site Plan, Figure 2. The boring and test pit logs have been included in Appendix A of this report.

Site Soil Conditions

Fill soil conditions were encountered in the subsurface exploration that and down to a maximum depth of 40 feet. The fill was thickness along the north and east central portion of the subject property with the fill decreasing in thickness to the west and south sides of the subject property. The fill consisted predominantly very loose to medium dense silty sand (SM), screenings and sandy silt (ML). Advanced outwash and glacial

till underlay the fill. Glacial till and advanced outwash, colvos sands, had a relative density that ranged from dense to very dense. The glacial till ranged in thickness from about 10 feet to 18 feet in thickness.

Our review of geologic information provided for the pit indicated that a glacial till mantle overlay a deposit of advance outwash. The advanced outwash was the product the pit was mined, which required removal of the overburden, glacial till.

During pit mining activities operations, the advanced outwash material was product removed from the pit area. As a result of the mining activities, the over burden and screenings from the mining operations were later spread over the pit as part of the reclamation. Our subsurface exploration indicated that the overburden material primarily comprised of till fill was spread along the south, west and north sides of the pit. The screenings appeared, which appear to be the more liquefiable material on this site appear to have concentrated north and south of the screening pile locations along the east pond area.

Subsurface Water

In test pits TP-1, TP-24, TP- 25 and TP-26 ground water or wet soils were encountered 10 to 19 feet bgs. NLO summarized the piezometer reading observed through this winter in Table 1 below.

Piezometer Readings Table 1			
B1	B2	B3	B4
1/10/19/dry	1/10/19 (4.0 feet)	1/11/19 (dry)	1/11/19 (7.8 feet)
2/25/2019/dry	2/25/2019 (7.1 feet)	2/25/2019 (17.6 feet)	2/25/2019 (7.4 feet)
3/11/2019/dry	3/11/2019 (9.9 feet)	3/11/2019 (dry)	3/11/2019 (7.8 feet)

Laboratory Testing

To aid in classifying the soils and to determine general soil gradation, laboratory tests were performed on selected representative samples. Phoenix Soil Research in Kingston, Washington was retained to provide geotechnical laboratory analysis. The results of the laboratory testing have been presented in Appendix A.

AVAILABLE GEOLOGIC INFORMATION

Washington Division of Geology and Earth Resource

The Washington Division of Geology and Earth Resource (WDGER), Geologic Map of Washington – Northwest Quadrant, dated 2002, indicates that the site is mapped as Quaternary sediments, dominantly glacial drift and includes alluvium. Glacial till consists of an unsorted, unstratified, highly compacted mixture of clay, silt, sand, gravel and boulders deposited by glacial ice.

USDA Soil Conservation Service

The USDA Soil Conservation Service (SCS) classifies the site’s native soils as (2) and (3) Alderwood very gravelly sandy loam, 6 to 15 and 15 to 30 percent slopes Permeability of this Alderwood soil is moderately rapid above the hardpan and very slow in the pan. The available water capacity is low. The effective rooting depth ranges from 20 to 40 inches for 6 to 15 percent slopes runoff is slow, and the hazard of water erosion is slight and 15 to 30 percent are indicated to have a medium runoff is medium, and the hazard of water erosion is moderate. We have provided the SCS mapping of the area on Figure 3.

Kitsap County Washington, Geologic Map Unit

The Kitsap County Washington, Geologic Map Unit, Washington State Department of Natural Resources, open file Report 2005-3 dated 2005 was utilized for our geologic review of the project area. Geologic Unit Quaternary Alluvium (Q_a), Geologic Age Quaternary, Quaternary unconsolidated or semi-consolidated alluvial clay, silt, sand, gravel, and (or) cobble deposits; locally includes peat, muck, and diatomite; locally includes beach, dune, lacustrine, estuarine, marsh, landslide, lahar, glacial, or colluvial deposits; locally includes volcanoclastic or tephra deposits; locally includes modified land and artificial fill.

SEISMIC

NLO has reviewed the IBC for seismic design criteria for the proposed construction. The site's ground acceleration was determined from the 2015 USGS Earthquake Hazard Program for the Conterminous 48 States. The PGA was based on the following location 47.64949 Latitude and -122.72721 Longitude. The interpolated probabilistic ground motion values (PGA) for Horizontal peak acceleration and spectral acceleration are as provided in Table 2.

Seismic Ground Shaking Summary			
Table 2			
Probability Of exceedance	Approximate Return Period (years)	Spectral Acceleration (g) Period (sec)	
		0.2 sec	1.0 sec
2% in 50 years	2,475	S _s = 1.398	S ₁ = 0.560

Seismic Design Ground Shaking Parameters IBC – 2015

NLO has reviewed the 2015 International Building Code (IBC) for seismic design criteria for the proposed construction. The IBC seismic design parameters for the subject site, include a seismic zone soil profile, **Type D**. Based on the encountered subsurface soil conditions, the recommended seismic design ground shaking parameters are the values in Seismic Parameter (2015 IBC) have been presented in Table 3.

Seismic Design Parameters (2015 IBC)	
Table 3	
Seismic Parameters	Values
Mapped Spectral Acceleration Short Period (S _s)	1.398
Mapped Spectral Acceleration For One Second (S ₁)	0.560
Site Class	D
Short period Site Coefficient (F _a)	1.000
1-second Site Coefficient (F _v)	1.500
MCE Spectral Response Acceleration for short period (S _{MS} =S _s xF _a)	1.398
MCE Spectral Response Acceleration for one second (S _{M1} = S ₁ xF _v)	0.840
Design Spectral Response Acceleration for Short Period (S _{DS} =2/3xS _{MS})	0.932
Design Spectral Response Acceleration for one second (S _{D1} =2/3xS _{M1})	0.560
Design Peak Ground Acceleration (DPA_D =SDS x 0.4)	<u>0.378</u>
The design level earthquake (DLE) utilized the Design Peak Ground Acceleration DPA _D =0.378 based on 2015 IBC Seismic Design Parameters.	

The seismic acceleration was also based on a near crustal one-in-100 year seismic event, with an assumed magnitude of 6.5 occurring below the site. The horizontal ground acceleration (K_h), $K_h=0.2$, was used to make a determination of the site's susceptibility to dynamic loading for an operating level earthquake (OLE).

The ground acceleration was determined from horizontal peak ground accelerations for glacial consolidated soils provided by Kitsap County's Ordinance Regarding Growth Management, Revisions to Title 19 Critical Areas, Seismic Information.

SEISMIC FAULT LINES REVIEW

NLO has reviewed the "Kitsap County, Washington, Fault Lines", dated 2017 and USGS geological mapping. Our review of the above referenced Fault Lines information indicates the following:

Hood Canal Fault

The Hood Canal fault resides roughly 15 miles west of the project area and parallels the Hood Canal in the north to south direction. At the time of this report, there is no information of prehistoric surface-rupturing or reoccurrence intervals to include anticipated magnitude for this fault.

Seattle Fault

The Seattle Fault resides roughly two (2) miles to the south. The Seattle Fault trends east to west from Hood Canal to the Cascades, passing below Bremerton along the Bremerton ship yard, through Manchester and Bainbridge Island into Seattle. This fault is capable of magnitude 7.1 seismic events with the most recent seismic event occurring roughly 1,100 years ago.

Southern Whidbey Island Fault

The Southern Whidbey Island Fault is located roughly 40 to 45 miles to the north of the project area. The fault is roughly as 25 to 30 miles wide and trends from the northwest to southeast along the Puget Sound above the northern tip of Kitsap County to the northern margins of Whidbey Island and trends between Duvall and Monroe. The southern Whidbey Island fault is considered capable of generating strong seismic events on par with the Seattle Fault with magnitude approaching magnitude 7.0 or greater and may have a similar return period.

SOIL LIQUEFACTION

NLO has reviewed Kitsap County Geologically Hazardous Map Seismic Hazards map published dated February 23, 2017. The referenced liquefaction mapping the pit has a high potential of liquefaction. The Seismic Hazards for liquefaction mapping has been provided on Figure 4.

To generate the necessary ground acceleration to initiate liquefaction, an earthquake of magnitude 5.0 or greater is typically needed and the liquefaction process is brought about by seismic waves passing through poorly draining saturated granular soil. As the seismic wave propagates through the stratum, the soil particles at the individual level are packed into a tighter arrangement decreasing the initial void space. Void space is the region between soil particles where the pore water resides. As a result of the decreased void space, the volume decrease has a corresponding water pressure increase also known as pore pressure increase. If the pore pressure is substantial, and cannot be dissipated, the soil fluidizes (or liquefies) and loses load carrying ability.

Liquefaction Assessment

The proposed development is located within a recently reclaimed pit area. Based on its previous use the area has been designated as a potentially liquefiable site per Kitsap County’s geologic hazardous mapping. In order to assess the site’s soil conditions, four (4) boring have been drilled at various locations within the subject area to determine ground water levels and subsurface conditions.

Beginning at a depth of approximately five feet below existing grades with the local groundwater level at a depth of approximately seven and one-half to nine and one-half feet below existing grades. The subsurface data obtained from our exploration program was utilized to perform a liquefaction analysis using LiquefyPro engineering software.

The results of the LiquefyPro analysis provides an understanding of the liquefaction susceptibility of the subject site as well as maximum anticipated liquefaction-induced settlements which could occur due to a design level earthquake (DLE) and operating level earthquake (OLE); the LiquefyPro results are provided in Table 4. The following are the results from the LiquefyPro analysis:

Liquefypro Analysis Result Table 4				
Boring B-1				
	PGA	Magnitude	GW_Depth(ft)	Settlement (in)
DLE	0.378	7.07	dry	1.06
OLE	0.200	6.50	dry	0.21
Boring B-2				
	PGA	Magnitude	GW_Depth(ft)	Settlement (in)
DLE	0.378	7.07	4	1.57
OLE	0.200	6.50	4	0.11
Boring B-3				
	PGA	Magnitude	GW_Depth(ft)	Settlement (in)
DLE	0.378	7.07	17.6	4.46
OLE	0.200	6.50	17.6	1.06
Boring B-4				
	PGA	Magnitude	GW_Depth(ft)	Settlement (in)
DLE	0.378	7.07	7.4	5.23
OLE	0.200	6.50	7.4	2.36

Note: In the area of boring B-3, the water level was observed at 17.6 feet bgs on one (1) occasion. On two other occasions that the piezometer subsurface water level readings, no groundwater was observed at this location. With no ground water, liquefaction settlement is substantially reduced.

CONCLUSIONS AND RECOMMENDATIONS

Following our review of Kitsap Counties Critical Areas Ordinance, Chapter 19.400 Geologic Hazardous Areas. It’s NLO’s opinion that the soils underlying the site could be susceptible to liquefaction during the design level seismic event. The liquefaction can be initiated by the underlying saturated loose fill sand deposits and relatively high groundwater tables as monitored through the winter undergoing the design

seismic event. In the areas of borings B-3 and B-4, a design level earthquake event could potentially experience settlements up to 4.5 inches 5.25 inches, respectively.

Note: In the area of boring B-3, the water level was observed at 17.6 feet bgs on one (1) occasion. On two other occasions that the piezometer subsurface water level readings, no groundwater was observed at this location. With no ground water, liquefaction settlement is substantially reduced.

As identified by borings B-1 and B-3, our subsurface exploration program encountered a dense roughly five feet thick surface layer of silty sand along the northern and southern portion of the subject site. In our opinion, this dense material should provide adequate foundation appeared suitable for foundation support if the continuous spread footings having isolated footings connected into a grade beam system below the residence. The goal of integrating the foundation elements is to minimize differential settlement during a design seismic event.

The drilling operations has also indicated the area of Boring B-4 was underlain by loose soils with a high liquefaction potential. Given the loose soil conditions encountered at this location, over excavation and backfill may be required to mitigate future settlement in conjunction with a 2 feet thick fill mat comprised with crushed rock underlain by a geogrid reinforcement. The structural fill mat as described later in this report could partially mitigate differential settlement and damage to the proposed structures. The structural fill mat will protect the building foundations from direct bearing loss and reduce liquefaction-induced differential settlements of up to two inches. In lieu the crushed rock matt underlain by geogrid reinforcement, pin piles could be an alternative for structural support in this area or a post-tensioned slab foundations also seem appropriate. Another option depending on present site grade modifications and grading operations could implement a preload/surcharge program.

In the area of B-2, the subsurface exploration encountered loose wet fill soils located along a slope area overlooking the pond area. In our opinion, this area may be susceptible to both slope instability and liquefaction. NLO recommends that this area should be field verified to the north and south to determine the extent of the loose wet fill with additional test pits to delineate the area and depth. To reduce the liquefaction and slope instability potential along this area, we are recommending that this area should be over excavated and backfilled with on-site fills benched into the hill side and compacted to the structural fill requirements provided in this report. Given the amount of subsurface water encountered at this location, NLO recommends that subsurface drainage system will need to be placed within the fill slope during installation.

This report is preliminary and developed to provide insight on the site's liquefaction potential. If portions of the ponds are being considered for development, Additional subsurface work could be necessary to determine the extent of liquefiable soils that may reside in these areas. NLO also recommends that the stock pile area should also be further investigated once site grades have been determined at that location.

This study has been prepared for specific application to this project only and in a manner consistent with that level of care and skill ordinarily exercised by other members of the profession currently practicing under similar conditions in this area for the exclusive use NLO and their representatives. No warranty, expressed or implied, is made. This study, in its entirety, should be included in the project contract documents for the information of the contractor.

SITE PREPARATION AND GRADING

All pavement, slab-on-grade, fill and/or building areas should be stripped of all organic soil, existing fill and debris. However, deeper excavations may be required to remove previously placed uncontrolled fill disclosed during proof roll operations. Deeper excavations may also be required to remove large tree root-balls, old foundations, "filled in basement area", septic tanks and associated drain fields. Stripped soils, contaminated with organics or debris, should be wasted off site or used in landscape areas.

After site stripping and previously placed unsuitable fill removed, NLO recommends the newly exposed subgrade should be proof rolled in parking lot areas. If necessary compaction may be necessary to achieve a firm, unyielding condition. As a preliminary guideline the equipment should be of appropriate size and type capable of developing a minimum dynamic compaction effort rating of at least 25,000 pounds with a static smooth drum weight of 13,000 pounds. Compaction of the stripped subgrade should be continued until field density tests indicate a minimum compaction of 95% of the maximum dry density, as determined by ASTM method D-1557, has been achieved in all fill, building, roadway, and parking areas. Soft or weaving areas disclosed during proof rolling shall be excavated and replaced with compacted structural fill. Areas, which are to be filled to bring the pavement grades up to the desired elevation, should be filled with compacted granular material free from roots, trash or other deleterious materials.

Native soils with a fine content greater than 5 percent passing the 200 sieve will degrade if exposed to excessive moisture, and compaction and grading of this material will be difficult or impossible if soil moisture significantly increases. If used, the on-site silt and clay soils utilized for structural fill should be moisture conditioned to within plus/minus 2 percent of the optimum moisture content, and compacted to 95 percent of the maximum dry density based on the Modified Proctor ASTM 1557. Additional fill layers shall not be placed until the previous lift meets the compaction requirements presented in this report. If compaction cannot be achieved structural fill may be required as discussed in the next section.

STRUCTURAL FILL

Structural fill is defined as compacted fill placed under buildings or pavements that consist of free draining gravelly sand having a maximum size of 1-1/2 inches and no more than 5.0% fines passing the No. 200 sieve. Soils with a fine content greater than 5 percent passing the 200 sieve will degrade if exposed to excessive moisture and will not meet recommended compaction requirements. All imported fill material should conform to the above recommendation regardless of the site's weather conditions. All structural fill should be placed on a firm, properly prepared subgrade in loose layers approximately 8 inches in thickness, conditioned to a moisture content suitable for compaction, and compacted to 95% of the maximum dry density as determined by ASTM D-1557 (Modified Proctor). All Structural fill material should be submitted for approval to the Geotechnical Engineer at least 48 hours prior to delivery to the site.

FOUNDATION

In general, building areas shall be stripped of all sod, organic soil, existing fill and debris. In most areas, a stripping depth or overexcavation depth of about half (1/2) foot to two (2) feet should be anticipated for the areas of Boring B-1 and B-3. However our investigation indicated that loose uncontrolled fill mixed or layered with topsoil and organics that ranged up to a maximum thickness to 20 feet as observed in boring B-2 feet as measured below present site grades. The thicker fills encountered in the general area of boring B-2 will require removal prior to structural fill placement. Following site stripping, and prior to the fill placement, the exposed building subgrade may require reworking the upper one to two feet of the subgrade to be compacted to a firm, unyielding condition.

For frost protections, footings should have a minimum embedment depth of 18 inches below adjacent grade. A base friction coefficient of 0.25 is considered appropriate for the expected foundations residing on structural fill. An ultimate passive equivalent fluid earth pressure for passive pressures, considering a horizontal ground surface, of 250 pcf is available to develop additional resistance to lateral pressures. Passive pressures should be ignored or appropriately reduced in areas where the ground slopes downward on the resisting side of the wall within 4 times the footing embedment depth of the wall. The upper two feet of soil should be neglected when calculating the passive resistance. A 1/3 increase in the above value may be used for short duration, wind, and seismic loads.

Compaction of the stripped subgrade should be continued until field density tests show that a minimum compaction of 95% of the maximum dry density, as determined by ASTM method D-1557, has been achieved in all fill, building, roadway, and parking areas, or NLO indicates a firm unyielding subgrade has been achieved.

For the building's continuous and column footing system bearing on properly compacted structural fill or dense undisturbed native granular soils an allowable bearing pressure of 1,500 pounds per square foot (psf) can be used.

NLO recommends field verification of the foundation subgrade areas prior to placement of concrete formwork or rebar. The field verification of the footing's foundation subgrade area is necessary to make certain that the foundation have been established on dense to very dense soil condition or on properly compacted crushed rock or structural fill. During our field verification, NLO's representative can also provide recommendations for over excavation and backfill.

Foundations Settlement

Based on an allowable soil bearing pressure of 1,500 psf, total settlement in the range of one inch is anticipated with differential settlement of about ½ inch over a span distance of 50 linear feet, which does not include liquefaction settlement.

Crushed Rock Pad

In the vicinity of Boring B-4, NLO recommends supporting the entire building pad area on a two feet minimum thickness of crushed rock with a particle size of about 2 inches similar to railroad ballast as measured below anticipated foundation level. The rock pad should extend two (2) feet beyond the furthest protrusion of the buildings perimeter foundation system. Prior to rock placement, the subsequent excavation shall be compacted to 95 percent of the ASTM 1557 - Modified Proctor. After compaction of the subsequent subgrade has been verified, the subgrade area can be overlain with a bi-direction geogrid reinforcement.

If the subgrade is prepared per the recommendations discussed in this report, the proposed single-family structures can be supported on conventional spread and continuous footings placed on the crushed rock base. The purpose of supporting the building footprint in this manner will provide a uniform support along the base of the structure is to mitigate differential settlement.

Provided the structure will be supported atop a crushed rock pad as described above (and below), the following parameters can be used for design of the new foundations:

- Allowable soil bearing capacity 1,500 psf
- Passive earth pressure 300 pcf (equivalent fluid) coefficient of friction, and
- A one-third increase in the allowable soil bearing capacity can be assumed for short-term wind seismic loading conditions.

With structural loading as expected, total settlement in the range of one inch is anticipated, with differential settlement of about one-half inch. The majority of the settlements should occur during construction, as dead loads are applied.

Building Pad and Footing Subgrade Preparation

As described above, we recommend supporting the proposed structure on a crushed rock base comprised of two (2) feet of crushed clean rock underlain by a bi-directional geogrid placed along the native subgrade soils. The process for crushed rock placement base should be prepared as follows:

- The building pad area shall be stripped of topsoil and organic laden soils two (2) feet beyond the anticipated building pad area to expose upper native silty soils.
- Place woven geotextile across entire building pad and footing subgrade area.
- Place two feet of crushed rock and compact with dual drum roller or similar compaction device until NLO representative determines the crushed rock has been properly seated.
- Construct footings and building slab directly atop the two feet of new structural fill and place additional structural fill to achieve footing and slab subgrade elevations, as necessary.
- NLO should be on-site to observe building pad and subgrade preparation activities.

SLAB-ON-GRADE

If slab-on-grade floors are utilized, it will be necessary to connect or structural support the floor slab into the strip and column foundations. A capillary break/drainage layer consisting of six inches of pea gravel, or clean crushed rock should be placed below the floor slab. The capillary break material should contain less than 1.0% material passing a U.S. No. 200 sieve and less than 4.0% material passing a U.S. No. 10 sieve. A visqueen vapor barrier having a minimum thickness of 6-mils should be placed between the capillary break and the floor slab. We understand that a sand cushion between the vapor barrier and the base of the slab may improve the curing of the slab concrete. If a sand cushion is placed between the capillary break material or the vapor barrier and the slab, it should not contain free moisture when the slab is constructed. Excess moisture in the cushion could cause impervious floor coverings to bubble.

SURFACE RUNOFF AND PERCHED GROUND WATER

Only minor storm water related problems are anticipated if site grading and preparation are undertaken during the normally drier portions of the year. If site work is undertaken during wet weather it should be

expected that the near surface silty and fine-grained soils would become over-saturated and unworkable. If the site work is undertaken during wet weather the contractor should be fully prepared to deal with soil and water problems normally encountered in these materials during wet weather work, including the filtering of runoff, as needed to prevent the siltation of down slope areas. To aid in minimizing potential erosion, it is recommended that the site not be stripped and left without erosion protection for an extended period of time prior to the actual start of construction and/or landscaping. Silt fencing and other erosion control devices and measures may be required to control water runoff over slope areas and sediment transport off the site.

EROSION

It is our experience that this risk of erosion can be mitigated through normal landscaping and the control of surface runoff. During construction and until fully surfaced and/or landscaped, the exposed site soils may be subject to some erosion. Erosion of the exposed soils would be most noticeable during periods of intense rainfall and may be controlled by the use of normal erosion control measures, i.e., silt fences, hay bales, mulching.

In a disturbed condition, the site soils may be eroded by channelized water or storm runoff from sheet flow. Therefore, it is recommended that all site preparation and excavation work be completed during the normally drier portion of the year. During periods of heavy rainfall, ditching should be used to divert water away from stripped areas and visqueen should be used to cover the slopes and soil stockpiles to aid in preventing excessive surface erosion. This covering also aids in preventing infiltration of water into the unprotected soils. All disturbed soil areas and slopes should be replanted with fast-growing, deep-rooted grass, shrubs and other ground cover as soon after final grading as possible. If the vegetation is not fully established prior to the onset of wet weather, the slopes should be covered with visqueen to aid in preventing excessive erosion and water infiltration.

TEMPORARY AND PERMANENT SLOPES

As a preliminary guideline for temporary cuts less than 10 feet in height not including footing subgrade areas, we recommend temporary slopes be made no steeper than 1H:1V for the dense granular soils and no steeper than 2H:1V in medium dense soils or structural fill placed and compacted as outlined above. For temporary cut slopes in existing fill, topsoil, or loose materials exceeding a vertical height of 10 feet, we recommend temporary slopes no steeper than 1 1/2H:1V for the full height of the cut. Temporary slopes or excavations should be benched as required by safety regulations in effect at the time of construction. These temporary slope recommendations are for native soils and fill materials; flatter slopes may be required in wet weather or if soil conditions other than those previously described are encountered. The contractor should be aware that slope height, slope inclination, and excavation depths (including utility trench excavations) should in no case exceed those specified in local, state, or federal safety regulations; e.g., OSHA Health and Safety Standards for Excavations, 29 CFR Part 1926, or successor regulations. Such regulations are strictly enforced and, if not followed, the owner, the contractor, or the earthwork or utility subcontractors could be liable for substantial penalties. The contractor should be made responsible for the stability of all excavations and slopes during construction because they are continually on site and can observe the stability of the exposed soils. In addition, the contractor should be prepared to shore any unstable slope area and provide shoring as required by local, state, or federal laws or codes. The provision of shoring design recommendations is beyond the authorized scope of this report.

REPORT LIMITATIONS

This report has been prepared for the client regarding the subject property. Information presented in this report has been collected and interpreted in a manner consistent with the level of care and skill ordinarily exercised by members of the profession currently practicing under similar conditions, and in accordance with sound and generally accepted principles consistent with normal consulting practice. No other warranty, expressed or implied, including (but not limited to) any warranty or merchantability or fitness for a particular use has been made.

In the event that change in the nature, design, or location of the proposed construction is made, or any physical changes to the site occur, recommendations are not be considered valid unless the changes are reviewed by NLO and conclusions of this report are modified or verified in writing.

NLO should be retained to provide geotechnical services during construction. This is to observe compliance with the design concepts, specifications or recommendations and to allow design changes in the event subsurface conditions differ from those anticipated prior to the start of construction. We do not accept responsibility for the performance of the foundation or earthwork unless we are retained to review the construction drawings and specifications, and to provide construction observation.

If we can be of further assistance or if you have any questions regarding this project, please contact our office.

Sincerely,


Wesley R. Johnson, P.E.
Geotechnical Division Manager



Attachments:

Pit Area	FIGURE 1
Outfall Area	FIGURE 2
SCS Soil Mapping	FIGURE 3
Seismic Mapping	FIGURE 4
Test Pit Logs	FIGURES 5 TO 35
Lab Results	FIGURES 36 TO 38

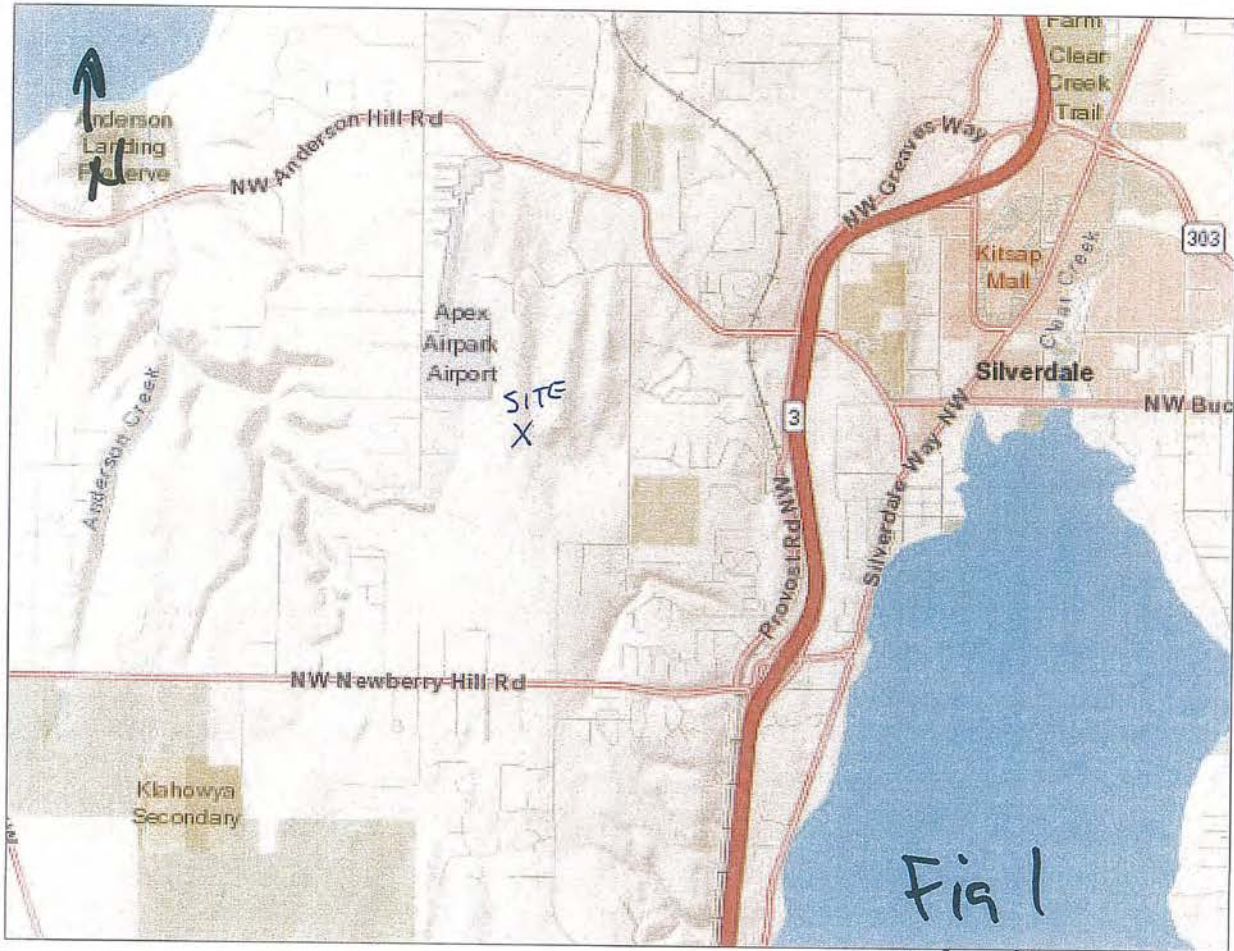


Fig 1
Vicinity MAP

Comment 2.47C

Site Sketch
Fig 2

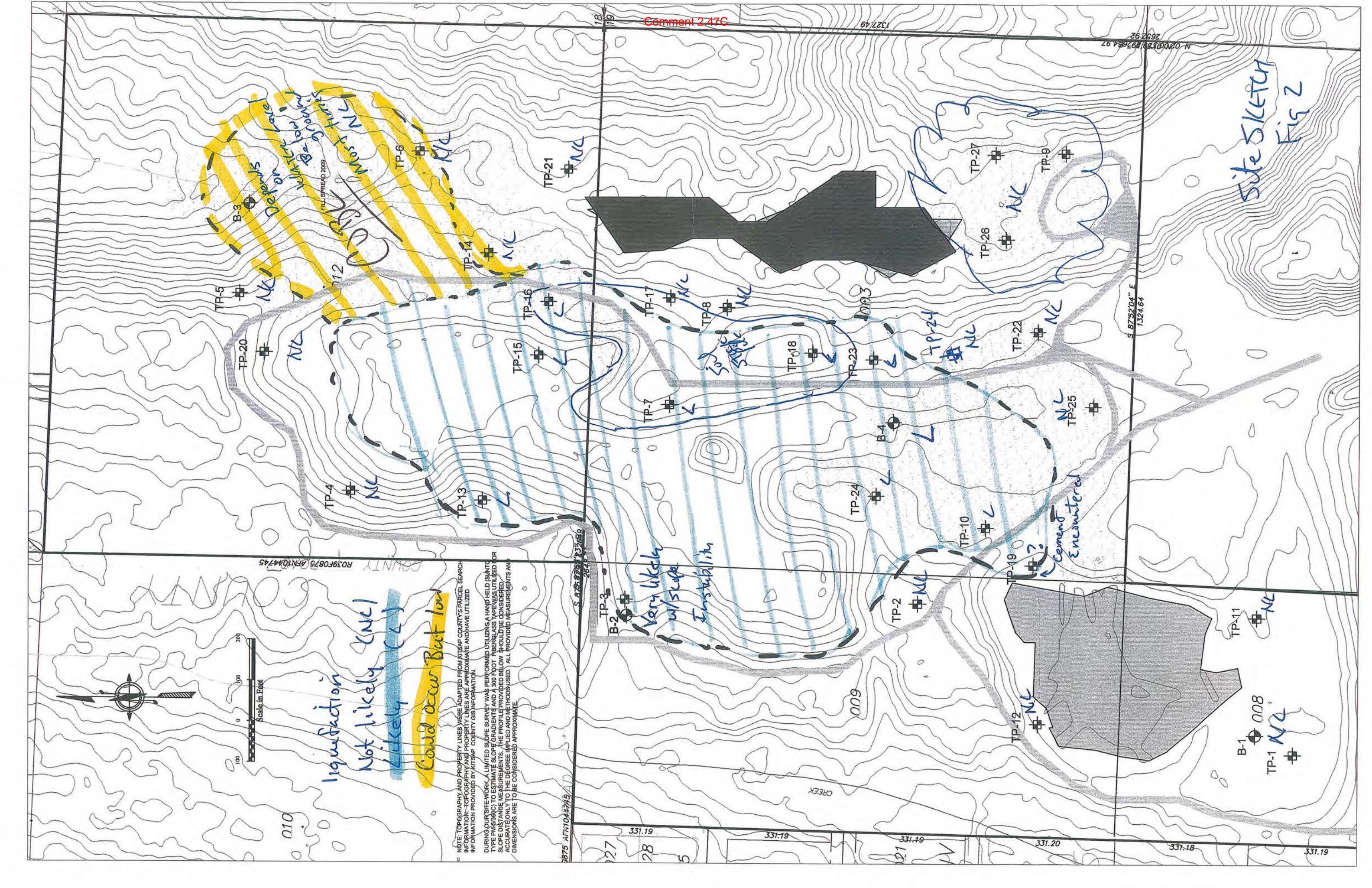
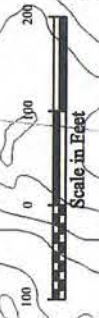
Most of this area depends on water level
Be low
N 02°03'39" W 2632.92
S 87°52'04" E 1324.64

liquefaction
Not likely (NC)
Likely (L)
Could occur But low

Very likely w/ slope
Instability

Cement Encountered

NOTE: TOPOGRAPHY AND PROPERTY LINES WERE ADAPTED FROM KITSAP COUNTY'S PARCEL SEARCH INFORMATION - TOPOGRAPHY AND PROPERTY LINES ARE APPROXIMATE AND HAVE UTILIZED INFORMATION PROVIDED BY KITSAP COUNTY GIS INFORMATION.
DURING OUR SITE WORK, A LIMITED SLOPE SURVEY WAS PERFORMED UTILIZING A HAND HELD (SUNIC TYPE PA9360C) TO ESTIMATE SLOPE GRADIENTS AND A 300 FOOT FIBERGLASS TAPE WAS UTILIZED FOR SLOPE DISTANCE MEASUREMENTS. THE PROFILE PROVIDED BELOW SHOULD BE CONSIDERED ACCURATE ONLY TO THE DEGREE IMPLIED AND METHODS USED. ALL PROVIDED MEASUREMENTS AND DIMENSIONS ARE TO BE CONSIDERED APPROXIMATE.



R039F0875 AR1044745

S 85°02'13" W 2841.13
S 85°02'13" W 2841.13

331.19 331.19 331.19 331.20 331.18 331.19

010

009

CREEK

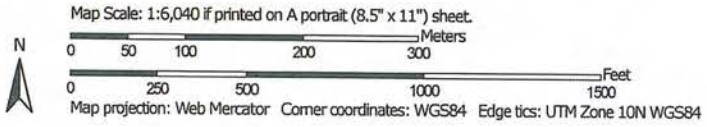
008

008

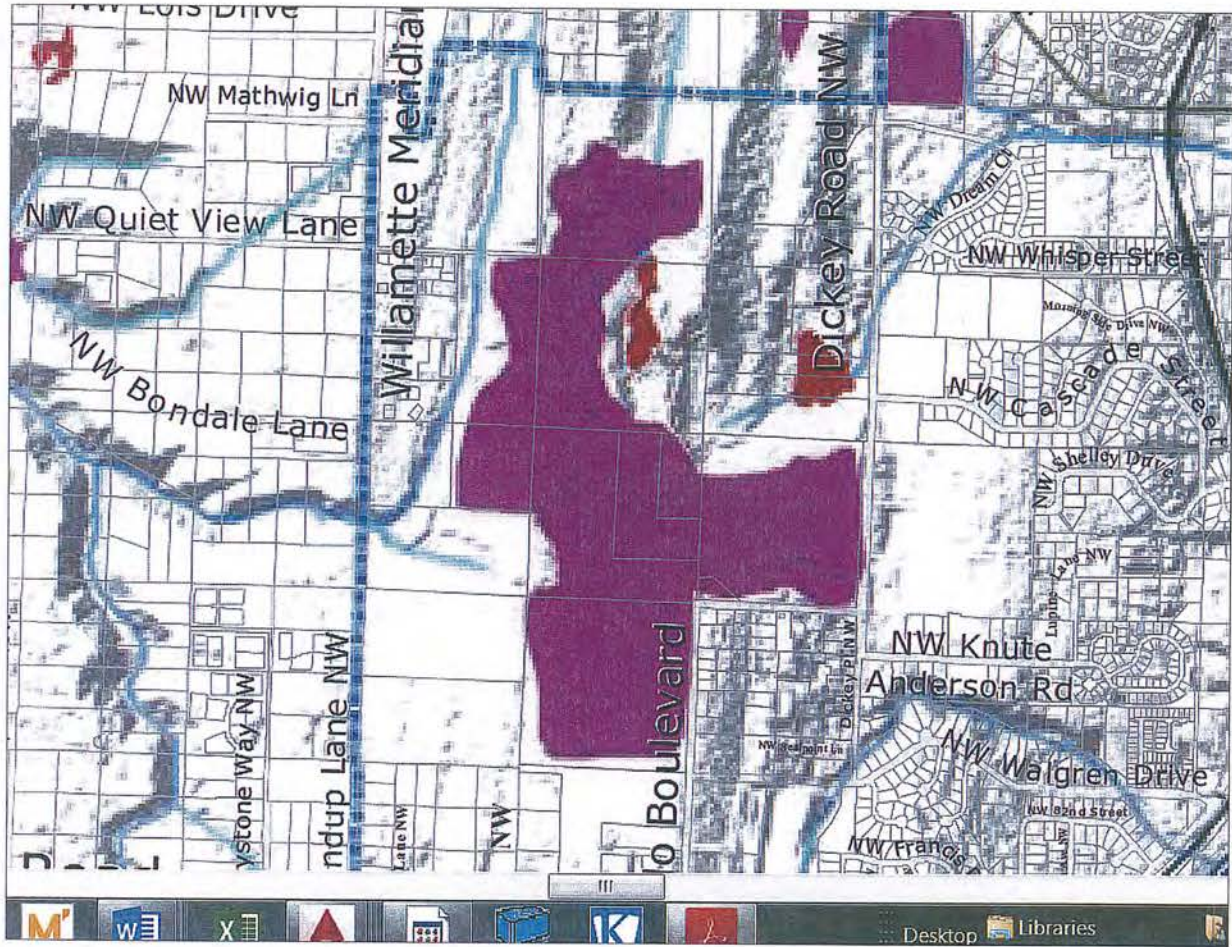
Soil Map—Kitsap County Area, Washington




Soil Map may not be valid at this scale.



SCS
Fig 3



Seismic Fig 4
MAP

 N.L. Olson & Associates, Inc. Engineering, Planning and Surveying 2453 BETHEL AVENUE P.O. BOX 637 PORT ORCHARD, WASHINGTON 98366-0637	<h2 style="margin:0;">Test Pit Log</h2> Dickey Pit 8857 Dickey Road. NW, Silverdale, WA
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Job Number: 10479	Logged By: SMC	Subsurface Exploration Start Date: Sept 17, 2018 End Date: Sept 19, 2018	Ground Surface Elevation 435	Test Pit Number TP-1	Page 1 of 1
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General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:
		TPSL			Topsoil 2" to 4"
2	[Symbol]		1		Fill: Brownish Gray Silty SAND with Gravel, Medium Dense, Moist (Till Fill) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded to Rounded Gravel and Cobbles
			2		
			3		
			4		
			5		
			6		
			7		
7	[Symbol]		8		- Trace to No gravel (Fill) - Fine Grained Sand - Pocket Pen 3.5 - At 9', Areas of varying density from loose to medium dense - At 10', becomes Wet
			9		
		SM	10		
			11		
			12		
			13		
			14		
			15		
			16		
			17		
18	[Symbol]		18		At 18', Becomes loose
			19		
			20		
			21		End of Test Pit at 20 feet
			22		

Contractor Vet Industrial	Operators Name Shannon	Sampling Method Grab	Drawn By: SMC	Date Sept 19, 2018	Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____
Equipment Case CX240B Excavator		Groundwater Elevation Wet soil 10 feet	Checked By: WRJ	Date Month XX, 2017	
Notes: GPS Location: 47.64514 degrees N, 122.73182 W			Revision By: SMC	Date Month XX, 2017	



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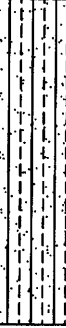
Test Pit Log

Dickey Pit
 8857 Dickey Road. NW,
 Silverdale, WA

Job Number: 10479		Logged By: SMC		Subsurface Exploration		Ground Surface Elevation 460	Test Pit Number TP-2	Page 1 of 1
				Start Date: Sept 17, 2018	End Date: Sept 19, 2018			
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:			
		TPSL			Topsoil 2" to 4"			
4		SM	1		Native: Light Brown Silty SAND with Gravel, Dense to Very Dense, Moist - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded to Rounded Gravel and Cobbles			
2								
3								
4								
5								
6								
7								
8								
9								
10								
10			11		End of Test Pit at 10.5 feet (Refusal)			
12								
13								
14								
15								
16								
17								
18								
19								
20								
21								
22								
Contractor Vet Industrial	Operators Name Shannon		Sampling Method Grab		Drawn By: SMC	Date Sept 19, 2018		
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected			Checked By: WRJ	Date Month XX, 2017		
Notes: GPS Location: 47.64816 degrees N, 122.73028 W					Revision By: SMC	Date Month XX, 2017		
					Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____			

Fig 6

 N.L. Olson & Associates, Inc. Engineering, Planning and Surveying 2453 BETHEL AVENUE P.O. BOX 637 PORT ORCHARD, WASHINGTON 98366-0637				Test Pit Log Dickey Pit 8857 Dickey Road. NW, Silverdale, WA			
Job Number: 10479	Logged By: SMC	Subsurface Exploration		Ground Surface Elevation 440	Test Pit Number TP-3	Page 1 of 1	
		Start Date: Sept 17, 2018	End Date: Sept 19, 2018				
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:		
		TPSL			Topsoil 2" to 4"		
2		SM	1		Fill: Light Brown Silty SAND with Gravel, Medium Dense, Moist - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded to Rounded Gravel and Cobbles At 6', Wood Debris (Roots/Branches up to 6" diameter)		
			2				
			3				
			4				
			5				
			6				
			7				
			8				
			9				
			10				
			11				
12			12				
			13				
			14				
			15				
			16				
			17				
17		SP	18				
18			19				
			20				
			21				
			22				
					End of Test Pit at 20 feet		
Contractor Vet Industrial		Operators Name Shannon	Sampling Method Grab		Drawn By: SMC	Date Sept 19, 2018	
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected		Checked By: WRJ	Date Month XX, 2017	Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____	
Notes: GPS Location: 47.65014 degrees N, 122.73027 W		Revision By: SMC	Date Month XX, 2017				

 N.L. Olson & Associates, Inc. Engineering, Planning and Surveying 2453 BETHEL AVENUE P.O. BOX 637 PORT ORCHARD, WASHINGTON 98366-0637				<h2 style="margin: 0;">Test Pit Log</h2> Dickey Pit 8857 Dickey Road. NW, Silverdale, WA			
Job Number: 10479	Logged By: SMC	Subsurface Exploration		Ground Surface Elevation 445	Test Pit Number TP-4	Page 1 of 1	
		Start Date: Sept 17, 2018	End Date: Sept 19, 2018				
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:		
		TPSL			Topsoil 2" to 4"		
4		SM	1 2 3 4 5 6 7		Native: Light Brown Silty SAND with Gravel, Very Dense, Moist (Glacial Till) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles		
			8 9 10 11 12 13 14 15 16 17 18 19 20 21 22		End of Test Pit at 7 feet		
Contractor Vet Industrial		Operators Name Shannon		Sampling Method Grab		Drawn By: SMC	Date Sept 19, 2018
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected		Checked By: WRJ		Date Month XX, 2017	
Notes: GPS Location: 47.65212 degrees N, 122.72925 W				Revision By: SMC		Date Month XX, 2017	
Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____							




 N.L. Olson & Associates, Inc. Engineering, Planning and Surveying 2453 BETHEL AVENUE P.O. BOX 637 PORT ORCHARD, WASHINGTON 98366-0637				Test Pit Log Dickey Pit 8857 Dickey Road. NW, Silverdale, WA			
Job Number: 10479	Logged By: SMC	Subsurface Exploration		Ground Surface Elevation 450	Test Pit Number TP-5	Page 1 of 1	
		Start Date: Sept 17, 2018	End Date: Sept 19, 2018				
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:		
		TPSL			Topsoil 2" to 4"		
1			1		Fill: Light Brown Silty SAND with Gravel, Dense, Moist - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles becomes loose to medium dense - Trace to No gravel - Fine Grained Sand - Pocket Pen 2.5 tsf - May contain sandy silt fill layers between 6 to 22 feet Organics content (Roots), less than 5% to 14'		
			2				
			3				
			4				
5			5				
			6				
			7				
		SM	8				
			9				
10			10				
			11				
			12				
			13				
			14				
			15				
			16				
			17				
			18				
			19				
			20				
			21				
			22				
End of Test Pit at 22 feet							
Contractor Vet Industrial		Operators Name Shannon	Sampling Method Grab		Drawn By: SMC	Date Sept 19, 2018	
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected		Checked By: WRJ	Date Month XX, 2017	Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____	
Notes: GPS Location: 47.65212 degrees N, 122.72925 W			Revision By: SMC	Date Month XX, 2017			

Fig 9

 N.L. Olson & Associates, Inc. Engineering, Planning and Surveying 2453 BETHEL AVENUE P.O. BOX 637 PORT ORCHARD, WASHINGTON 98366-0637				<h2 style="margin: 0;">Test Pit Log</h2> <p style="margin: 0;">Dickey Pit 8857 Dickey Road. NW, Silverdale, WA</p>							
Job Number: 10479	Logged By: SMC	Subsurface Exploration Start Date: Sept 17, 2018 End Date: Sept 19, 2018		Ground Surface Elevation 430	Test Pit Number TP-6	Page 1 of 1					
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:						
		TPSL			Topsoil 1" to 3"						
3		SM	1		<p>Fill: Light Brown Silty SAND with Gravel, Dense, Moist</p> <ul style="list-style-type: none"> - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles <p>Contains Gray Sandy SILT fill layers, loose to medium dense</p> <ul style="list-style-type: none"> - Trace to No gravel - Fine Grained Sand - Organics to 9.5' (roots) <ul style="list-style-type: none"> - Fine Grained Sand - Pocket Pen 1.5 <i>e = f</i> <p>Brownish Gray Silty SAND, Dense, Moist (<i>fill</i>)</p> <ul style="list-style-type: none"> - Fine Grained Sand 						
2											
3											
4											
5											
6											
7											
8											
9											
10											
11											
12											
13											
14											
15											
16											
17											
18											
19											
						20		End of Test Pit at 20 feet			
			21								
			22								
Contractor Vet Industrial		Operators Name Shannon		Sampling Method Grab		Drawn By: SMC		Date Sept 19, 2018		Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____	
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected		Checked By: WRJ		Date Month XX, 2017					
Notes: GPS Location: 47.65115 degrees N, 122.72579 W				Revision By: SMC		Date Month XX, 2017					



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Test Pit Log

Dickey Pit
 8857 Dickey Road. NW,
 Silverdale, WA

Job Number: 10479		Logged By: SMC		Subsurface Exploration Start Date: Sept 17, 2018 End Date: Sept 19, 2018		Ground Surface Elevation 445	Test Pit Number TP-7	Page 1 of 1
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:			
		TPSL			Topsoil 1" to 3"			
		SM	1- 2- 3-		Light Brown Silty SAND with Gravel, Dense, Moist (Till Fill) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles			
4		SM	4- 5- 6- 7- 8- 9- 10- 11- 12- 13- 14- 15- 16- 17- 18- 19- 20-		Gray Sandy SILT/LAYERS <i>LOOSE TO medium dense Moist</i> - Trace to No gravel - Fine Grained Sand - Organics to 9.5' (roots)			
14			14- 15- 16- 17- 18- 19- 20-		Contains Gray Sandy SILT fill layers - Fine Grained Sand - Pocket Pen 1.5 <i>cf</i>			
			21- 22-		End of Test Pit at 20 feet			
Contractor Vet Industrial		Operators Name Shannon		Sampling Method Grab		Drawn By: SMC	Date Sept 19, 2018	
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected				Checked By: WRJ	Date Month XX, 2017	
Notes:						Revision By: SMC	Date Month XX, 2017	
Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____								

Fig 11

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 PORT ORCHARD, WASHINGTON 98366-0637

Test Pit Log
 Dickey Pit
 8857 Dickey Road. NW,
 Silverdale, WA

Job Number: 10479		Logged By: SMC		Subsurface Exploration Start Date: Sept 17, 2018 End Date: Sept 19, 2018		Ground Surface Elevation 440	Test Pit Number TP-8	Page 1 of 1
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:			
		TPSL			Topsoil 2" to 4"			
4		SM	1		Light Brown Silty SAND with Gravel, Dense, Moist (Till Fill)			
			2		- Fine Grained Sand			
			3		- Trace Gravel and Cobbles			
			4		- Subrounded Gravel and Cobbles			
			5		Contains Gray Sandy SILT fill layers			
			6		- Trace to No gravel			
			7		- Fine Grained Sand			
			8		- At 7', Gray/Brown Mottled			
			9		- Organics Less Than 5% between 7'-10'			
			10					
			11					
12								
13								
14		Organics (Roots, Tree Debris) from 14'-17'						
15								
16								
17								
18								
19								
20								
21		End of Test Pit at 20 feet						
22								
Contractor Vet Industrial		Operators Name Shannon		Sampling Method Grab		Drawn By: SMC		Date Sept 19, 2018
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected		Checked By: WRJ		Date Month XX, 2017		Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____
Notes: GPS Location: 47.64949 degrees N, 122.72721 W				Revision By: SMC		Date Month XX, 2017		



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 P.O. BOX 637
 PORT ORCHARD, WASHINGTON 98366-0637

Test Pit Log

Dickey Pit
 8857 Dickey Road. NW,
 Silverdale, WA

Job Number: 10479		Logged By: SMC		Subsurface Exploration Start Date: Sept 17, 2018 End Date: Sept 19, 2018		Ground Surface Elevation 410		Test Pit Number TP-9		Page 1 of 1	
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:						
		TPSL			Topsoil 2" to 4"						
4	[Symbol]	SM	1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20	Light Brown Silty SAND with Gravel, Dense, Moist (Till Fill) - Fine Grained Sand - Trace Gravel and Cobbles -Subrounded Gravel and Cobbles Contains Gray Sandy SILT fill layers - Trace to No gravel - Fine Grained Sand - Density Increases at Depth Starting at 12'							
10	[Symbol]	SM	21 22	End of Test Pit at 20 feet							
Contractor Vet Industrial		Operators Name Shannon		Sampling Method Grab		Drawn By: SMC		Date Sept 19, 2018		Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____	
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected		Checked By: WRJ		Date Month XX, 2017					
Notes: GPS Location: 47.64744 degrees N, 122.72618 W		Revision By: SMC		Date Month XX, 2017							




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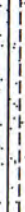
Test Pit Log

Dickey Pit
 8857 Dickey Road. NW,
 Silverdale, WA

Job Number: 10479		Logged By: SMC		Subsurface Exploration		Ground Surface Elevation 445	Test Pit Number TP-10	Page 1 of 1		
				Start Date: Sept 17, 2018	End Date: Sept 19, 2018					
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:					
		TPSL			Topsoil 2" to 4"					
3		SM	1		Light Brown Silty SAND with Gravel, Dense to Very Dense, Moist (Till Fill)					
			2		- Fine Grained Sand					
			3		- Trace Gravel and Cobbles					
			4		- Subrounded Gravel and Cobbles					
			5		- Bcomes Very Dense at 3'					
6					6					
7					7		Contains Gray Sandy SILT fill layers between 6.5 to 12 feet			
					8		- Trace to No gravel			
					9		- Fine Grained Sand			
					10		- Density Increases at Depth Starting at 12'			
12					12		<i>Gravelly</i>			
13					13		Brown Silty SAND, Soft to Medium Stiff, Wet (Fill)			
					14		- Trace to No gravel			
					15		- Fine Grained Sand			
					16					
					17					
					18					
					19					
					20		End of Test Pit at 20 feet			
					21					
			22							
Contractor Vet Industrial		Operators Name Shannon		Sampling Method Grab		Drawn By: SMC		Date Sept 19, 2018		
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected <i>12</i>		Checked By: WRJ		Date Month XX, 2017		Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____		
Notes: GPS Location: 47.63744 degrees N, 122.72905 W				Revision By: SMC		Date Month XX, 2017				

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Job Number: 10479	Logged By: SMC	Subsurface Exploration		Ground Surface Elevation 480	Test Pit Number TP-12	Page 1 of 1
		Start Date: Sept 17, 2018	End Date: Sept 19, 2018			

General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:
		TPSL			Topsoil 2" to 4"
3			1 2 3 4 5 6 7 8		Light Brown Silty SAND with Gravel, Dense to Very Dense, Moist (Till Fill) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles - Bcomes Very Dense at 3'
10		SM	9 10 11 12 13 14 15		At 10', Becomes Brown, Density Increases to Medium Dense to Dense, Organics (Wood Debris) from 10'-13'
16			16 17 18 19		
			20 21 22		End of Test Pit at 20 feet

Contractor Vet Industrial	Operators Name Shannon	Sampling Method Grab	Drawn By: SMC	Date Sept 19, 2018	Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected	Checked By: WRJ	Date Month XX, 2017	
Notes: GPS Location: 47.64740 degrees N, 122.73139 W			Revision By: SMC	Date Month XX, 2017	




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Test Pit Log

Dickey Pit
 8857 Dickey Road. NW,
 Silverdale, WA

Job Number: 10479		Logged By: SMC		Subsurface Exploration Start Date: Sept 17, 2018 End Date: Sept 19, 2018		Ground Surface Elevation 465	Test Pit Number TP-13	Page 1 of 1		
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:					
		TPSL			Topsoil 2" to 4"					
3		SM	1		Light Brown Silty SAND with Gravel, Medium Dense, Moist (Till Fill) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles Organics (Roots and Tree Debris) to 6'					
			2							
			3							
			4							
			5							
			6							
7					7		Contains Gray Sandy SILT fill layers - Trace Cobbles - Fine Grained Sand - Subrounded to Rounded Gravel and Cobbles - Pocket Pen 0.5			
			8							
			9							
			10							
			11							
			12							
			13							
			14							
			15							
			16							
			17							
			18							
			19							
19					20					
	21									
	22									
Contractor Vet Industrial	Operators Name Shannon	Sampling Method Grab		Drawn By: SMC	Date Sept 19, 2018					
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected		Checked By: WRJ	Date Month XX, 2017					
Notes: GPS Location: 47.65080 degrees N, 122.72944 W				Revision By: SMC	Date Month XX, 2017					
								Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____		

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Job Number: 10479	Logged By: SMC	Subsurface Exploration		Ground Surface Elevation 435	Test Pit Number TP-14	Page 1 of 1			
		Start Date: Sept 17, 2018	End Date: Sept 19, 2018						
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:				
		TPSL			Topsoil 2" to 4"				
3			1		Light Brown Silty SAND with Gravel, Medium Dense, Moist (Till Fill) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles Organics (Roots and Tree Debris) to 6'				
			2						
			3						
			4						
			5						
			6						
7			7		Contains Gray Sandy SILT fill layers - Trace Cobbles - Fine Grained Sand - Subrounded to Rounded Gravel and Cobbles - Pocket Pen 0.5 At 14', Caving At 18', Increased Density				
			8						
			9						
			10						
			11						
			12						
			13						
			14						
			15						
			16						
			17						
			18						
19			19						
			20		End of Test Pit at 20 feet				
			21						
			22						
Contractor Vet Industrial		Operators Name Shannon		Sampling Method Grab		Drawn By: SMC	Date Sept 19, 2018	Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____	
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected		Checked By: WRJ		Date Month XX, 2017			
Notes: GPS Location: 47.65176 degrees N, 122.72685 W				Revision By: SMC		Date Month XX, 2017			



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Test Pit Log

Dickey Pit
 8857 Dickey Road. NW,
 Silverdale, WA

Job Number: 10479	Logged By: SMC	Subsurface Exploration		Ground Surface Elevation 425	Test Pit Number TP-15	Page 1 of 1
		Start Date: Sept 17, 2018	End Date: Sept 19, 2018			

General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:
		TPSL			Sandy Gravel (Stockpile Wash)
2		SM	1		Light Brown Silty SAND with Gravel, Medium Dense, Moist (Till Fill)
			2		- Fine Grained Sand
			3		- Trace Gravel and Cobbles
			4		- Subrounded Gravel and Cobbles
			5		Organics (Roots and Tree Debris) to 6'
			6		
			7		
			8		Contains Gray Sandy SILT fill layers
			9		-Trace Cobbles
			10		- Fine Grained Sand
			11		- Subrounded to Rounded Gravel and Cobbles
			12		- Pocket Pen 0.5
			13		
			14		At 14', Caving
			15		
			16		
			17		
			18		At 18', Increased Density
19					
20		End of Test Pit at 20 feet			
21					
22					

Contractor Vet Industrial	Operator's Name Shannon	Sampling Method Grab	Drawn By: SMC	Date Sept 19, 2018	Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____
Equipment Case CX240B Excavator	Groundwater Elevation No Water Detected		Checked By: WRJ	Date Month XX, 2017	
Notes: GPS Location: 47.65112 degrees N, 122.72798 W			Revision By: SMC	Date Month XX, 2017	



 N.L. Olson & Associates, Inc. Engineering, Planning and Surveying 2453 BETHEL AVENUE P.O. BOX 637 PORT ORCHARD, WASHINGTON 98366-0637				Test Pit Log Dickey Pit 8857 Dickey Road. NW, Silverdale, WA			
Job Number: 10479	Logged By: SMC		Subsurface Exploration		Ground Surface Elevation	Test Pit Number	Page
			Start Date: Sept 17, 2018	End Date: Sept 19, 2018	440	TP-16	1 of 1
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:		
		TPSL			Sandy Gravel (Stockpile Wash)		
3	[Symbol]		1		Light Brown Silty SAND with Gravel, Medium Dense, Moist (Till Fill) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles		
			2				
			3				
			4				
			5				
5	[Symbol]	SM	6		Gray Sandy SILT/CLAY with Gravel, Soft to Medium Stiff, Moist - Trace Cobbles - Fine Grained Sand - Subrounded to Rounded Gravel and Cobbles - Pocket Pen 1.5		
			7				
			8				
			9				
			10				
			11				
			12				
			13				
			14				
			15				
			16				
			17				
			18				
			19				
			20				
			21				
			22				
					End of Test Pit at 20 feet		
Contractor Vet Industrial	Operators Name Shannon		Sampling Method Grab		Drawn By: SMC	Date Sept 19, 2018	Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected		Checked By: WRJ	Date Month XX, 2017		
Notes: GPS Location: 47.65106 degrees N, 122.72689 W				Revision By: SMC	Date Month XX, 2017		

Fig 20

 N.L. Olson & Associates, Inc. Engineering, Planning and Surveying 2453 BETHEL AVENUE P.O. BOX 637 PORT ORCHARD, WASHINGTON 98366-0637	<h2 style="margin:0;">Test Pit Log</h2> Dickey Pit 8857 Dickey Road. NW, Silverdale, WA
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Job Number: 10479	Logged By: SMC	Subsurface Exploration Start Date: Sept 17, 2018 End Date: Sept 19, 2018	Ground Surface Elevation 435	Test Pit Number TP-17	Page 1 of 1
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General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:
		TPSL			Top Soil 2" to 4"
3			1		Light Brown Silty SAND with Gravel, Medium Dense, Moist (Till Fill)
			2		- Fine Grained Sand
			3		- Trace Gravel and Cobbles
			4		- Subrounded Gravel and Cobbles
15			5		
			6		
			7		Gray Sandy SILT' <i>LAYERS LOOSE to medium dense Moist</i>
			8		- Trace Cobbles
			9		- Fine Grained Sand
			10		- Subrounded to Rounded Gravel and Cobbles
		SM	11		- Pocket Pen 1.5
			12		- Organics (Tree Debris) from 16'-20'
			13		
			14		
			15		
			16		From 16'-18, Organics (Tree Debris)
			17		
			18		
			19		
			20		
			21		
			22		End of Test Pit at 21 feet

Contractor Vet Industrial	Operators Name Shannon	Sampling Method Grab	Drawn By: SMC	Date Sept 19, 2018	Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected	Checked By: WRJ	Date Month XX, 2017	
Notes: GPS Location: 47.65017 degrees N, 122.72734 W			Revision By: SMC	Date Month XX, 2017	

Fig 21



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Test Pit Log

Dickey Pit
 8857 Dickey Road. NW,
 Silverdale, WA

Job Number: 10479		Logged By: SMC		Subsurface Exploration		Ground Surface Elevation 435	Test Pit Number TP-18	Page 1 of 1
				Start Date: Sept 17, 2018	End Date: Sept 19, 2018			
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:			
		TPSL			Top Soil 2" to 4"			
4		SM	1		Light Brown Silty SAND with Gravel, Medium Dense, Moist (Till Fill) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles At 10', Increase in Density			
2								
3								
4								
5								
6								
7								
8								
9								
10								
10								
12								
13								
14								
15								
16								
17								
18								
19								
20								
21								
22								
Contractor Vet Industrial	Operators Name Shannon		Sampling Method Grab		Drawn By: SMC	Date Sept 19, 2018		
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected			Checked By: WRJ	Date Month XX, 2017		
Notes: GPS Location: 47.64875 degrees N, 122.72774 W					Revision By: SMC	Date Month XX, 2017		
Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____								

Fig 22



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Test Pit Log



Dickey Pit
 8857 Dickey Road. NW,
 Silverdale, WA





Job Number: 10479	Logged By: SMC	Subsurface Exploration		Ground Surface Elevation 440	Test Pit Number TP-19	Page 1 of 1
		Start Date: Sept 17, 2018	End Date: Sept 19, 2018			


General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:
		TPSL			Top Soil/ Fill 6"
			1		Concrete
			2		End of Test Pit at 1 foot
			3		
			4		
			5		
			6		
			7		
			8		
			9		
			10		
			11		
			12		
			13		
			14		
			15		
			16		
			17		
			18		
			19		
			20		
			21		
			22		

Contractor Vet Industrial	Operators Name Shannon	Sampling Method Grab	Drawn By: SMC	Date Sept 19, 2018	Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____
Equipment Case CX240B Excavator	Groundwater Elevation No Water Detected		Checked By: WRJ	Date Month XX, 2017	
Notes: GPS Location: 47.64706 degrees N, 122.72985 W			Revision By: SMC	Date Month XX, 2017	

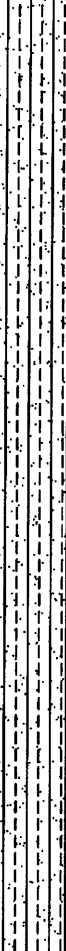
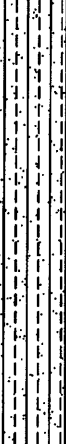
Fig 23

 N.L. Olson & Associates, Inc. Engineering, Planning and Surveying 2453 BETHEL AVENUE P.O. BOX 637 PORT ORCHARD, WASHINGTON 98366-0637				Test Pit Log Dickey Pit 8857 Dickey Road. NW, Silverdale, WA			
Job Number: 10479	Logged By: SMC	Subsurface Exploration		Ground Surface Elevation 450	Test Pit Number TP-20	Page 1 of 1	
		Start Date: Sept 17, 2018	End Date: Sept 19, 2018				
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:		
		TPSL			Top Soil 1" to 3"		
1		SM	1- 2- 3- 4-		Native: Light Brown Silty SAND with Gravel, Very Dense, Moist (Glacial Till) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles		
			5- 6- 7- 8- 9- 10- 11- 12- 13- 14- 15- 16- 17- 18- 19- 20- 21- 22-		End of Test Pit at 4 feet		
Contractor Vet Industrial	Operators Name Shannon	Sampling Method Grab	Drawn By: SMC	Date Sept 19, 2018	Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____		
Equipment Case CX240B Excavator	Groundwater Elevation No Water Detected	Checked By: WRJ	Date Month XX, 2017				
Notes: GPS Location: 47.65238 degrees N, 122.72753 W		Revision By: SMC	Date Month XX, 2017				

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Job Number: 10479	Logged By: SMC	Subsurface Exploration		Ground Surface Elevation 425	Test Pit Number TP-21	Page 1 of 1			
		Start Date: Sept 17, 2018	End Date: Sept 19, 2018						
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:				
		TPSL			Top Soil 1" to 3"				
5		SM	1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20		Light Brown Silty SAND with Gravel, Loose to Medium Dense, Moist (Till Fill) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles - Becomes Medium Dense at 4'				
15		SM	14 15 16 17 18 19 20		Contains Gray Sandy SILT fill layers - Trace Cobbles - Fine Grained Sand - Subrounded to Rounded Gravel and Cobbles - Organics (Tree Debris) from 16'-20'				
20		SM	20 21 22		End of Test Pit at 21 feet				
Contractor Vet Industrial		Operators Name Shannon		Sampling Method Grab		Drawn By: SMC	Date Sept 19, 2018		
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected		Checked By: WRJ		Date Month XX, 2017			
Notes: GPS Location: 47.65088 degrees N, 122.72636 W				Revision By: SMC		Date Month XX, 2017			
						Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____			

 N.L. Olson & Associates, Inc. Engineering, Planning and Surveying 2453 BETHEL AVENUE P.O. BOX 637 PORT ORCHARD, WASHINGTON 98366-0637	<h2 style="margin:0;">Test Pit Log</h2> Dickey Pit 8857 Dickey Road. NW, Silverdale, WA
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Job Number: 10479	Logged By: SMC	Subsurface Exploration Start Date: Sept 17, 2018 End Date: Sept 19, 2018	Ground Surface Elevation 445	Test Pit Number TP-22	Page 1 of 1
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General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:
		TPSL			Top Soil 1" to 3"
5		SM	1 2 3 4 5 6 7 8 9 10		Light Brown Silty SAND with Gravel, Medium Dense, Moist (Till Fill) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles
11			11 12 13 14 15 16 17 18 19 20		Contains Gray Sandy SILT fill layers - Trace Cobbles - Fine Grained Sand - Subrounded to Rounded Gravel and Cobbles - Minor Caving
			20 21 22		End of Test Pit at 20 feet

Contractor Vet Industrial	Operators Name Shannon	Sampling Method Grab	Drawn By: SMC	Date Sept 19, 2018	Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected	Checked By: WRJ	Date Month XX, 2017	
Notes: GPS Location: 47.64764 degrees N, 122.72787 W			Revision By: SMC	Date Month XX, 2017	



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
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Test Pit Log

Dickey Pit
 8857 Dickey Road. NW,
 Silverdale, WA

Job Number: 10479		Logged By: SMC		Subsurface Exploration Start Date: Sept 17, 2018 End Date: Sept 19, 2018		Ground Surface Elevation 450	Test Pit Number TP-24	Page 1 of 1		
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:					
		TPSL			Top Soil 1" to 3"					
3		SM	1		Light Brown Silty SAND with Gravel, Medium Dense, Moist (Till Fill) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles					
			2							
			3							
5					4		Contains Gray Sandy SILT fill layers - Trace Cobbles - Fine Grained Sand - Subrounded to Rounded Gravel and Cobbles - Severe Caving at starting at 5-7' - Organics (Roots) Less Than 5% to ~7'			
			5							
			6							
			7							
			8							
			9							
			10							
			11							
			12							
			13							
			14							
			15							
			16							
17					17		Gray Well Graded SAND with Gravel, Medium Dense, Wet - Trace Silt and Cobbles - Subrounded to Rounded Gravel and Cobbles			
			18							
			19		End of Test Pit at 18 feet					
			20							
			21							
			22							
Contractor Vet Industrial		Operators Name Shannon		Sampling Method Grab		Drawn By: SMC		Date Sept 19, 2018		
Equipment Case CX240B Excavator		Groundwater Elevation Water Seepage at 13'		Checked By: WRJ		Date Month XX, 2017		Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____		
Notes: GPS Location: 47.64764 degrees N, 122.72787 W				Revision By: SMC		Date Month XX, 2017				

F 29 28

 N.L. Olson & Associates, Inc. Engineering, Planning and Surveying 2453 BETHEL AVENUE P.O. BOX 637 PORT ORCHARD, WASHINGTON 98366-0637					<h2 style="margin: 0;">Test Pit Log</h2> Dickey Pit 8857 Dickey Road. NW, Silverdale, WA				
Job Number: 10479	Logged By: SMC	Subsurface Exploration		Ground Surface Elevation XXX'	Test Pit Number TP-25	Page 1 of 1			
		Start Date: Sept 17, 2018	End Date: Sept 19, 2018						
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:				
		TPSL			Top Soil 2" to 4"				
4		SM	1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18		Light Brown Silty SAND with Gravel, Medium Dense, Moist (Till Fill) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles Contains Gray Sandy SILT fill layers - Trace Gravel and Cobbles - Fine Grained Sand - Subrounded to Rounded Gravel and Cobbles - Severe Caving Starting at 7'				
19		SW	19 20 21 22		Gray Well Graded SAND with Gravel, Medium Dense, Wet - Trace Silt and Cobbles - Subrounded to Rounded Gravel and Cobbles End of Test Pit at 20 feet				
Contractor Vet Industrial		Operators Name Shannon		Sampling Method Grab	Drawn By: SMC	Date Sept 19, 2018			
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected 19'		Checked By: WRJ	Date Month XX, 2017	Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____			
Notes: GPS Location: 47.64715 degrees N, 122.72815 W				Revision By: SMC	Date Month XX, 2017				


 N.L. Olson & Associates, Inc. Engineering, Planning and Surveying 2453 BETHEL AVENUE P.O. BOX 637 PORT ORCHARD, WASHINGTON 98366-0637	<h2 style="margin:0;">Test Pit Log</h2> Dickey Pit 8857 Dickey Road. NW, Silverdale, WA
--	---

Job Number: 10479	Logged By: SMC	Subsurface Exploration		Ground Surface Elevation 410	Test Pit Number TP-26	Page 1 of 1
		Start Date: Sept 17, 2018	End Date: Sept 19, 2018			

General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:
		TPSL			Top Soil 1" to 3"
4	[Symbol]	SM	1		Light Brown Silty SAND with Gravel, Medium Dense, Moist (Till Fill) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles Contains Gray Sandy SILT fill layers - Trace Cobbles - Fine Grained Sand - Subrounded to Rounded Gravel and Cobbles - Pocket Pen 1.0 - From 7' to 9', Organics (Tree Debris) Becomes Wet at 17' At 19', Moderate Water Seepage on North side End of Test Pit at 20 feet
			2		
			3		
			4		
			5		
			6		
			7		
			8		
9			9		
			10		
			11		
			12		
			13		
			14		
			15		
			16		
17			17		
			18		
19			19		
			20		
			21		
			22		

Contractor Vet Industrial	Operators Name Shannon	Sampling Method Grab	Drawn By: SMC	Date Sept 19, 2018	Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____
Equipment Case CX240B Excavator		Groundwater Elevation Water Seepage at 19'	Checked By: WRJ	Date Month XX, 2017	
Notes: GPS Location: 47.64780 degrees N, 122.72611 W			Revision By: SMC	Date Month XX, 2017	

Fig 30

 N.L. Olson & Associates, Inc. Engineering, Planning and Surveying 2453 BETHEL AVENUE P.O. BOX 637 PORT ORCHARD, WASHINGTON 98366-0637				<h2 style="margin: 0;">Test Pit Log</h2> Dickey Pit 8857 Dickey Road. NW, Silverdale, WA			
Job Number: 10479	Logged By: SMC	Subsurface Exploration Start Date: Sept 17, 2018 End Date: Sept 19, 2018		Ground Surface Elevation 405	Test Pit Number TP-27	Page 1 of 1	
General Notes SAMPLE COLLECTION DEPTH (FT)	Graphic Symbol	USCS SYMBOL	Depth (ft)	Moisture Content (%)	Surface Conditions:		
		TPSL			Top Soil 1" to 3"		
3	[Symbol]	SM	1 2 3		Light Brown Silty SAND with Gravel, Medium Dense, Moist (Till Fill) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded Gravel and Cobbles		
7	[Symbol]	SM	4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19		Contains Gray Sandy SILT fill layers - Trace Cobbles - Fine Grained Sand - Subrounded to Rounded Gravel and Cobbles - Pocket Pen 1.0 - Organics Less Than 5%		
18	[Symbol]	SM	20 21		Gray Silt SAND with Gravel, Very Dense, Moist (Glacial Till) - Trace Cobbles - Fine Grained Sand - Subrounded to Rounded Gravel and Cobbles		
19	[Symbol]	SM	22		End of Test Pit at 19.5 feet		
Contractor Vet Industrial		Operators Name Shannon		Sampling Method Grab		Drawn By: SMC	
Equipment Case CX240B Excavator		Groundwater Elevation No Water Detected		Date Sept 19, 2018		Checked By: WRJ	
Notes: GPS Location: 47.64783 degrees N, 122.72582 W				Revision By: SMC		Date Month XX, 2017	
Test Pit Completion <input type="checkbox"/> Monitoring Well <input type="checkbox"/> Piezometer <input checked="" type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> _____ <input type="checkbox"/> _____ <input type="checkbox"/> _____							



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Boring Log
 JWW Group Dickey Pit
 8857 Dickey Road, NW,
 Silverdale, WA

Job Number 10479	Logged By SMC	Subsurface Exploration Drilling		Ground Surface Elevation 440'	Boring: B - 1	Page 1 of 2
		Start Date JAN 9, 2019	End Date JAN 9, 2019			

General Notes	Graphic Symbol	USCS SYMBOL	Recovery (in)	Depth (ft)	Blow per (ft) (N-Value)	Surface Conditions:	Moisture Content (%)
B1, 2.5 ft		SM		1		Fill: Brownish Gray Silty SAND with Gravel, Medium Dense, Moist to Damp (Till Fill) - Fine Grained Sand - Trace Gravel and Cobbles - Subrounded to Rounded Gravel and Cobbles	
				2			
				3	20		
				4	13		
				5	10		
B1, 5.0 ft				6	6		
				7	13		
				8	18		
B1, 7.5 ft				9	8		
				10	12		
				11	17		
B1, 10.0 ft				12	12		
	13	14					
	14	20					
	15						
B1, 12.5 ft	ML			13	10	Brownish Gray Sandy SILT/CLAY, Stiff, Moist - Trace Gravel - Fine Grained Sand	
				14	8		
				15	9		
B1, 15.0 ft				16	15		
				17	36		
				18	17		
				19			
B1, 20.0 ft				20			

Monitoring Well installed

Continued on Page 2

Start Time 0930	End Time 1020	Hammer Type 140 lb Manual with cats head	Drawn By: SMC	Date Jan 11, 2019	Hole Completion <input type="checkbox"/> Monitoring Well <input checked="" type="checkbox"/> Piezometer <input type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> Inclinator <i>F 1 9 32</i>
Drilling Contractor Advanced Drill	Operators Name Wade Bellaf	Drilling and sampling Method Standard Split Spoon	Checked By: WRJ	Date Jan 24, 2019	
Equipment d50 tracked drill rig	Groundwater Elevation No Water Encountered		Revision By: SMC	Date Jan 25, 2019	
Job Location					

Remarks: Near Test Pit 11 24-hr Piezometer Check: Dry	Sampling Method Standard Split Spoon I California Sampler II
---	--

Comment 2.47C



N.L. Olson & Associates, Inc.

Job Number
10479

Job Name
Dickey Pit

Logged By
SMC

Boring:
B - 1

Page
2 of 2

Moisture
Content
(%)

General Notes	Graphic Symbol	USCS SYMBOL	Recovery (ft)	Depth (ft)	Blow per (ft) (N-Value)	Description	Moisture Content (%)
B1, 20.0 ft		ML	5'	20 3 1 3 2 3 4	6	Gray Sandy SILT/CLAY, Medium Stiff, Moist - Trace Gravel - Fine Grained Sand	
B1, 25.0 ft		ML	7'	25 2 6 2 1 7 8 9	3	Becomes Soft, Moist	
B1, 30.0 ft		SP-SM	12'	30 30 1 50 2 3 4	50/5"	Native: Gray Poorly Graded SAND with Silt, Stiff, Moist (Advanced Outwash) - Trace Gravel - Medium To Coarse Grained Sand - Gravel 29.6%, Sand 62.7% and Fine 7.7%	4
B1, 35.0 ft		SP-SM	13'	35 50 6	50/6"	End of boring at 36.5 ft bgs	
				7 8 9 1 2 3 4 45 6 7 8 9 50			



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 PORT ORCHARD, WASHINGTON 98366-0637

Boring Log
 JWJ Group Dickey Pit
 8857 Dickey Road, NW,
 Silverdale, WA

Job Number 10479	Logged By SMC	Subsurface Exploration Drilling		Ground Surface Elevation 450'	Boring: B - 2	Page 1 of 2
		Start Date JAN 9, 2019	End Date JAN 9, 2019			

General Notes	Graphic Symbol	USCS SYMBOL	Recovery (in)	Depth (ft)	Blow per (ft) (N-Value)	Surface Conditions:	Moisture Content (%)
B2, 2.5 ft				1		Fill: Light Brown Silty SAND with Gravel, Very Loose, Wet - Fine Grained Sand - Trace Gravel and Cobbles	
				2			
			18"	3	18		
				4	19/		
B2, 5.0 ft			18"	5			
				6	6		
				7			
B2, 7.5 ft			18"	8			
				9	7		
				10			
B2, 10.0 ft			18"	11	9	- Gravel 19.2%, Sand 54.9% and Fine 25.9%	
		SM		12			
B2, 12.5 ft			12"	13	72	Native: Brownish Gray Silty SAND, Dense to Very Dense, Moist - Trace Gravel - Fine to Medium Grained Sand	
				14			
B2, 15.0 ft			18"	15	50/6"	Gray Poorly Graded SAND with Silt, Stiff, Moist (Advanced Outwash) - Trace Gravel - Medium To Coarse Grained Sand - Gravel 35.2%, Sand 58.3% and Fine 6.5%	
				16			
				17			
				18			
				19			
B2, 20.0 ft				20		Continued on Page 2	

Monitoring Well installed

Start Time 1155	End Time 1220	Hammer Type 140 lb Manual with cats head	Drawn By: SMC	Date Jan 11, 2019	Hole Completion <input type="checkbox"/> Monitoring Well <input checked="" type="checkbox"/> Piezometer <input type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> Inclinator
Drilling Contractor Advanced Drill	Operators Name Wade Bellaf	Drilling and sampling Method Standard Split Spoon	Checked By: WRJ	Date Jan 24, 2019	
Equipment d50 tracked drill rig	Groundwater Elevation Wet Soils encountered at 2.5'	Revision By: SMC	Date Jan 25, 2019		
Job Location					

Fig 33

Remarks: Near Test Pit 3 24-hr Piezometer Check: 3.9'	Sampling Method Standard Split Spoon I California Sampler II
---	--

Comment 2.47C



N.L. Olson & Associates, Inc.

General Notes

Graphic Symbol

USCS SYMBOL

Recovery (ft)

Depth (ft)

Blow per (ft) (N-Value)

Job Number
10479

Job Name
Dickey Pit

Logged By
SMC

Boring:
B - 2

Page
2 of 2

Moisture Content (%)

B2, 20.0 ft



SM

50

20

50/5"

Brownish Gray Sity SAND, Very Dense, Moist
- Fine to Medium Grained Sand

End of boring at 20.5 ft bgs

- 1
- 2
- 3
- 4
- 25
- 6
- 7
- 8
- 9
- 30
- 1
- 2
- 3
- 4
- 35
- 6
- 7
- 8
- 9
- 40
- 1
- 2
- 3
- 4
- 45
- 6
- 7
- 8
- 9
- 50



General Notes	Graphic Symbol	USCS SYMBOL	Recovery (ft)	Depth (ft)	Blow per (ft) (N-Value)	Soil Description	Moisture Content (%)		
B3, 20.0 ft			18"	20 13	32	Fill: Brown Gray Silty SAND, dense, Moist - Trace Gravel - Fine Grained Sand - Gravel 19.2%, Sand 54.9% and Fine 25.9%	6		
			1	15					
			17						
			2						
			3						
			4						
B3, 25.0 ft			25	12				24	- Becomes medium dense
			6	15					
			9						
			7						
	8								
	9								
B3, 30.0 ft	30	2	5	- Becomes loose					
	1	2							
	3								
	2								
	3								
	4								
B3, 35.0 ft	35	1			10	- Grades to Brown increase increase in Sand content			
	6	3							
	7								
	8								
	9								
	40	20	67	Native: Brown Silty SAND with Gravel, Very Dense, Moist - Fine to Medium Sand					
B3, 40.0 ft	1	30							
	37								
	2								
	3								
	4								
B3, 45.0 ft	45	19			72	Grades to Gray Brown			
	6	27							
	45								
	7								
	8								
	9								
	50								
	7	End of boring at 46.5 ft bgs							



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Boring Log

JWJ Group Dickey Pit
 8857 Dickey Road. NW,
 Silverdale, WA

Job Number 10479	Logged By SMC	Subsurface Exploration Drilling		Ground Surface Elevation 435'	Boring: B - 4	Page 1 of 2
		Start Date Jan 10, 2019	End Date JAN 10, 2019			

General Notes	Graphic Symbol	USCS SYMBOL	Recovery (in)	Depth (ft)	Blow per (ft) (N-Value)	Surface Conditions:	Moisture Content (%)
B4, 2.5 ft		SM	14"	1 2 3 4	11 11 11 22	Fill: Brown Silty SAND with Gravel, Dense, Moist (Till Fill) - Fine Grained Sand - Trace Gravel	
B4, 5.0 ft			16"	5 6 7	10 8 4 12	Gray Brown Sandy SILT/CLAY, Soft to Medium Stiff, Moist to Damp - Trace to No gravel - Medium to Coarse Grained Sand	
B4, 7.5 ft			18"	8 9	3 4 4 8		
B4, 10.0 ft			18"	10 11 12	2 1 1 2	Becomes Wet, Becomes Very Soft <u>Monitoring Well installed</u>	
B4, 12.5 ft			13"	13 14	1 1 1 2	Decrease in Sand content - Gravel 25.1%, Sand 53.9% and Fine 21%	11
B4, 15.0 ft			18"	15 16	0 0 0 0	- Gravel 22.6%, Sand 55.6% and Fine 21.8%	12
B4, 20.0 ft				17 18 19 20		Continued on Page 2	

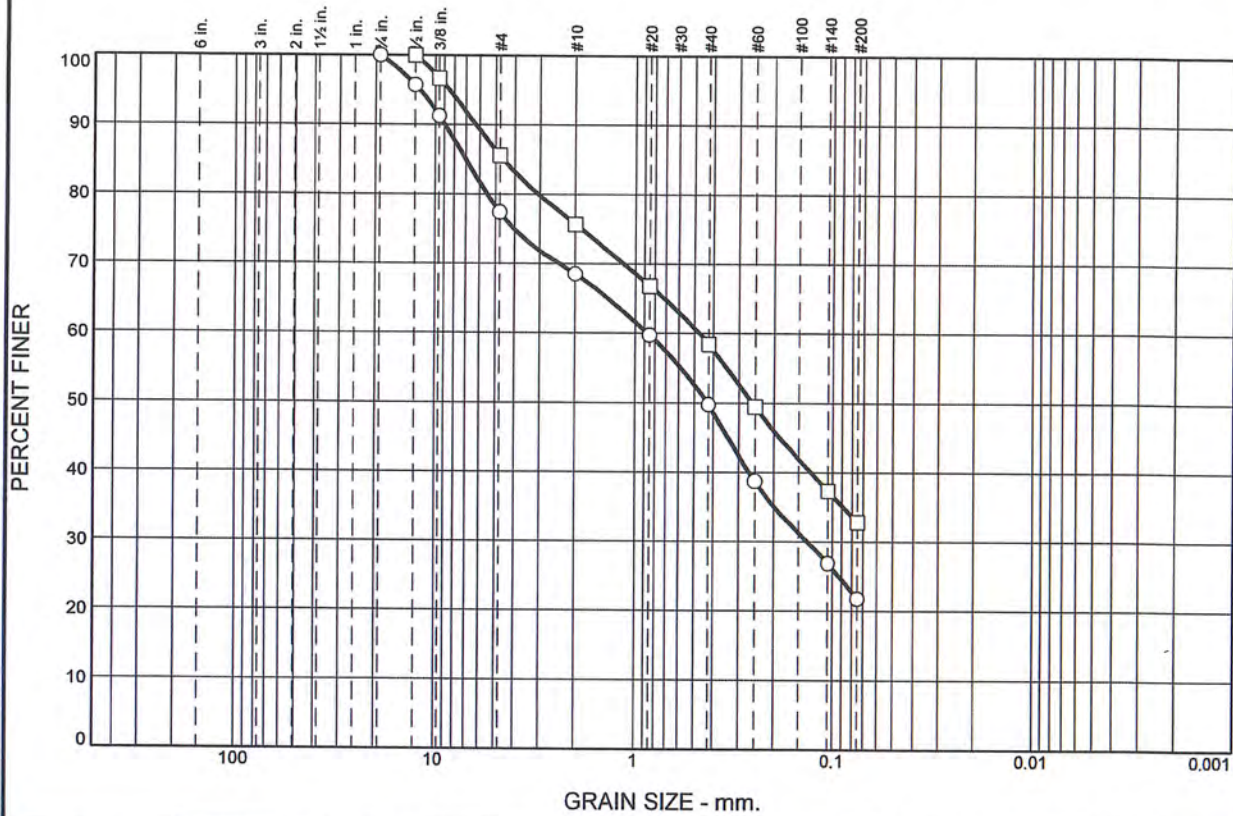
Start Time 1030	End Time 1115	Hammer Type 140 lb Manual with cats head	Drawn By: SMC	Date Jan 11, 2019	Hole Completion <input type="checkbox"/> Monitoring Well <input checked="" type="checkbox"/> Piezometer <input type="checkbox"/> Abandoned and backfilled <input type="checkbox"/> Inclinator
Drilling Contractor Advanced Drill	Operators Name Wade Bellaf	Drilling and sampling Method Standard Split Spoon	Checked By: WRJ	Date Jan 24, 2019	
Equipment d50 tracked drill rig	Groundwater Elevation Water Encountered ~10'		Revision By: SMC	Date Jan 25, 2019	
Job Location 47.64819 degrees North, 122.72842 degrees West					

Remarks: Near Test Pit 7 24-hr Piezometer Check: 7.7'	Sampling Method	
	Standard Split Spoon	I
	California Sampler	II

Fig 35

General Notes	Graphic Symbol	USCS SYMBOL	Recovery (ft)	Depth (ft)	Blow per (ft) (N-Value)	N.L. Olson & Associates, Inc.				Page 2 of 2	Moisture Content (%)
						Job Number 10479	Job Name Dickey Pit	Logged By SMC	Boring: B - 4		
B4, 20.0 ft			18"	20 1 2 3 4	14 15 17 32	Native: Brown Silty SAND, Very Dense, Moist - Trace Gravel - Medium to Coarse Sand					
B4, 25.0 ft			18"	25 6 7 8 9	50 50/5"	Grades to Grayish Brown and becomes Very Dense, - Gravel 14.5%, Sand 52.7% and Fine 32.8%				11	
B4, 30.0 ft			18"	30 1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32 33 34 35 36 37 38 39 40 41 42 43 44 45 46 47 48 49 50	36 50 50/4"	End of boring at 31.5 ft bgs					

Particle Size Distribution Report



	+3"	% GRAVEL	% SAND	% SILT	% CLAY	USCS	AASHTO	PL	LL
○	0.0	22.6	55.6	21.8		SM	A-1-b	NP	NV
□	0.0	14.5	52.7	32.8		SM	A-2-4(0)	NP	NV

SIEVE inches size	PERCENT FINER	
	○	□
0.75	100.0	
0.5	95.7	100.0
0.375	91.2	96.6
GRAIN SIZE		
D ₆₀	0.8717	0.4736
D ₃₀	0.1352	
D ₁₀		
COEFFICIENTS		
C _c		
C _u		

SIEVE number size	PERCENT FINER	
	○	□
#4	77.4	85.5
#10	68.5	75.7
#20	59.7	66.8
#40	49.7	58.4
#60	38.7	49.4
#140	26.9	37.3
#200	21.8	32.8

Material Description

○ silty sand with gravel

□ silty sand

REMARKS:

○

□

○ Depth: 15 Sample Number: B4
 □ Depth: 25 Sample Number: B4

N.L. Olson & Associates
Dicken Pit
10475

Moisture Contents
ASTM D-2216
Table 1

Sample Number	Depth(ft)	Moisture Content %
B-1	10	8
B-1	30	4
B-2	10	10
B-2	15	11
B-3	20	6
B-4	12.5	11
B-4	15	12
B-4	25	11

Fig 3d



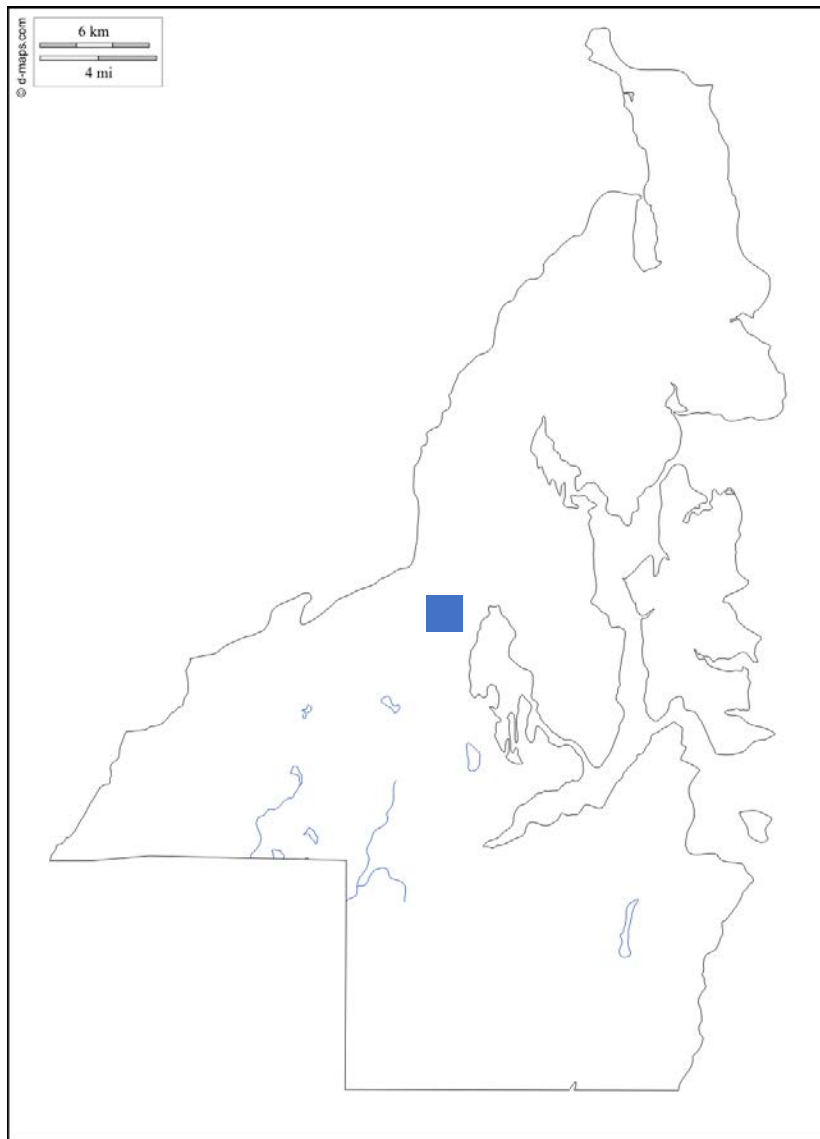
Kitsap County
Annual Comprehensive Plan Amendment Process for 2018
Site-Specific Amendment Application
Maps



Instructions: A vicinity map and site map must be submitted with your site-specific Comprehensive Plan amendment application form. You may complete and submit this document or prepare and submit comparable maps of your own making formatted for 8.5" x 11" paper. You may print, mark-up, and submit a scanned copy of this document. [See example maps.](#)

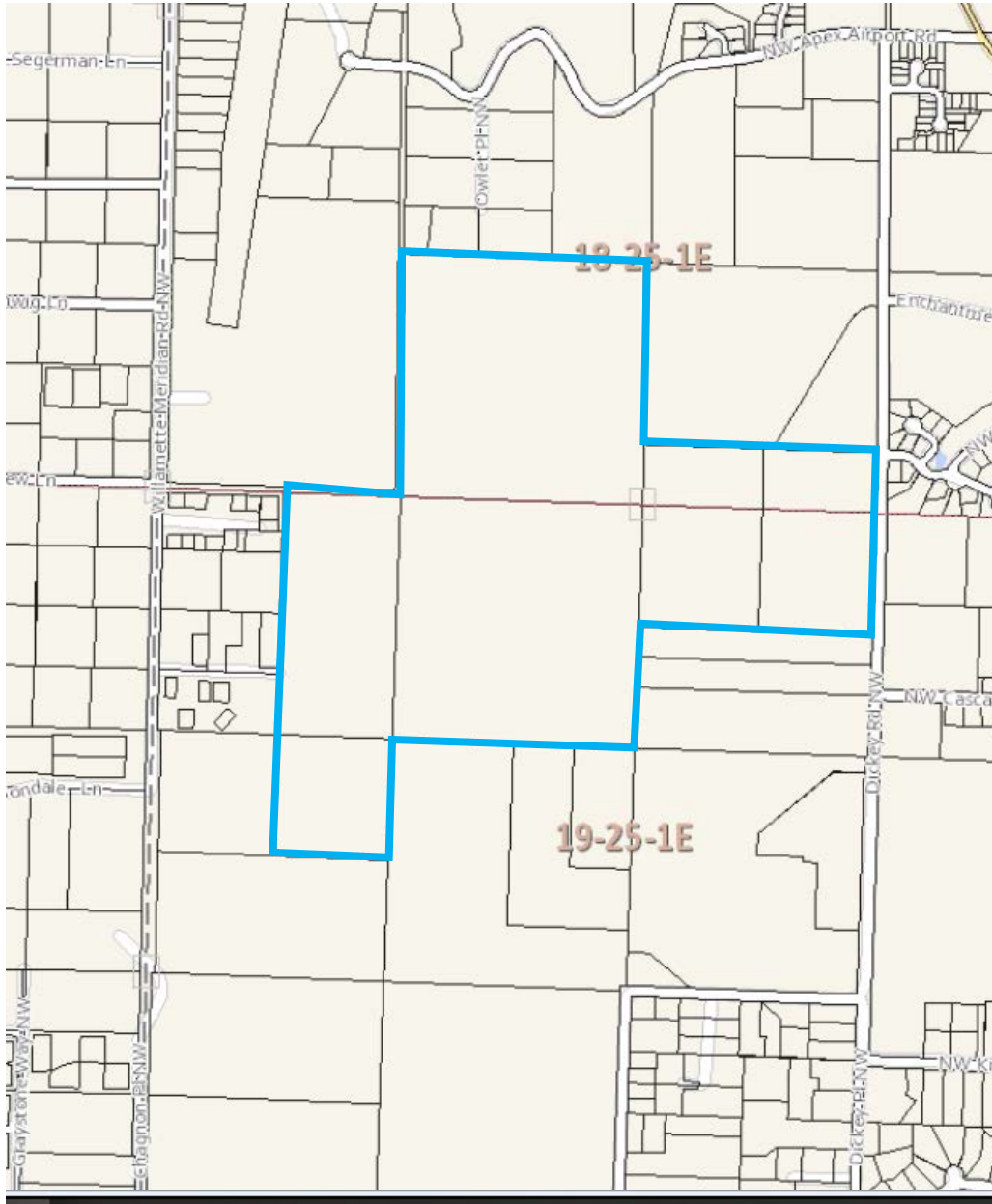
Vicinity Map

Move the blue square (in MS-Word: left-click and drag the blue square) to mark the general location of your site-specific amendment on the vicinity map provided below. You may also use your own method to mark the general location.

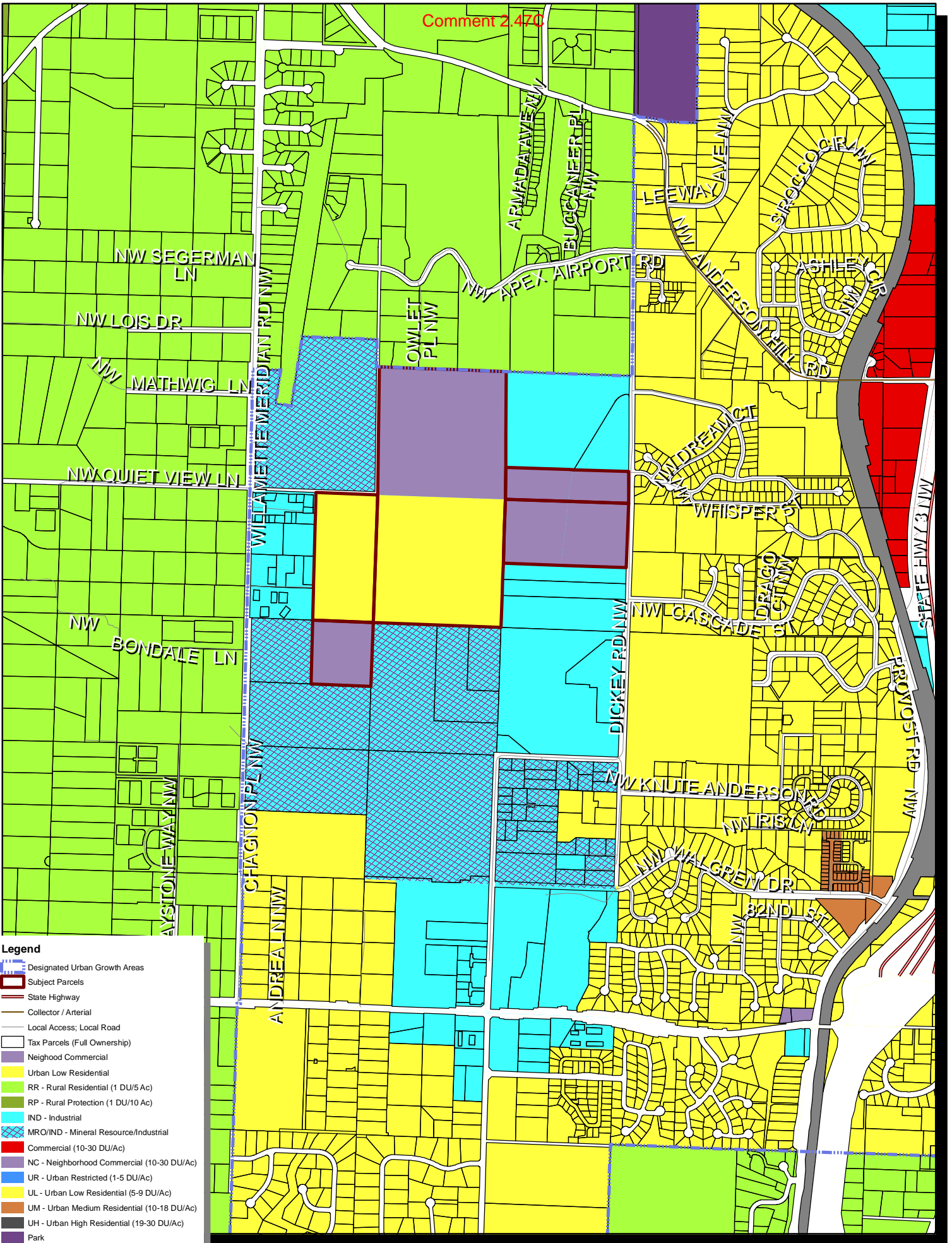


Site Map

Got to the [Kitsap Parcel Search Map](#) and zoom into the subject parcel(s). Insert a digital image (e.g. "screen snip", "screenshot", or "print screen") of the subject parcel(s) and adjacent streets, fit the image to this page, and clearly outline the subject parcel(s). You can modify the shape of the existing blue polygon (in MS-Word: right click the blue box, select "edit points", and edit the shape by right-clicking the points as needed) or you may use your own method to clearly outline the subject parcel(s).



Comment 2.47C

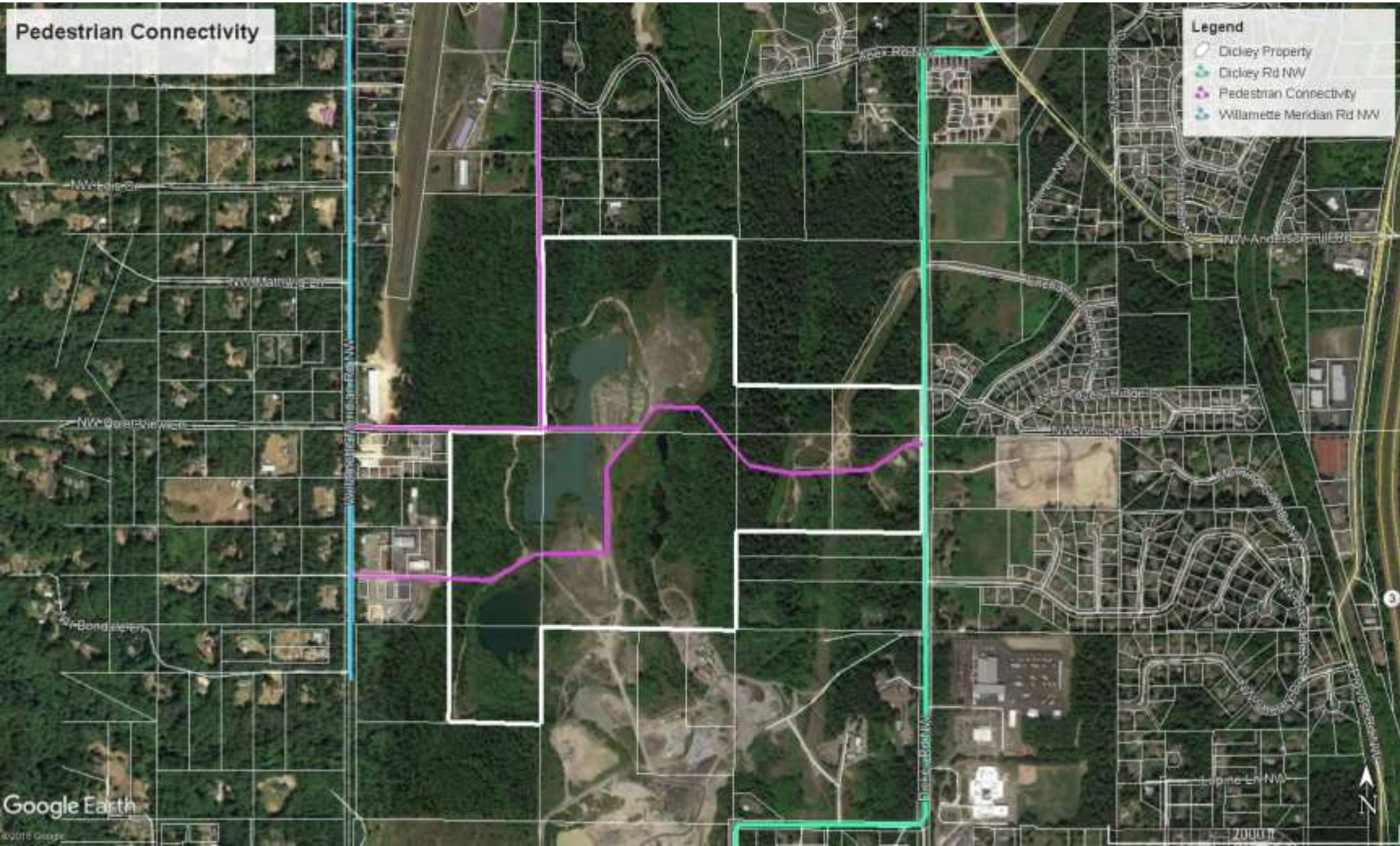


- Legend**
- Designated Urban Growth Areas
 - Subject Parcels
 - State Highway
 - Collector / Arterial
 - Local Access; Local Road
 - Tax Parcels (Full Ownership)
 - Neighborhood Commercial
 - Urban Low Residential
 - RR - Rural Residential (1 DU/5 Ac)
 - RP - Rural Protection (1 DU/10 Ac)
 - IND - Industrial
 - MRO/IND - Mineral Resource/Industrial
 - Commercial (10-30 DU/Ac)
 - NC - Neighborhood Commercial (10-30 DU/Ac)
 - UR - Urban Restricted (1-5 DU/Ac)
 - UL - Urban Low Residential (5-9 DU/Ac)
 - UM - Urban Medium Residential (10-18 DU/Ac)
 - UH - Urban High Residential (19-30 DU/Ac)
 - Park
 - MIL - Military

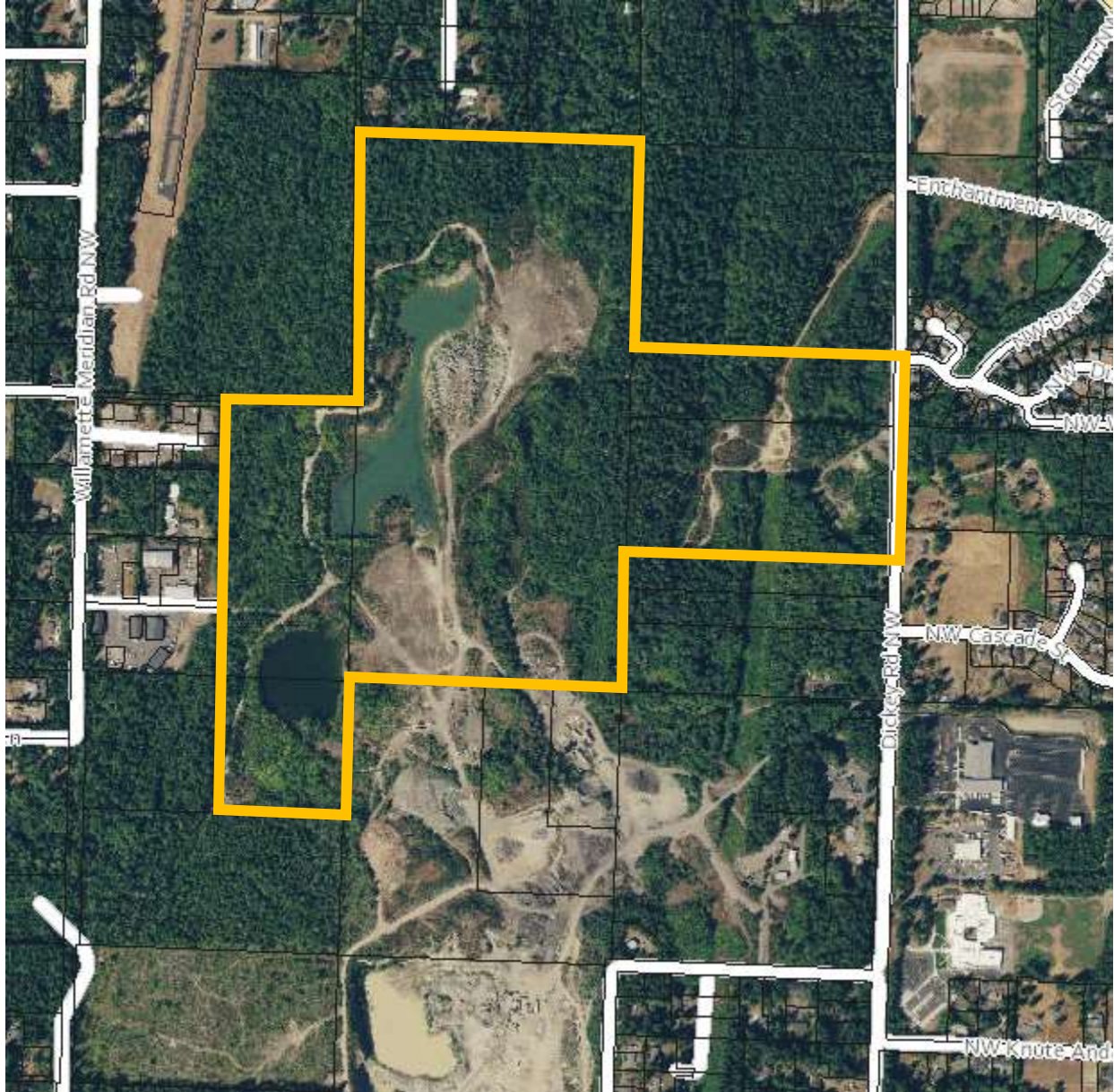
Proposed Zoning

DRAFT CPA 18-00495

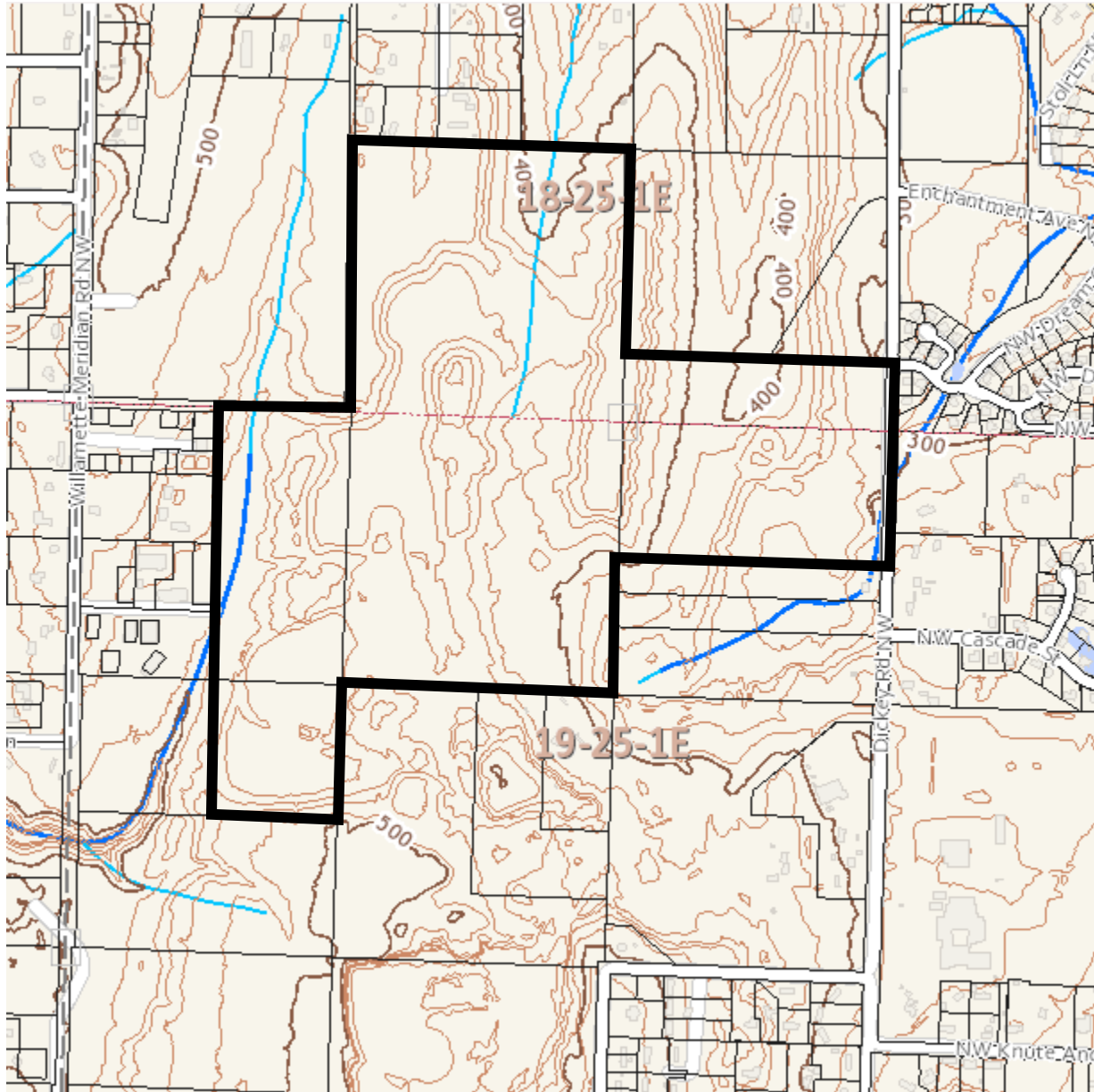




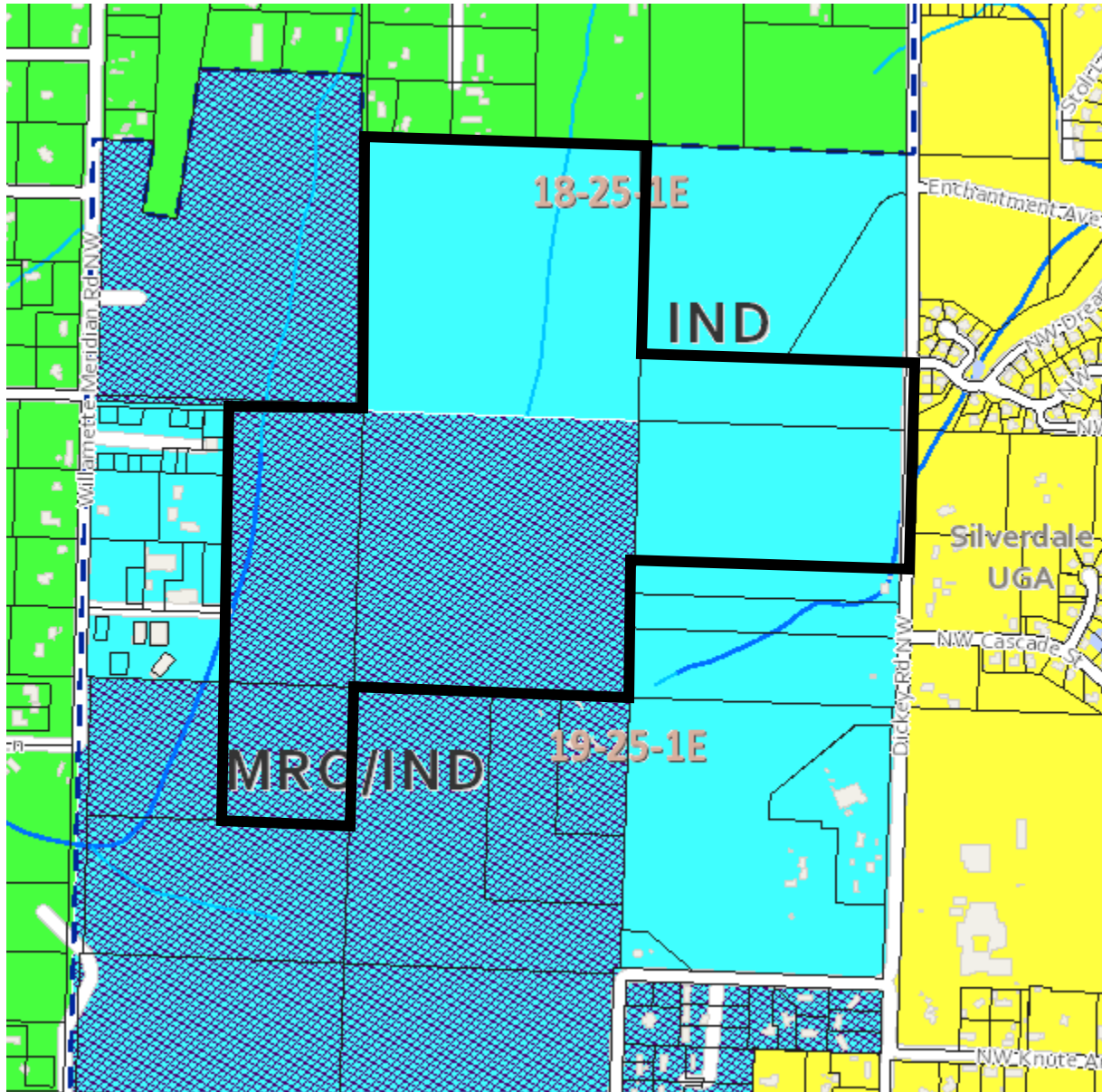
Aerial Photo (2017)



Topography



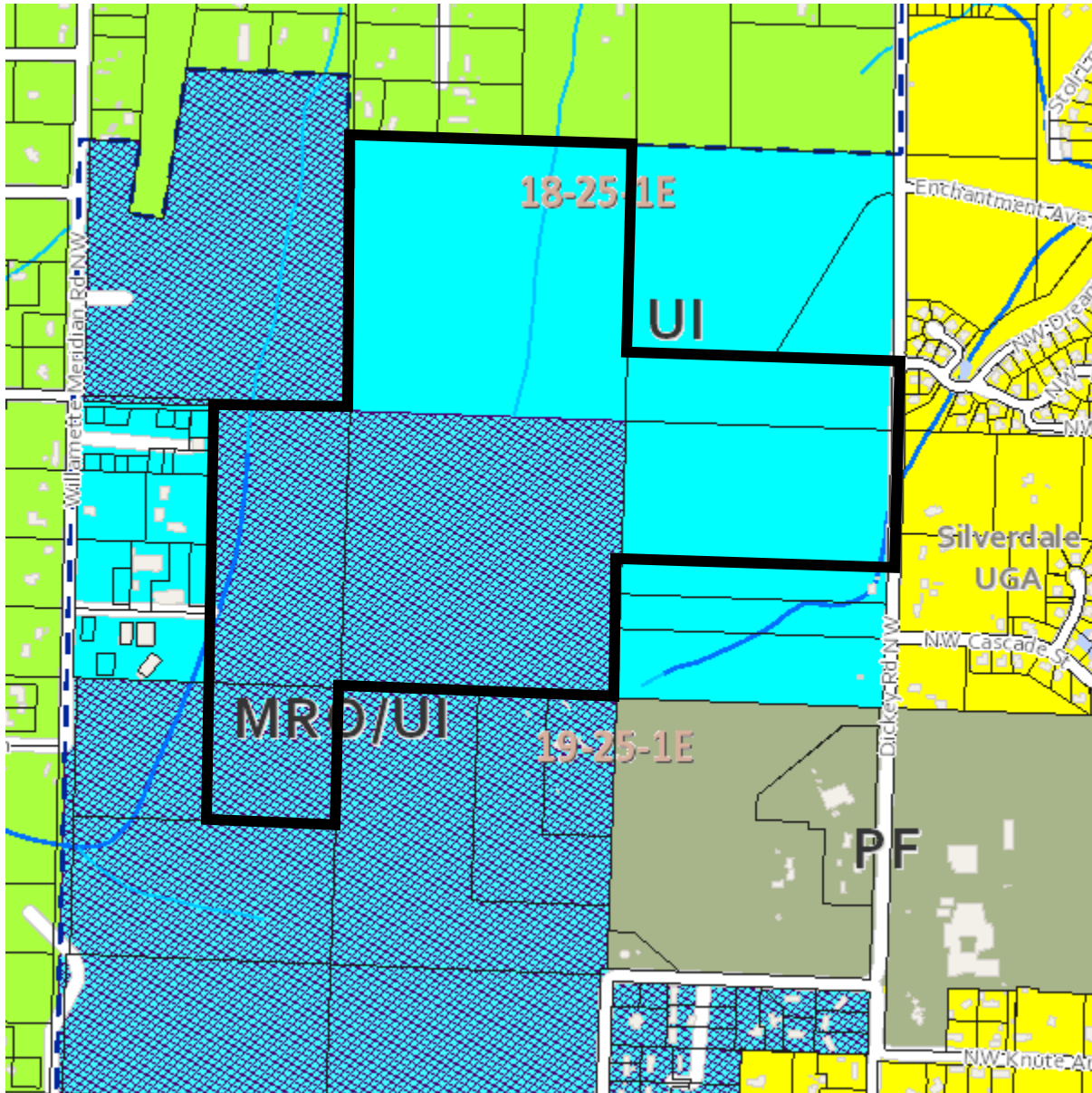
Current Zoning Classifications








Kitsap County Zoning Designations

- RURAL
- RR - Rural Residential (1 DU/5 Ac)
- IND - Industrial
- MRO/IND - Mineral Resource/Industrial
- UL - Urban Low Residential (5-9 DU/Ac)

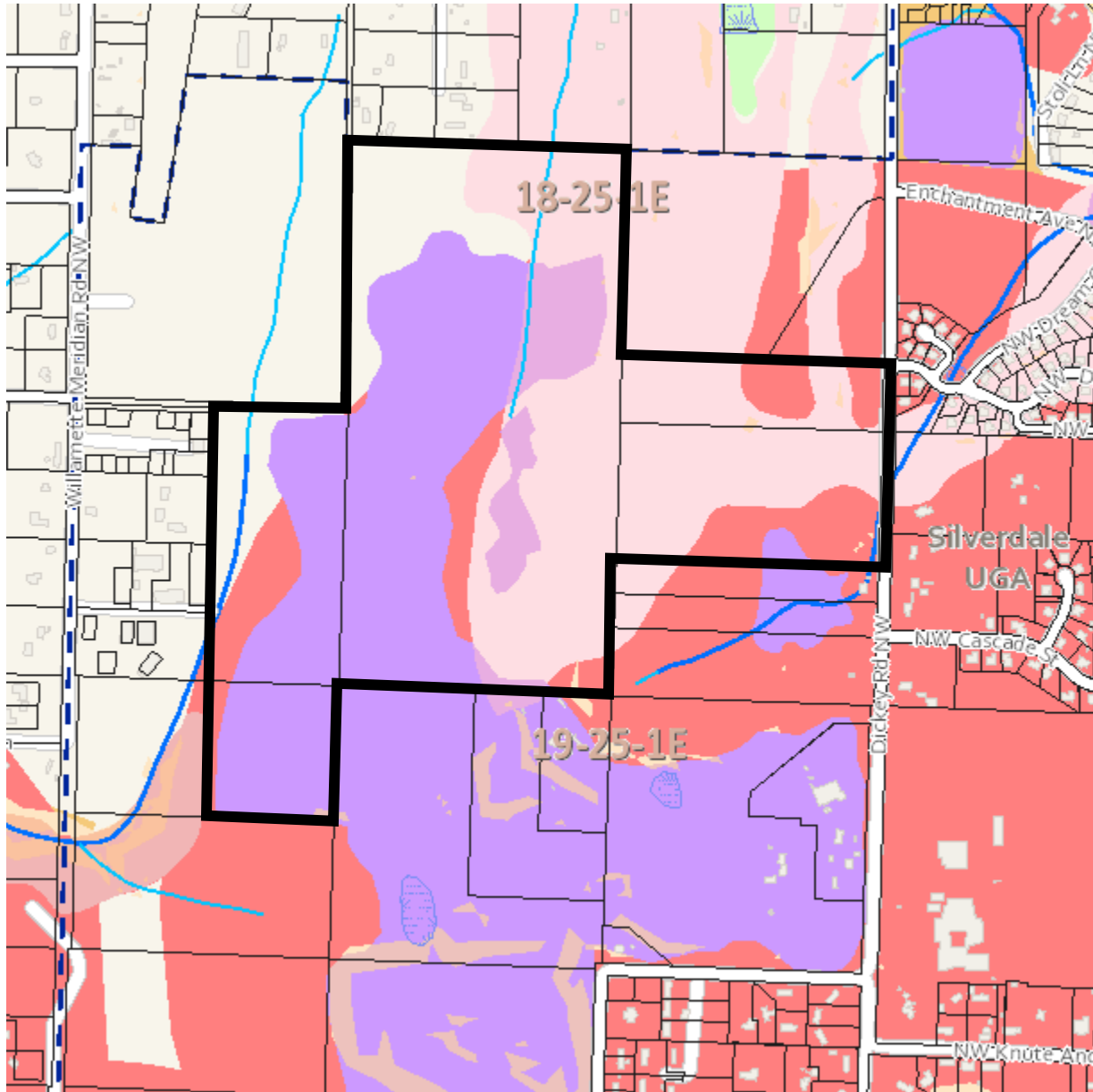
Current Comprehensive Plan Land Use Designation



Comprehensive Plan Designations

- RURAL
-  Rural Residential
- URBAN
-  Urban Industrial
-  Mineral Resource/Urban Industrial
-  Urban Low-Density Residential
-  Public Facility

Critical Areas



Geologic Critical Areas

Seismic High Hazard Area

High

Seismic Moderate Hazard Area

Moderate

Landslide Hazard areas

High

Moderate

Erosion Hazard Area

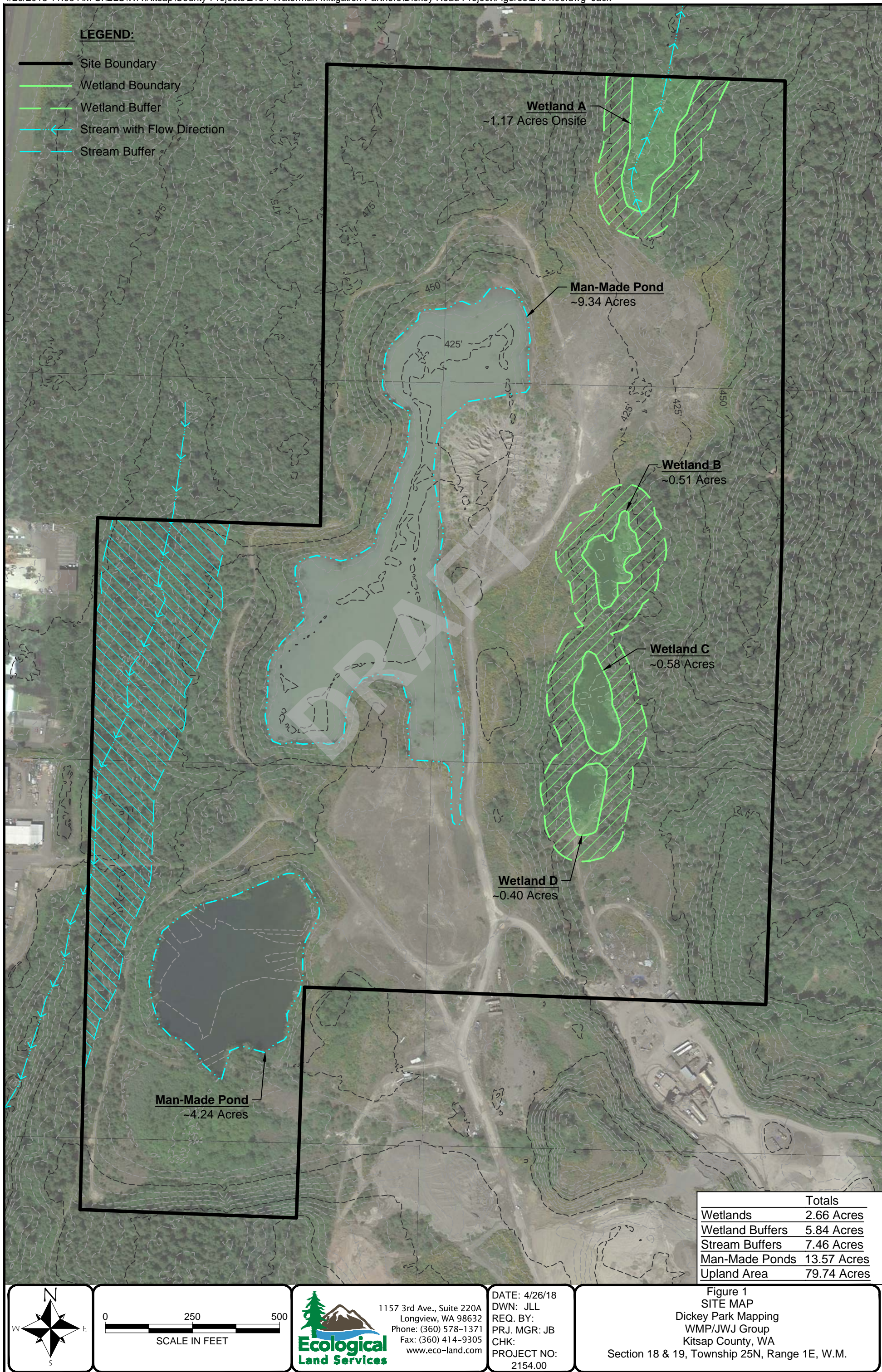
High

Moderate

Critical Aquifers

Category I Critical Aquifer Recharge Areas

Category II Critical Aquifer Recharge Areas



Comment 2.47C
CPA 18-00495 Land Capacity Estimate

Silverdale UGA Capacity-to-Target Estimate				
	2016 Baseline	2016 Adjusted Baseline (with Mining removed)	2019 Alternative 1 (UL & NC)*	2019 Alternative 2 (NC)*
Population	0.3%	0.3%	4.7%	0.3%
Employment	-3.8%	1.0%	1.2%	7.2%

* Using adjusted baseline/classifying all parcels as vacant

Countywide UGA Capacity-to-Target Estimate				
	2016 Baseline	2016 Adjusted Baseline (with Mining removed)	2019 Alternative 1 - Applicant Proposal	2019 Alternative 2 - Applicant Proposal
Population	-5.1%	-5.1%	-4.0%	-5.1%
Employment	1.4%	4.4%	4.6%	8.4%

Silverdale UGA Population Capacity Estimate				
	2016 Baseline	2016 Adjusted Baseline (with Mining removed)	2019 Alternative 1 (UL & NC)	2019 Alternative 2 (NC)
Silverdale UGA Target	8,723	8,723	8,723	8,723
2016 Plan Capacity	8,753	8,753	8,753	8,753
UL zone			377	
NC zone				
Industrial Zone				
Revised Capacity		8,753	9,130	8,753
Variance from Target (#)	30	30	407	30
New Dwelling Units (SFR)			151	
New Dwelling Units (MF)			-	

Countywide Population Capacity Estimate				
	2016 Baseline	2016 Adjusted Baseline (with Mining removed)	2019 Alternative 1 (UL & NC)	2019 Alternative 2 (NC)
Countywide UGA Target	33,551	33,551	33,551	33,551
2016 Plan Capacity	31,837	31,837	31,837	31,837
UL zone			377	
NC zone				
Industrial Zone				
Revised Capacity		31,837	32,214	31,837
Variance from Target (#)	(1,714)	(1,714)	(1,337)	(1,714)
New Dwelling Units (SFR)			151	
New Dwelling Units (MF)			-	

Silverdale UGA Employment Capacity Estimate				
	2016 Baseline	2016 Adjusted Baseline (with Mining removed)	2019 Alternative 1 (UL & NC)	2019 Alternative 2 (NC)
Silverdale UGA Target	8,928	8,928	8,928	8,928
2016 Plan Capacity	8,592	8,592	8,592	8,592
Adj to 2016 Capacity		428	428	428
Revised 2016 Capacity		9,020	9,020	9,020
UL zone				
NC zone			822	1,355
Industrial Zone			(803)	(803)
Revised Capacity		9,020	9,038	9,571
Variance from Target (#)	(336)	92	110	643

Countywide Employment Capacity Estimate				
	2016 Baseline	2016 Adjusted Baseline (with Mining removed)	2019 Alternative 1 (UL & NC)	2019 Alternative 2 (NC)
Countywide UGA Target	14,007	14,007	14,007	14,007
2016 Plan Capacity	14,200	14,200	14,200	14,200
Adj to 2016 Capacity		428	428	428
Revised 2016 Capacity		14,628	14,628	14,628
UL zone				
NC zone			822	1,355
Industrial Zone			(803)	(803)
Revised Capacity		14,628	14,646	15,179
Variance from Target (#)	193	621	639	1,172

CPA 18-00495 - Capacity Estimate in the UL Zone

	2019 Alternative 1 (UL & NC)	
VACANT LAND	URBAN LOW <i>6.0 DU/AC</i>	
Gross Residential Zoned Acres		
Total Gross Vacant Acres	59.53	see below
Subtotal	59.53	
Redevelopable Acres		
Subtotal	59.53	
Vacant Platted Lots (-)	0.00	
Critical Areas		
Total Redevelopable Acres	59.53	
Unencumbered Acres	59.53	see below
Acres within Critical Areas	20.22	see below
Critical Areas reduction 75% (-)	n/a	No reduction due to available site assessment
Acres within Area of Concern	n/a	None per site assessment/applicant
Area of Concern reduction 50% (-)	n/a	No reduction due to available site assessment
Subtotal	39.31	
Roads/Right-of-Way (Future)		
20% (-)	31.45	
Public Facility (Future)		
20% (-)	25.16	
Unavailable Lands		
5% (-)	n/a	No reduction for site-specific (site will be platted)
Commercial Split		
50% (-)	0.00	
Platted Lots		
Vacant Platted Lots	0	
Net Developable Acres	25.16	
	<i>6.0 DU/AC</i>	
Dwelling Unit Capacity	151	
	<i>2.5 pph</i>	
Population Capacity	377	

CPA 18-00495 - Capacity Estimate in the UL Zone

Applicant Provided Information	
Critical Areas	2019 Alternative 1 (UL & NC)
Stream 1	7.70
Stream 2	0.00
Wetland 1	4.90
Wetland 2A	0.00
Wetland 2B	0.00
Wetland 2C	0.00
Man-made Pond - North	5.50
Man-made Pond - South	2.12
Total Critical Areas	20.22

Unencumbered Acres	2019 Alternative 1 (UL & NC)
Redevelopable Acres	59.53
Power Easement	0.00
Total Unencumbered Acres	59.53

Gross Acres	2019 Alternative 1 (UL & NC)
182501-3-031-2003 (North 1/2)	0.00
182501-3-031-2003 (South 1/2)	40.25
192501-2-009-2002	19.28
192501-2-008-2003	0.00
182501-4-026-2008 (East 1/2)	0.00
182501-4-026-2009 (West 1/2)	0.00
192501-1-023-2006 (East 1/2)	0.00
192501-1-023-2006 (West 1/2)	0.00
Gross Acres	59.53

CPA 18-00495 - Capacity Estimate in the NC Zone

	2019 Alternative 1 (UL & NC)	2019 Alternative 2 (NC)
	Neighborhood Commercial	Neighborhood Commercial
Gross Acres		
Total Gross Acres	78.93	138.46
Remove 100% Unavailable Lands	0.00	0.00
Remove 100% Platted Lots	0.00	0.00
Subtotal	78.93	138.46
Critical Areas		
	Applicant Provided Critical Areas	Applicant Provided Critical Areas
Total Redevelopable Acres	78.93	138.46
Unencumbered Acres	73.18	132.71
Acres within Critical Areas	12.56	32.78
Critical Areas reduction 75% (-)	n/a	n/a
Acres within Area of Concern	n/a	n/a
Area of Concern reduction 50% (-)	n/a	n/a
Subtotal	60.62	99.93
Roads/Right-of-Way (Future)		
20% (-)	48.50	79.94
Public Facility (Future)		
20% (-)	38.80	63.96
Unavailable Lands		
20% (-)	31.04	51.16
Vacant Acres	31.04	51.16
Total Net Developable Acres	31.04	51.16
Vacant Square Feet	1351991	2228711
Industrial Land Conversion (32%)	432637	713187
Vacancy Rate Adjustment (5%)	411005	677528
Employment Rate - Square feet per Employee	500	500
Resulting Employment Capacity - Jobs	822	1355

see below

see below

see below

No reduction due to av

None per site assessme

No reduction due to av

Applicant Provided Information		
Critical Areas	2019 Alternative 1 (UL & NC)	2019 Alternative 2 (NC)
Wetland 2A	0.65	0.65
Wetland 2B	2.55	2.55
Wetland 2C	0.00	0.00
Stream 1	0.00	7.70
Stream 2	2.85	2.85
Wetland 1	0.55	5.45
Man-made Pond - North	3.84	9.34
Man-made Pond - South	2.12	4.24

2.14 Acres not counted

CPA 18-00495 - Capacity Estimate in the NC Zone

Total Critical Areas	12.56	32.78
-----------------------------	--------------	--------------

Unencumbered Acres	2019 Alternative 1 (UL & NC)	2019 Alternative 2 (NC)
Redevelopable Acres	78.93	138.46
Power Easement	-5.75	-5.75
Total Unencumbered Acres	73.18	132.71

Gross Acres	2019 Alternative 1 (UL & NC)	2019 Alternative 2 (NC)
182501-3- 031 -2003 (North 1/2)	40.25	40.25
182501-3- 031 -2003 (South 1/2)	0.00	40.25
192501-2- 009 -2002	0.00	19.28
192501-2- 008 -2003	9.50	9.50
182501-4- 026 -2008 (East 1/2)	4.85	4.85
182501-4- 026 -2009 (West 1/2)	4.85	4.85
192501-1- 023 -2006 (East 1/2)	9.74	9.74
192501-1- 023 -2006 (West 1/2)	9.74	9.74
Gross Acres	78.93	138.46

CPA 18-00495 - Capacity Estimate in the NC Zone

Available site assessment
None per site assessment/applicant
Available site assessment

2.14 Acres not counted due to Power Easement overlap

CPA 18-00495 - Capacity Estimate in the NC Zone

CPA 18-00495 - Capacity Estimate in the IND Zone

	2016 Baseline (Vacant)	2016 Baseline (Underutilized)	2016 Adjusted Baseline (with Mining removed)
	Industrial	Industrial	Industrial
Gross Acres			
Total Gross Acres	59.69	78.77	138.46
Developed Acres	0.00	69.12	0.00
Remove 100% Platted Lots	0.00	0.00	0.00
Subtotal	59.69	9.65	138.46
Critical Areas			
Total Redevelopable Acres	59.69	9.65	138.46
Unencumbered Acres	22.08	2.20	60.88
Acres within Critical Areas	3.07	1.90	11.84
Critical Areas reduction 75% (-)	0.77	0.48	2.96
Acres within Area of Concern	34.66	5.41	65.72
Area of Concern reduction 50% (-)	17.33	2.71	32.86
Subtotal	40.18	5.38	96.70
Roads/Right-of-Way (Future)			
20% (-)	32.14	4.30	77.36
Public Facility (Future)			
20% (-)	25.71	3.44	61.89
Unavailable Lands			
20% vacant, 25% underutilized (-)	20.57	2.58	49.51
Vacant Acres	20.57	2.58	49.51
Total Net Developable Acres	20.57	2.58	49.51
Vacant Square Feet	896068	112489	2156673
Industrial Land Conversion (38%)	340506	42746	819536
Vacancy Rate Adjustment (5%)	323480	40609	778559
Employment Rate - Square feet per Employee	969	969	969
Resulting Employment Capacity - Jobs	334	42	803
Capacity from Vacant Lands			
	334		803
Capacity from Underutilized lands			
		42	
TOTAL Employment Capacity			
	376		803
2016 Baseline Adjustment			
	428		

Applicant Provided Information			
Critical Areas	2016 Baseline	2016 Baseline	2016 Adjusted Baseline (with Mining removed)
Stream 1	0.00	0.00	0.00
Stream 2	0.00	0.00	0.00
Wetland 1	0.00	0.00	0.00
Wetland 2A	0.00	0.00	0.00
Wetland 2B	0.00	0.00	0.00
Wetland 2C	0.00	0.00	0.00
Man-made Pond - North	0.00	0.00	0.00
Man-made Pond - South	0.00	0.00	0.00
Total Critical Areas	0.00	0.00	0.00

Unencumbered Acres	2016 Baseline	2016 Baseline	2016 Adjusted Baseline (with Mining removed)
Redevelopable Acres	59.69	9.65	138.46
Power Easement	0.00	0.00	0.00
Total Unencumbered Acres	59.69	9.65	138.46

Gross Acres	2016 Baseline	2016 Baseline	2016 Adjusted Baseline (with Mining removed)
182501-3-031-2003 (North 1/2) (3-012)	40.25	40.25	40.25
182501-3-031-2003 (South 1/2) (3-012)	0.00	0.00	40.25
192501-2-009-2002	0.00	19.28	19.28
192501-2-008-2003	0.00	9.50	9.50
182501-4-026-2008 (4-027)	9.70	0.00	9.70
192501-1-023-2006 (1-004, 1-003)	9.74	9.74	19.48
Gross Acres	59.69	78.77	138.46

Summary of REVISED Site-Specific Comprehensive Plan Amendment Application #18-00495

Note: This application previously included three alternatives. It has been revised to one new proposal.

Description

This is a site-specific Comprehensive Plan amendment proposing to rezone *138.5 acres* just inside the boundary of the Silverdale Urban Growth Area (UGA) from an urban Industrial (IND) zone and removing the Mineral Resource Overlay (MRO) designation. Due to encumbrances on the parcels such as (easements, critical areas, etc), the net developable acreage for this project will be *56.25 acres*.

The request is to rezone the site to a mix of Urban Low Density Residential (UL, 5-9 DU/Ac) and Neighborhood Commercial (NC, 10-30 DU/Ac). The site is nearby Silverdale Elementary School. High-voltage power lines run through the site. While this is not a development permit, the application proposes future development that would include:

- A new residential neighborhood and commercial uses (see revised proposal map).
- A new road connecting Dickey Rd NW and Willamette-Meridian Rd NW (see map).
- Non-motorized connections to three surrounding roads (see map).

Current Use

Undeveloped land and a former sand and gravel mine that has not been reclaimed.

Location

The site is generally located between Dickey Rd NW and Willamette-Meridian Rd NW. See vicinity maps.

182501-3-031-2003	Port Orchard Sand & Gravel Company INC
192501-2-009-2002	Port Orchard Sand & Gravel Company INC
192501-2-008-2003	Port Orchard Sand & Gravel Company INC
192501-1-023-2006	Dickey Wood LLC C/O Financial Peace Company LLC
182501-4-026-2008	Dickey Wood LLC C/O Financial Peace Company LLC

Change in Growth Assumptions

The County has historically provided public service providers with population and employment growth assumptions broken down geographically by Transportation Analysis Zone (TAZ) to aid in modeling and development of long-range capital facilities plans, including the County’s 20-year Capital Facilities Plan (CFP). This site is in TAZ #276. A summary of the previous and new growth assumptions (for the period 2012-2036) are provided below. This application is estimated to increase both the population and employment growth assumptions within the TAZ as highlighted below in yellow.

TAZ 276	Population	Employment
2012 Baseline	165	49
Previous 2016 Growth Assumption (2012- 2036)	-3	1,119
New 2019 Growth Assumption (2012- 2036)	374	1,565
*Net Change from Previous 2016 Growth Assumption	377	446
*Net Change in Dwelling Units (Single Family)	151	-

* Net change refers to the estimated population (people), dwelling units (SFRs), and employment (jobs) that will be added by this amendment to the growth assumptions used in 2016 capital facilities planning.

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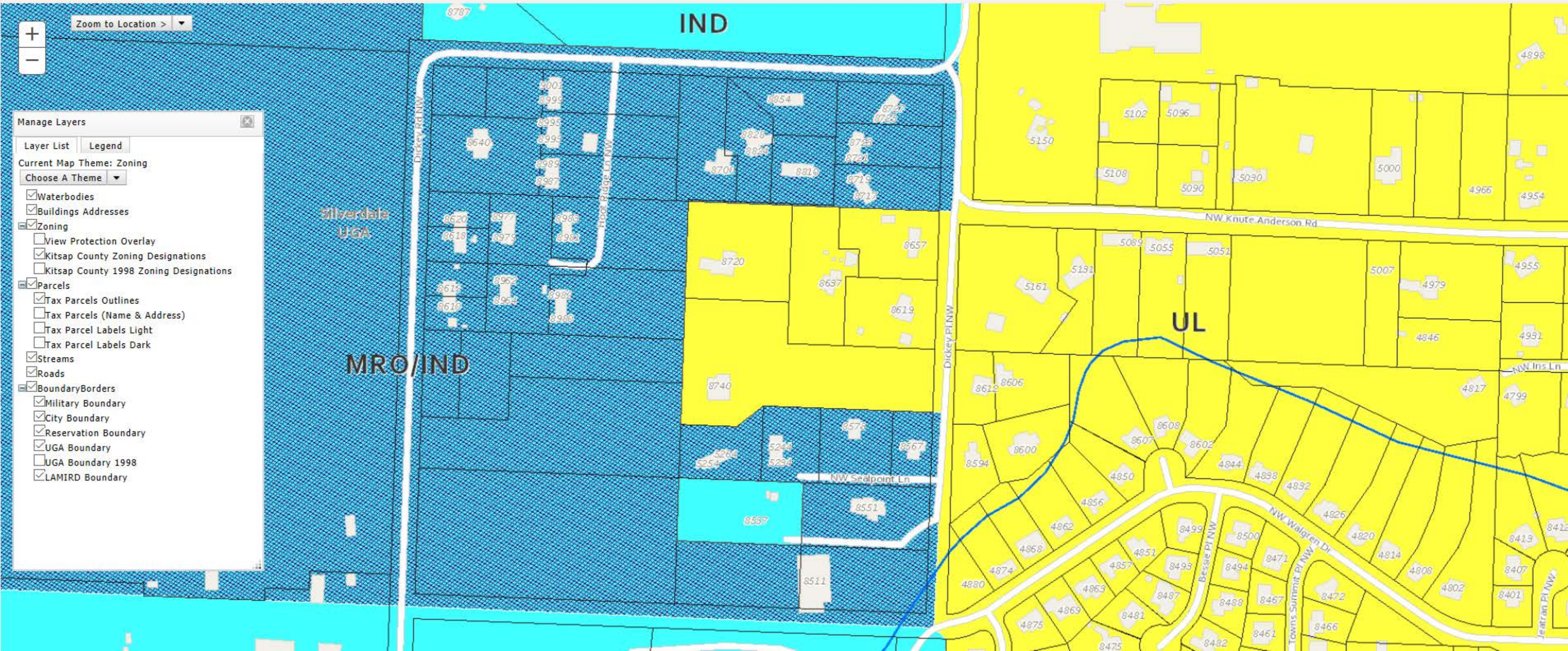
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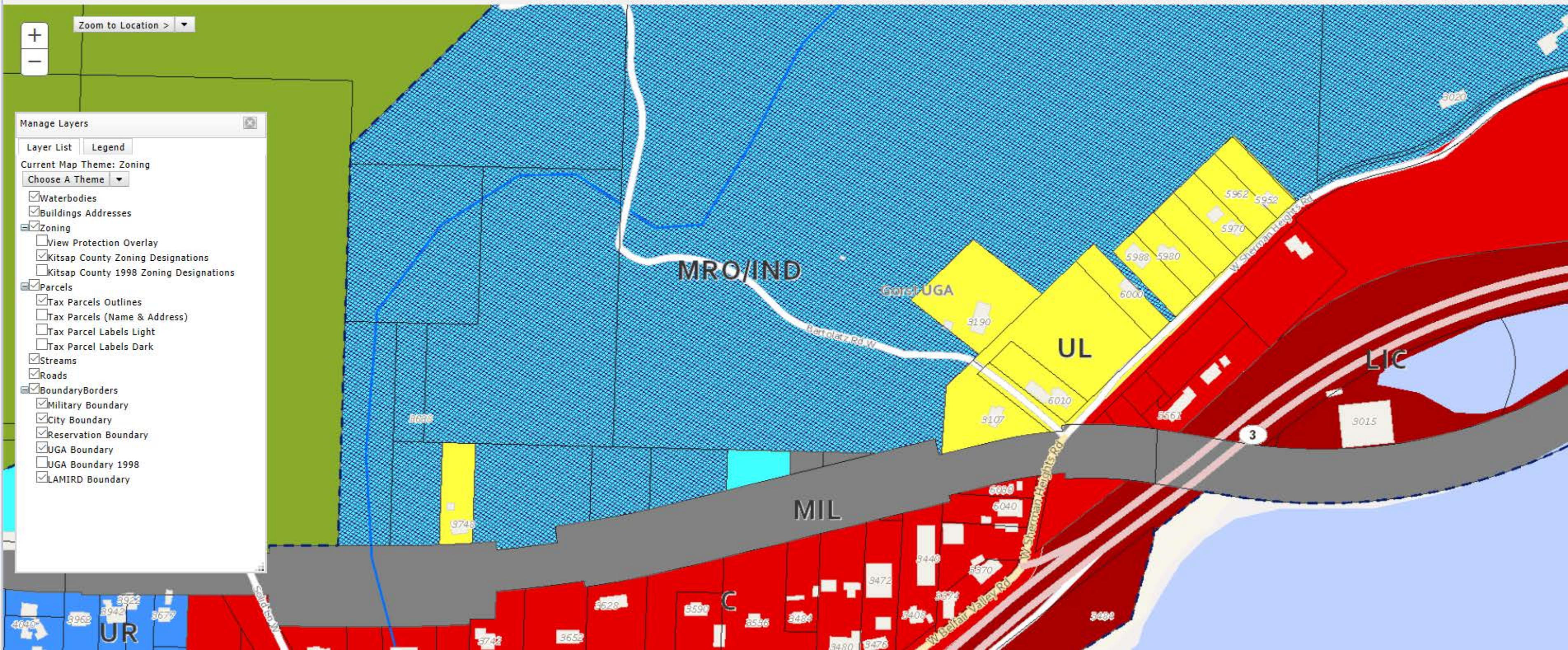
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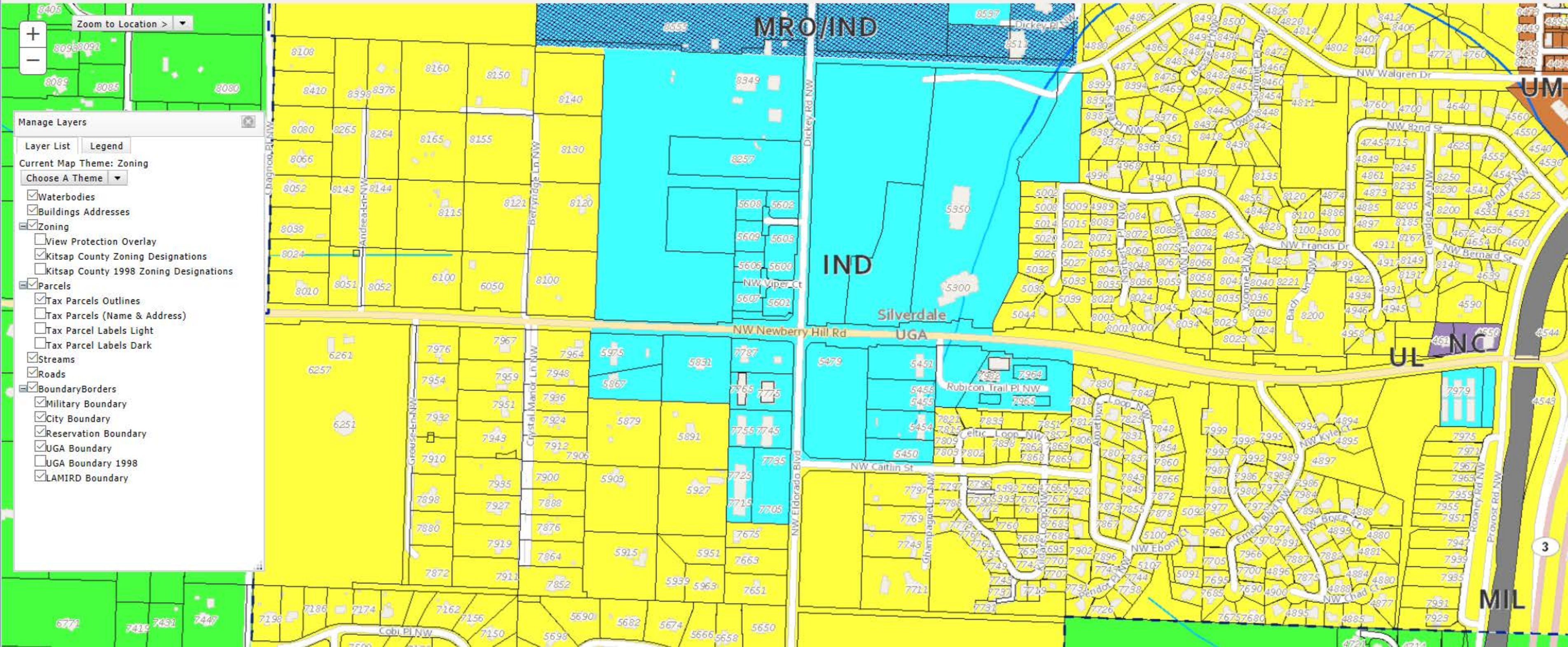
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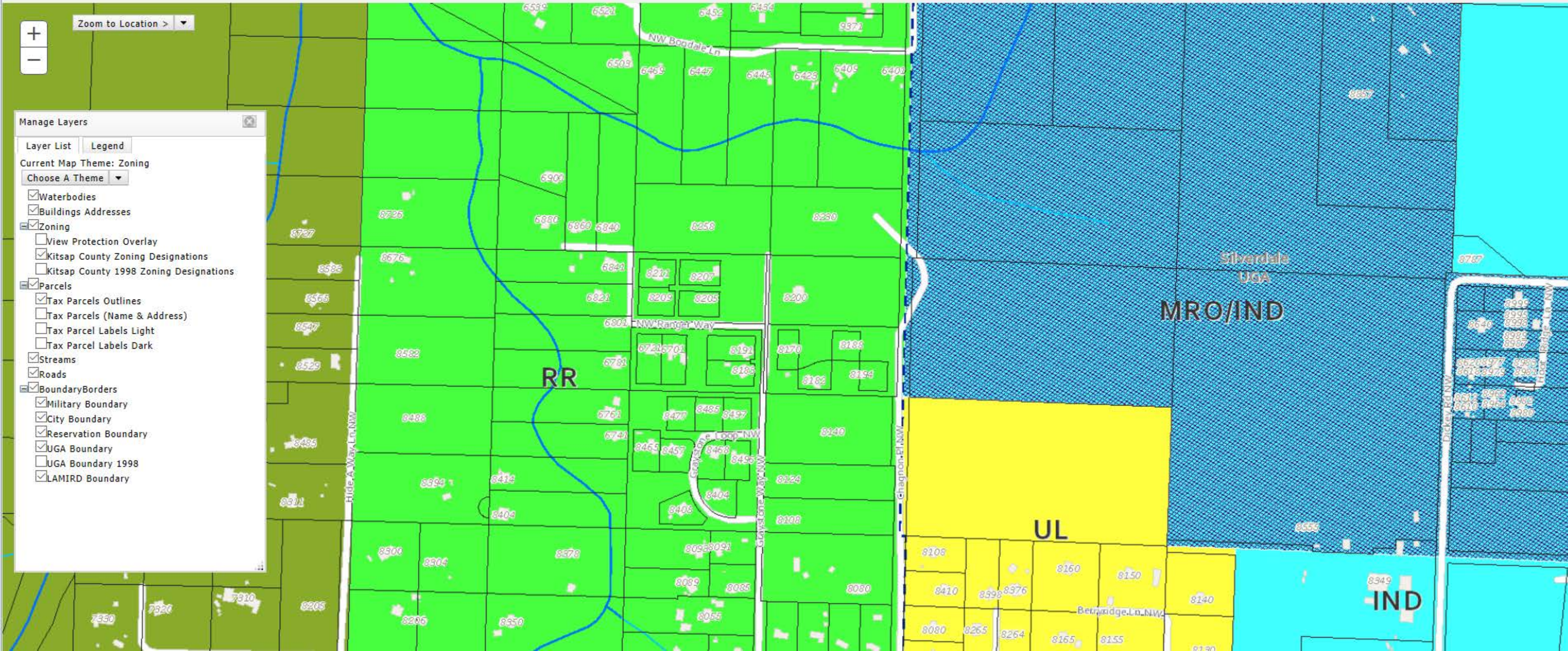
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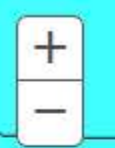
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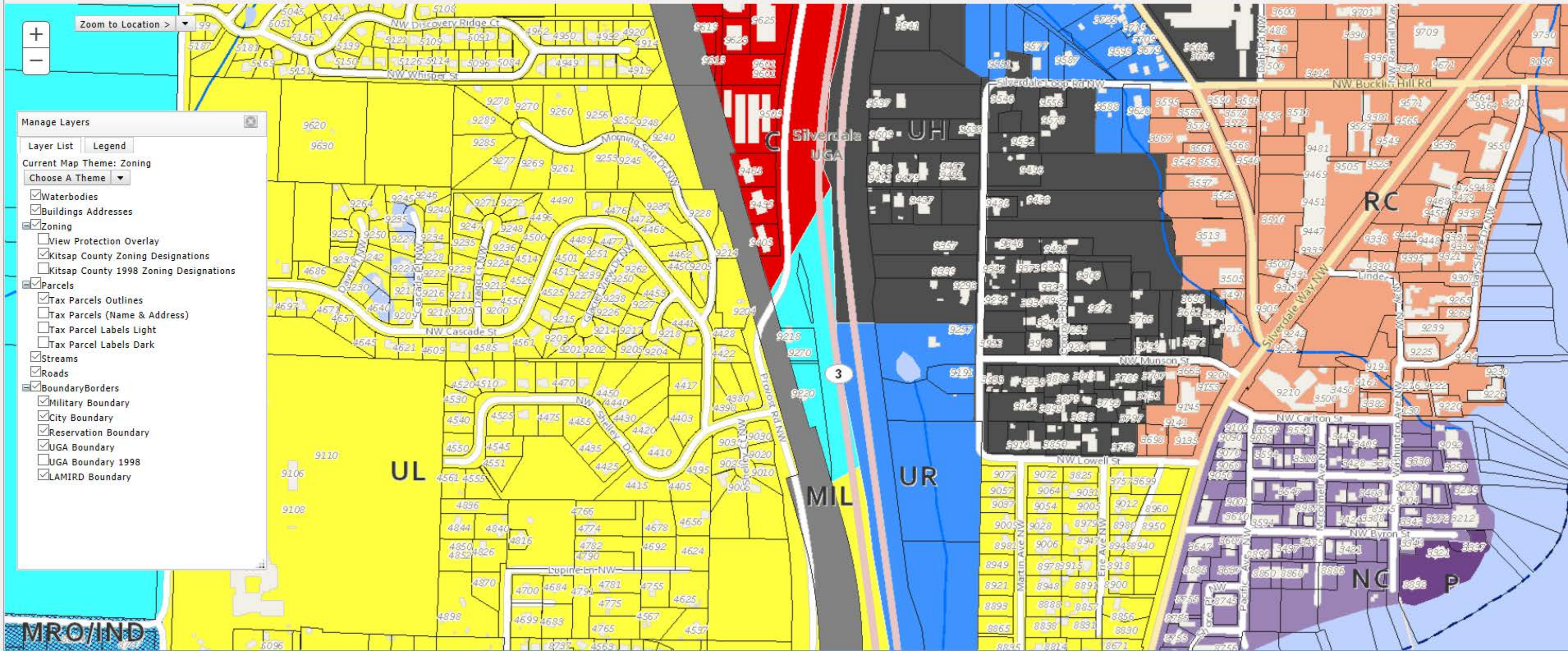
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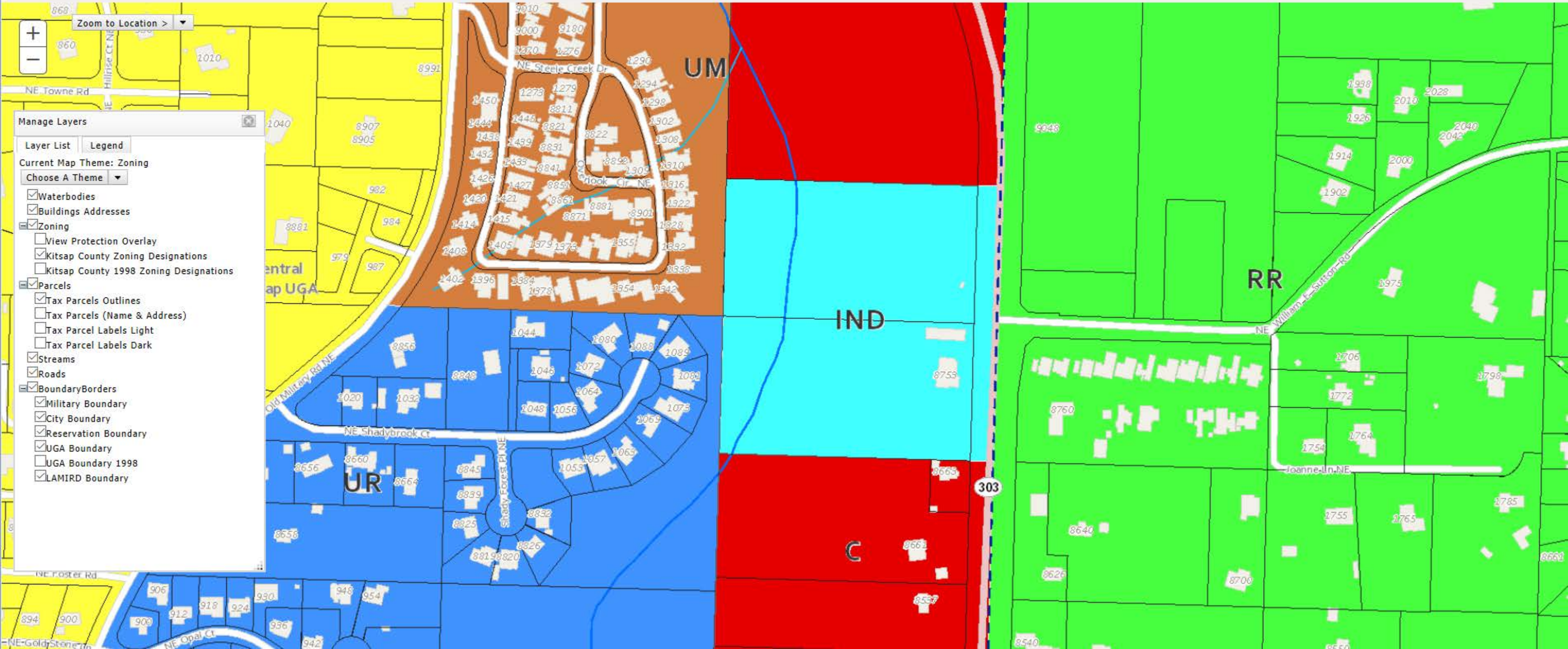
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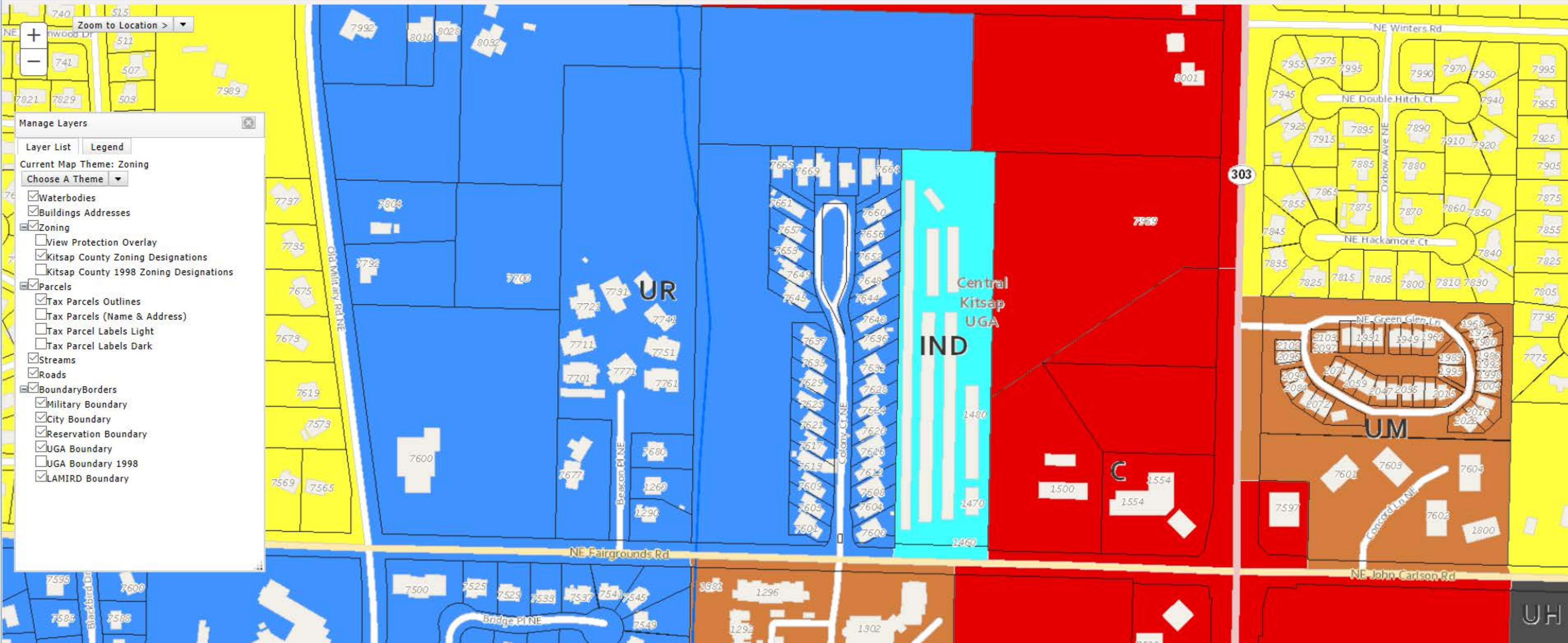
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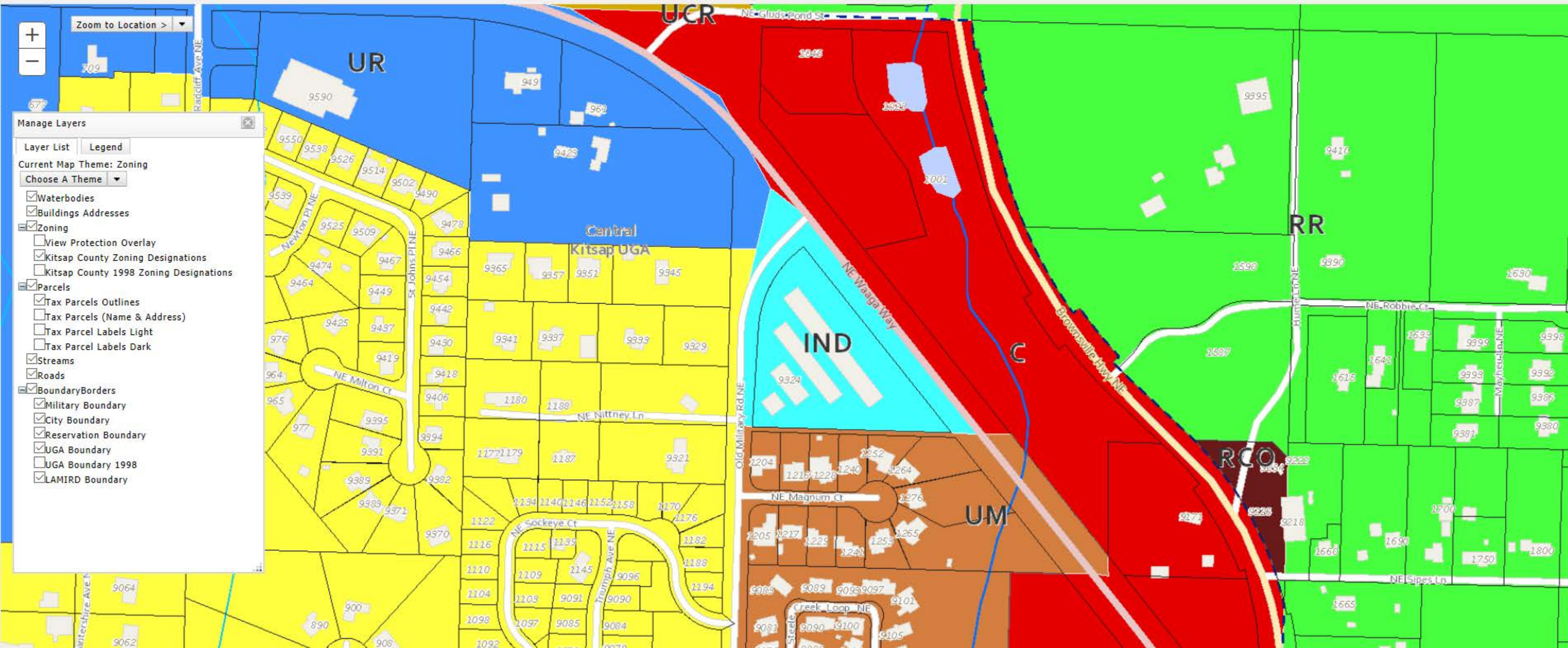


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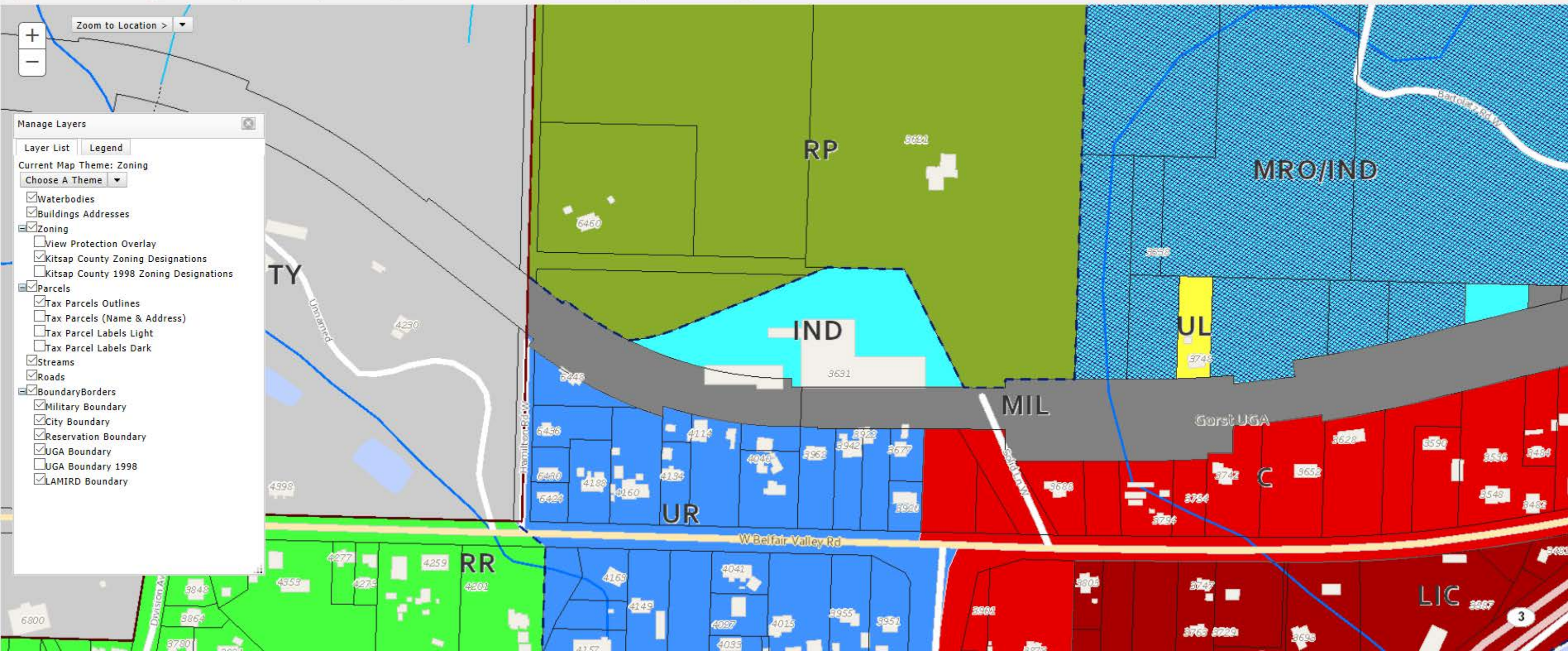
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







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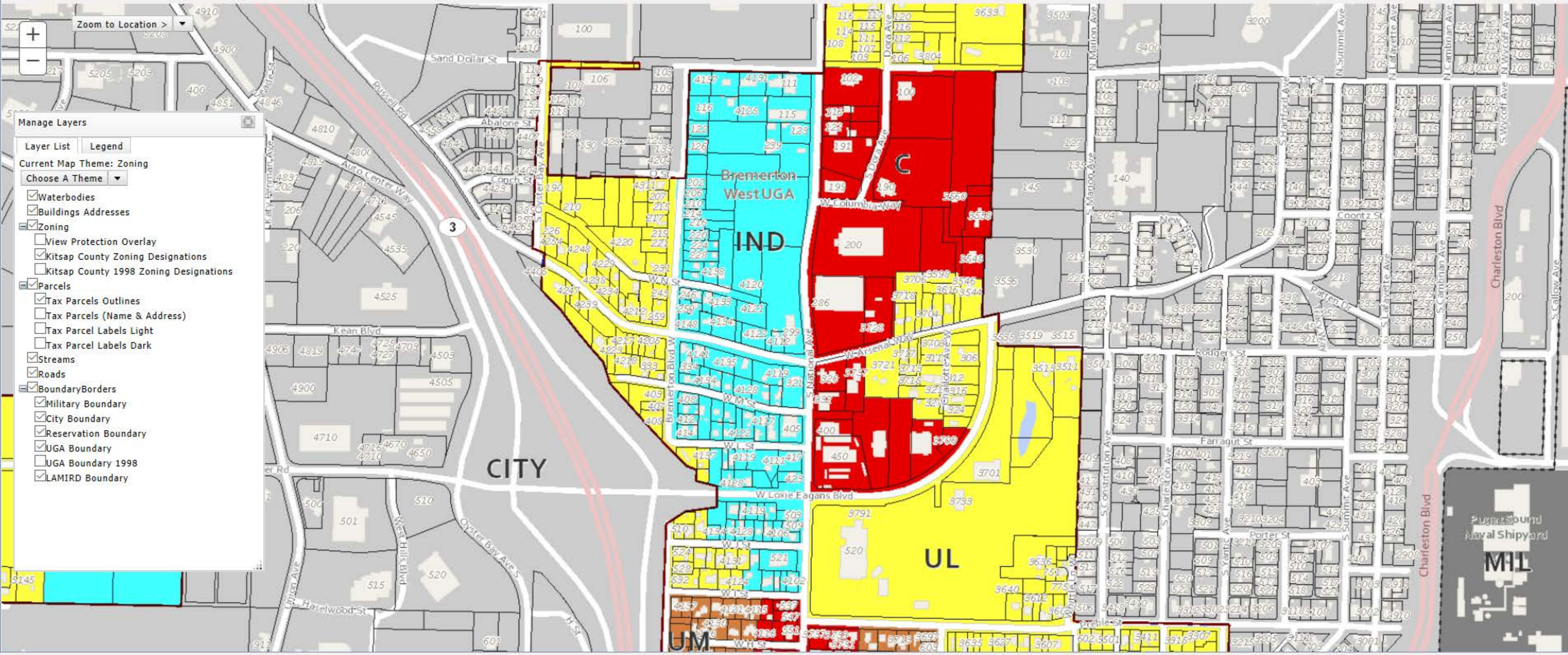
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 - City Boundary
 - Reservation Boundary
 - UGA Boundary
 - UGA Boundary 1998
 - LAMIRD Boundary



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Parcel Search

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Show results in a table, not the map.

Map Scale: 1 : 10,000

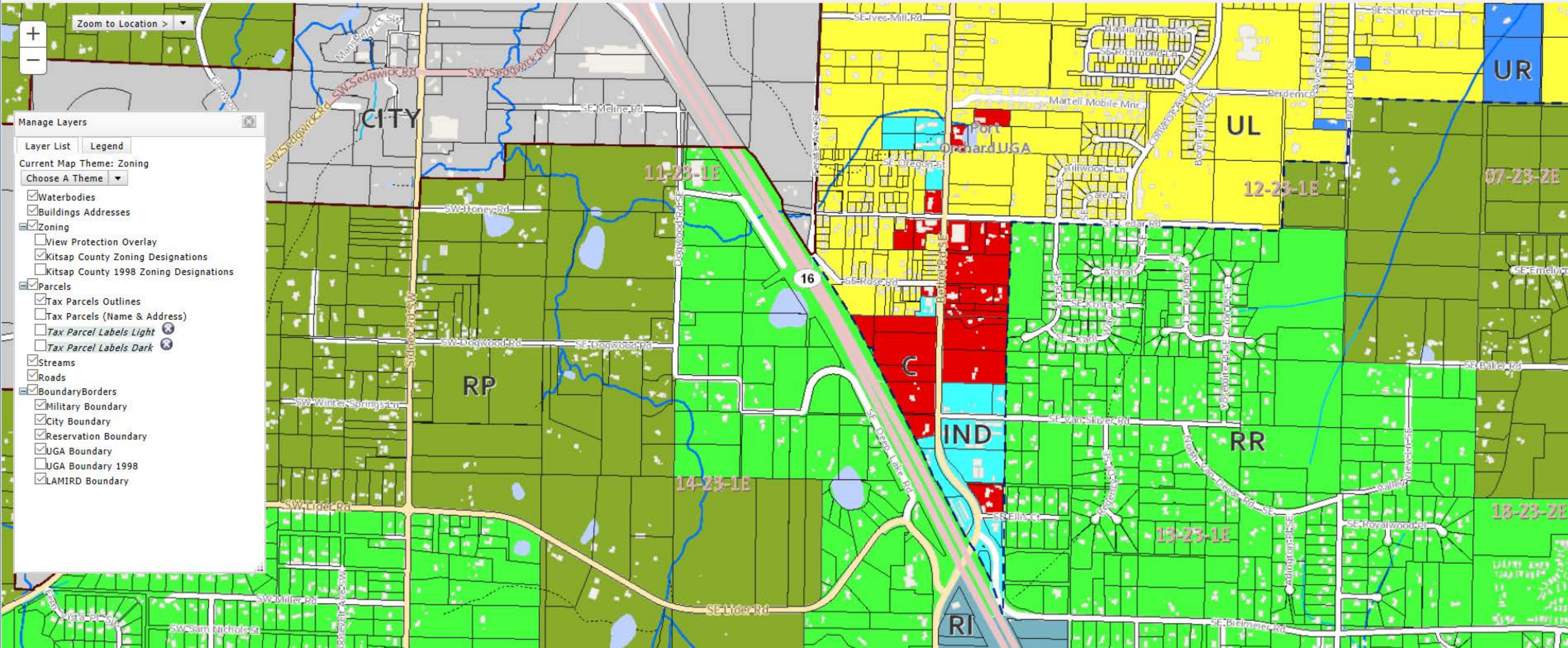
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- Buildings Addresses
- Zoning
 - View Protection Overlay
 - Kitsap County Zoning Designations
 - Kitsap County 1998 Zoning Designations
- Parcels
 - Tax Parcels Outlines
 - Tax Parcels (Name & Address)
 - Tax Parcel Labels Light
 - Tax Parcel Labels Dark
- Streams
- Roads
- BoundaryBorders
 - Military Boundary
 - City Boundary
 - Reservation Boundary
 - UGA Boundary
 - UGA Boundary 1998
 - LAMIRD Boundary

**REVISED ADDENDUM TO:
KITSAP COUNTY 2016 COMPREHENSIVE PLAN
UPDATE
Draft SEIS: November 6, 2015
Final SEIS: April 29, 2016
Addendum: September 12, 2016; Revised August 28, 2017**

Introduction

The Kitsap County 2016 Comprehensive Plan Update Supplemental Environmental Impact Statement (SEIS) evaluated the County's proposed Comprehensive Plan Update and associated changes to development regulations. Several of the County's proposed policies and development regulation amendments were designed to address reasonable measures to provide an efficient use of urban land and direct growth away from rural areas. Reasonable measures were evaluated in Appendix G of the Draft SEIS; a preferred list of measures was included as well in Appendix B of the Final SEIS.

In June 2016 Kitsap County adopted its Comprehensive Plan. Kitsap County is continuing to develop its implementing development regulations including those intended to promote reasonable measures to promote growth in urban areas.

This addendum describes the reasonable measures under consideration in relation to the alternatives considered in the Kitsap County 2016 Comprehensive Plan Update SEIS, and the Comprehensive Plan approved in June 2016. In August 2016, the Board adopted additional reasonable measures – including increased building height allowances and maximum urban lot size in certain zones - along with a resolution directing that measures relating to urban infill incentives, dry sewers and rural legacy lots be further analyzed and considered for adoption by June 2017.

This addendum also provides clarifications and corrections to the Final SEIS that do not substantially change the analysis of significant impacts and alternatives in the Final Supplemental Environmental Impact Statement (SEIS) issued for the Kitsap County 2016 Comprehensive Plan Update in April 2016.

Reasonable Measures

Kitsap County Countywide Planning Policies (2013) indicate each jurisdiction is to implement reasonable measures to support the efficient use of urban lands:

Policies for Urban Growth Areas (UGA). 2. Each jurisdiction is responsible for implementing appropriate reasonable measures within its jurisdictional boundaries. If the Buildable Lands Analysis shows that a jurisdiction's Comprehensive Plan growth goals are not being met, that jurisdiction shall consider implementing additional reasonable measures in order to use its designated urban land more efficiently.

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This section describes the reasonable measures under consideration in 2016-2017 by Kitsap County, the policies of the recently adopted Comprehensive Plan that are fulfilled, and similarity to the SEIS analysis of reasonable measures. All proposed measures would meet Growth Management Act goals to encourage growth in urban areas, reduce sprawl, and offer a greater variety in housing. (RCW 36.70A.020).

The measures would not alter growth targets, but would help Kitsap County achieve the intent of adopted urban land use plan designations and implementing zones designed to provide capacity to achieve the growth targets.

Exhibit 1. Options to Amend the Kitsap County Code to Address Reasonable Measures

#	Topic Title	2016 Comprehensive Plan Policy	Discussion
1.	Dry Sewer Policy KCC 17.410.050 (A)(48) 17.460.020	CapF and Utilities Policy 16. Require urban-level sanitary sewer service or equivalent service in all Urban Growth Areas. Update county-owned and operated sewer facility plans to include, not only capacity demand and needs, but also future major collection or conveyance systems for the 2036 planning horizon (existing and projected). CapF and Utilities Policy 26. Encourage the use of alternative sanitary sewer techniques within Urban Growth Areas, such as package plants, membrane and drip systems and/or community drainfields, in areas where public sewer system may be more than 200 feet away. The use of these alternative sanitary sewer techniques for new development shall also achieve minimum urban densities of the applicable zone.	This measure would allow properties within an Urban Growth Area, that are too far from sewer, to develop with increased densities provided a dry sewer is constructed with a mandatory sewer hook-up agreement to connect once sewer is available. This measure is related to Draft SEIS Appendix G, Section 4.8 Service and Infrastructure Investments in UGAs, including: Allow for and monitor alternative sanitary sewer systems in unincorporated UGAs.
2.	Reduced Regulatory Fees in UGA. Allow use of general funds for permit review when a project achieves the maximum density allowed by the zone. KCC: Commitment only during this code update. Code changes in development.	Land Use Policy 32. Explore the creation of incentives and streamlined administrative processes for new short plats in high priority areas to be identified within the Urban Growth Areas as a Reasonable Measure.	This measure is intended to encourage infill development and increased residential density in Urban Growth Areas. It could help increase densities in the range allowed by the plan and is similar in intent to measures considered in Draft SEIS Appendix G Sections 4.2 Focus Growth Near Transit, Urban Centers, and Urban Villages and 4.7 Reduce Administrative Barriers and Regulatory Requirements.

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#	Topic Title	2016 Comprehensive Plan Policy	Discussion
3.	Tax Incentives for Infill or Redevelopment KCC: Commitment only during this code update. Code changes in development.	Land Use Policy 5. Examine incentives for infill development.	The purpose is to temporarily reduce taxation rate for infill developments. This is similar to the concept behind the following measure addressed in Draft SEIS Appendix G, Section 6.3 Future Urban Measure to Monitor: Multifamily Tax Exemptions.
4.	Minimum Lot Size KCC 17.420.050 (A)	Land Use Policy 29. Through application of Growth Management Act goals, increase density in urban areas and limit sprawl in rural lands.	This measure would use lot averaging calculation method to achieve the minimum lot size in urban residential zoning districts. This would allow efficient use of lots that may differ in shape or critical areas. This is similar to Draft SEIS Appendix G, Mandate minimum densities for new subdivisions, and to Final SEIS Appendix B Minimum / Maximum urban lot size.
5.	Streamline Short Plat Process in UGA KCC: Commitment only during this code update. Code changes in development.	Land Use Policy 32. Explore the creation of incentives and streamlined administrative processes for new short plats in high priority areas to be identified within the Urban Growth Areas as a Reasonable Measure.	This measure would allow use of general funds for permit review when a detached single family dwelling permit requires the subdivision of one parcel into three or less parcels. It could help property owners subdivide properties that may be less likely to convert to urban style development, and is similar in intent to measures considered in Draft SEIS Appendix G Sections 4.2 Focus Growth Near Transit, Urban Centers, and Urban Villages and 4.7 Reduce Administrative Barriers and Regulatory Requirements.
6.	Increased Heights Allowed in UGA KCC 17.420.050 (A) 17.420.060(17)	Land Use Policy 5. Examine incentives for infill development. SRC Policy 5. Allow increased heights and densities and parking requirement reductions as incentives to provide frontage improvements, additional open space, multi-family or affordable housing, rooftop gardens, and energy and environmental design certifications.	The proposal would increase heights in UM and UH zones. Base height UM: 35-45 feet, UH: 35-55 feet With footnote 17 UM: 35-55 feet, UH: 65 feet This is similar to Draft SEIS Appendix G, Section 4.6 Encourage Increased Density and Intensity of Development such as: Increased building height limits through incentives. See also Section 5.0 Summary of Trends where expanding height incentives to other zones was addressed.

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#	Topic Title	2016 Comprehensive Plan Policy	Discussion
7.	<p>Infrastructure Investment in UGA. Target infrastructure development to support other reasonable measures. For example, combine incentives for building in certain areas of a UGA with increased infrastructure development in the same area.</p> <p>KCC: Coordination with public works TIP and CIP</p>	<p>CapF and Utilities Policy 17. Prioritize the Urban Growth Areas in Kitsap County expenditures for public services and facilities as an incentive to encourage development, to make urban areas desirable places to live and to use existing infrastructure more efficiently and cost effectively.</p>	<p>Similar to Draft SEIS Appendix G, Section 4.8 Service and Infrastructure Investments in UGAs, including Targeted Capital Facilities Investments. See also Section 5.0 Summary of Trends which discusses priorities for infrastructure investments in higher density areas.</p>
8.	<p>Lot Size Averaging in UGA</p> <p>KCC 17.420.050 (A) 17.420.060 (19)</p>	<p>Land Use Policy 29. Through application of Growth Management Act goals, increase density in urban areas and limit sprawl in rural lands.</p>	<p>This measure would use lot averaging calculation method to achieve the minimum lot area and dimensions in urban residential zoning districts. This would allow efficient use of lots that may differ in shape or critical areas. This is similar to Draft SEIS Appendix G, Mandate minimum densities for new subdivisions, and to Final SEIS Appendix B Minimum / Maximum urban lot size.</p>
9.	<p>Remove minimum lot widths in UGAs</p> <p>KCC 17.420.050 (A) 17.420.060 (19)</p>	<p>Land Use Policy 5. Examine incentives for infill development.</p>	<p>This provision would remove minimum lot widths in urban residential zoning districts to increase flexibility and achieve the densities allowed in the zones. This could help increase the average density of development in zones where added lots become feasible with the removal of the lot width standard. It would be similar to concepts designed to Increase Urban Residential Densities per Section 4.1 of the Reasonable Measures evaluation in Draft SEIS Appendix G.</p>

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#	Topic Title	2016 Comprehensive Plan Policy	Discussion
10.	<p>Transfer of Development Rights</p> <p>KCC: TDR code revised concurrent with Comprehensive Plan update. Board to consider resolution establishing increased ratios in support of code updates.</p>	<p>Land Use Policy 73. Develop and promote incentives for continued rural and resource uses, including but not limited to a Transfer of Development Rights program, an expedited agricultural activity permit review program, and educational and agritourism activities.</p>	<p>This would establish increased ratios and use market based values for Transfer of Development Rights. See Draft SEIS Appendix G, 6.1 Amend Reasonable Measures, Transfer of Development Rights, and Final SEIS Appendix B.</p>
11.	<p>Rural Legacy Lots: Footnote 39</p> <p>KCC 17.420.060 (39)</p>	<p>Land Use Policy 29. Through application of Growth Management Act goals, increase density in urban areas and limit sprawl in rural lands.</p>	<p>Note: The following measure was considered, but is no longer proposed. The Board has directed staff to conduct further analysis of Rural Legacy Lots prior to taking any action on this measure.</p> <p>Building permits are not allowed for rural lots located outside a UGA or designated LAMIRD, created prior to July 1, 1974, less than one acre, contiguous ownership, and no improvements.</p> <p>Similar to Final SEIS Appendix B, Recognition of Rural Legacy Lots, and concepts addressed in Draft SEIS Appendix G Reasonable Measures evaluation, Section 6.2 Consider New Reasonable Measures, Rural Lot Aggregation.</p>
12.	<p>Maximum Urban Lot Size</p> <p>KCC 17.420.050 (A) 17.420.060 (19)</p>	<p>Land Use Policy 29. Through application of Growth Management Act goals, increase density in urban areas and limit sprawl in rural lands.</p>	<p>This measure would establish a 9,000 square foot maximum lot size in Urban Low Residential (ULR) and Urban Cluster Residential (UCR) zones. This is similar to Final SEIS Appendix B Minimum/Maximum Urban Lot Size, and the Maximum Urban Lot Size measure described in Draft SEIS Appendix G Reasonable Measures evaluation, Section 6.2 Consider New Reasonable Measures.</p>

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#	Topic Title	2016 Comprehensive Plan Policy	Discussion
13.	Urban Medium Residential and Urban High Residential Use Permissibility KCC 17.410.040 (A) 17.410.050(26) 17.410.050(37) 17.410.050(47)	Land Use Policy 29. Through application of Growth Management Act goals, increase density in urban areas and limit sprawl in rural lands.	This measure reinforces the zone intent as an area designated for higher density residential uses and allows for zones to provide the variety in housing type and affordability. The provisions would: <ul style="list-style-type: none"> • Add footnote 26 to limit ability to develop detached single family dwellings. • Modify footnote 37 language and application to require residential uses in conjunction with certain allowed commercial uses. • Modify footnote 47 for consistency with zone intent with regards to commercial and mixed use development. This is similar to the range of measures in Section 4.2 Focus Growth Near Transit, Urban Centers, and Urban Villages, which seeks to encourage transportation-efficient land use and to encourage development of urban centers and villages described in Draft SEIS Appendix G.

Growth Levels, Final Plan June 2016

In June 2016, Kitsap County adopted its Comprehensive Plan Update and allows growth levels in the range of the SEIS alternatives. This section describes the Final Plan and its placement in the range of SEIS alternatives.

Growth targets are adopted by the Countywide Planning Policies (CPPs) for the period 2010-2036. In Exhibit 2 growth targets are presented with a 2012 base year (see Draft and Final SEIS for details). Each growth alternative would represent different patterns and capacities for growth and are compared to the growth targets to determine if they can adequately accommodate growth.

The SEIS Alternatives study the impacts of expected city growth collectively with unincorporated UGA growth. City limit boundaries are not sized in the same way as unincorporated UGAs are sized. Cities may plan for expected growth rather than the full capacity for growth within their city limits. Though city limits are not “sized” under GMA, Kitsap County has considered the sizing of Unincorporated UGAs in light of city capacities. Accordingly, the SEIS Alternatives each have growth assumptions for the cities generally including adopted plan growth assumptions or assuming growth targets plus 5% (see Draft SEIS Appendix B or Final SEIS Appendix A).

The Preferred Alternative studied in the Final SEIS allowed capacity for a countywide growth assumption of 78,493 persons between 2012-2036 considering city limit growth assumptions and unincorporated UGA capacities. The Final Plan added in a land use request and abutting urbanized land in the Silverdale UGA studied under Alternative 3, and slightly modified the population total to be 78,606 persons, a change of 112 people. Because the Silverdale UGA as analyzed in the Preferred Alternative had excess capacity for 82 people, this addition resulted in a slight overage (30 people) to the Silverdale UGA, which is less than 1% over the target population. Countywide, population growth assumptions would be 2% above of CPP growth targets as noted for the Preferred Alternative. Attached hereto as Attachment A are revised land capacity analysis worksheets for the Silverdale UGA.

Regarding employment, a commercial interchange area in the Port Orchard Urban Growth Area (UGA) under Alternative 1 was retained in the Final Plan, whereas it was excluded in the Preferred Alternative. Under the Preferred Alternative, employment growth capacity was anticipated at 52,409 countywide, whereas it is 52,594 with the Final Plan. Employment growth capacity would be 13% above CPP growth targets rather than 12% identified with the Preferred Alternative.

See Exhibit 2. Final Plan Growth Capacities. Also see Final SEIS Appendix A with a method for growth distribution across the county.

Exhibit 2. Final Plan Growth Capacities

City or UGA	Adjusted Population Growth Target 2012-2036	Final Plan Population Growth Capacity	Difference with Population Target	Adjusted Employment Growth Target 2012-2036	Final Plan Employment Growth Capacity	Difference with Employment Target
City of Bremerton	12,367	13,757	1,390	18,276	21,191	2,915
Bremerton UGA	3,972	4,028	56	1,443	1,689	246
Total Bremerton	16,339	17,785	1,446	19,719	22,880	3,161

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City of Bainbridge Island	5,570	5,849	279	2,720	2,856	136
City of Port Orchard	8,778	10,358	1,580	3,074	5,570	2,496
Port Orchard UGA	6,110	4,600	(1,510)	1,140	1,377	237
Total Port Orchard	14,888	14,958	70	4,214	6,947	2,733
City of Poulsbo	1,192	5,227	249	4,138	4,345	207
Poulsbo UGA	3,786	See above*		14	64	50
Total Poulsbo	4,978	5,227	249	4,152	4,409	257
Central Kitsap UGA	6,842	6,375	(467)	1,885	1,793	(92)
Silverdale UGA	8,723	8,753	30	8,928	8,592	(336)
Kingston UGA	2,926	2,854	(72)	597	685	88
Total City	27,907	35,191	3,498	28,208	33,962	5,754
Unincorporated UGA	32,359	26,610	(1,963)	14,007	14,200	193
Total City and UGA	60,266	61,801	1,535	42,215	48,161	5,946
Rural Non-UGA	16,805	16,805	0	4,432	4,432	0
Total	77,071	78,606	1,535	46,647	52,594	5,947

Notes: See Final SEIS Appendix A for information on city assumptions.

*The Poulsbo unincorporated and incorporated UGAs were treated as a single unit based on joint planning and the small size of the unincorporated UGA.

Source: Kitsap County Community Development Department; BERK Consulting 2015

Counties are required to size unincorporated UGAs to accommodate growth that is planned; they should be sized to promote urban patterns of development with appropriate urban services and to avoid sprawl. Unincorporated UGAs are evaluated based on growth capacity in Exhibit 3. The Final Plan shows that collectively the unincorporated UGAs would be below population targets by 5-7%, similar to the Preferred Alternative, and within 1% of for employment capacity and targets, slightly higher than the Preferred Alternative.

Exhibit 3. Final Plan Unincorporated UGA Capacities and Target

Unincorporated UGA	Adjusted Pop. Growth Target 2012-2036	Final Plan Population Growth Capacity	Difference with Population Target	% Diff. Population Target	Adjusted Emp. Growth Target 2012-2036	Preferred Alternative Emp. Growth Capacity	Difference with Emp. Target	% Diff. Emp. Target
Bremerton	3,972	4,028	56	1%	1,443	1,689	246	17%
Port Orchard	6,110	4,600	(1,510)	-25%	1,140	1,377	237	21%
Poulsbo City + UGA	4,978	5,227	249	5%				
Poulsbo UGA only					14	64	50	355%
Central Kitsap	6,842	6,375	(467)	-7%	1,885	1,793	(92)	-5%
Silverdale	8,723	8,753	30	0%	8,928	8,592	(336)	-4%
Kingston	2,926	2,854	(72)	-2%	597	685	88	15%
Total excl. Poulsbo	28,573	26,610	(1,963)	-7%	N/A	N/A	N/A	N/A
Total with Poulsbo	33,551	31,837	(1,714)	-5%	14,007	14,200	193	1%

Source: Kitsap County Community Development Department; BERK Consulting 2015

Comparing the Final Plan to the SEIS Alternatives, UGAs countywide have capacity comparable to Alternative 3, the maximum studied, and the unincorporated UGA population capacity is very close to the Preferred Alternative. In terms of employment the Final Plan provides less capacity than Alternatives 1 and 2 (the latter was the maximum studied), both at the countywide and unincorporated UGA scales. The Final Plan employment growth is similar to Alternative 2 and the Preferred Alternative. At a countywide level UGA boundaries under the Final Plan would be more similar to Alternative 2 and the Preferred Alternative. See Exhibit 4.

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Exhibit 4. Comparison of Alternative Growth Assumptions

Topic	Alternative 1 No Action	Alternative 2 Whole Community	Alternative 3 All Inclusive	Preferred Alternative	Final Plan
Growth					
Countywide Population 2036 Assumptions	329,923	331,550	333,076	332,993	333,053
Countywide Population Growth Targets 2012-2036	77,071	77,071	77,071	77,071	77,071
Countywide Population Growth 2012-2036	75,423	77,050	78,576	78,493	78,606
Unincorporated UGA Targets 2012-2036 (range with and without the combined Poulsbo city limits and UGA)	33,551 28,573	33,551 28,573	33,551 28,573	33,551 28,573	33,551 28,573
Unincorporated UGA Population Capacity	29,630	31,053 25,826	32,579 27,353	31,725 26,498	31,837 26,610
Unincorporated UGA Population Capacity % within Target (range with and without the combined Poulsbo city limits and UGA)	-11%	-7 to -10%	-3 to -4%	-5 to -7%	-5 to -7%
Countywide Employment 2036 Assumptions	129,760	134,425	131,980	131,987	132,171
Countywide Employment Growth Targets 2012-2036	46,647	46,647	46,647	46,647	46,647
Countywide Employment Growth 2013-2036	50,182	54,847	52,402	52,409	52,593
Unincorporated UGA Targets 2012-2036	14,007	14,007	14,007	14,007	14,007
Unincorporated UGA Employment Capacity	15,719	16,453	14,008	14,015	14,200
UGA Employment Capacity % within Target	12%	17%	0%	0%	1%
Unincorporated UGAs					
UGAs with Areas of Expansion	None	Silverdale, West Bremerton	Kingston, Silverdale, Central Kitsap, West Bremerton	Kingston, Silverdale, Central Kitsap, West Bremerton	Kingston, Silverdale, Central Kitsap, West Bremerton
UGAs with Areas of Reduction	None	Central Kitsap, East Bremerton, Port Orchard	Central Kitsap, East Bremerton, Port Orchard	Silverdale, Port Orchard	Silverdale, Port Orchard

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Topic	Alternative 1 No Action	Alternative 2 Whole Community	Alternative 3 All Inclusive	Preferred Alternative	Final Plan
Total UGA Acres*	18,949	18,167	19,703	18,745	18,962
Plans and Policies					
Comprehensive Plan Goals, Policies and Strategies Updated		X	X	X	X
Future Land Use Plan and Zoning Amended		X	X	X	X
Subarea, LAMIRD and Community Plan goals and policies Updated		X	X	X	X
Silverdale RGC Plan Alternatives		X	X	X	X
Capital Facility Plan Updated		X	X	X	X

Note: *Includes areas of parcels and roads and excludes water.

Source: Kitsap County Community Development; BERK Consulting 2015

A table summarizing the acres of each UGA under each alternative is also provided below. Areas of expansion and reduction in individual locations vary between Alternatives.

- Kingston would have no change under Alternative 2 and an increase under Alternative 3, the Preferred Alternative, and the Final Plan.
- Poulsbo UGA would not change under any alternative.
- The amount of the Silverdale UGA expansion varies from 25 to 705 acres under Alternatives 2 and 3 respectively. The Preferred Alternative would reduce the UGA by a net 61 acres and the Final Plan would increase it by 138 acres, although much of that includes an already developed area that has urban services in the Chico area.
- Central Kitsap would be reduced under Alternative 2 and increased under Alternative 3, the Preferred Alternative, and the Final Plan.
- The West Bremerton portion of the Bremerton UGA would be increased and the East Bremerton portion reduced for a net increase in the total Bremerton UGA under both Alternatives 2 and 3. The Preferred Alternative and the Final Plan would maintain East Bremerton UGA boundaries, and expand West Bremerton UGA boundaries though primarily for city watershed purposes.
- The Port Orchard UGA would be reduced under Alternatives 2 and 3, as well as the Preferred Alternative and Final Plan.

As described by alternative and in the comparison chart in Exhibit 5, Alternative 1 represents the status quo. Alternative 2 would reduce UGA acres overall by 4% while Alternative 3 would increase UGA acres by 4%. The Preferred Alternative would reduce UGA acres overall by 1%. The Final Plan would change UGA boundaries by less than 1% compared to No Action. However, it should be noted that Alternative 3 and the Final Plan included nearly 500 acres of city-owned watershed into the Bremerton UGA for municipal purposes and not for growth. If that area were excluded, Alternative 3 would increase the UGA territory by 1% and the Final Plan would reduce it by over 2% compared to Alternative 1.

Exhibit 5. Unincorporated UGA Acres by Alternative

UGA	Alternative 1	Alternative 2	Alternative 3	Preferred	Final Plan	Difference Alt 2 - Alt 1	Difference Alt 3 - Alt 1	Difference Preferred - Alt 1	Difference Final - Alt 1
Bremerton	2,563	2,815	2,815	3,058	3,058	252	252	495	495
Bremerton East UGA	1,141	900	900	1,141	1,141	(241)	(241)	0	0
Bremerton West UGA	1,094	1,587	1,587	1,591	1,591	493	493	496	496
Gorst UGA	328	328	328	328	328	0	0	0	0
Central Kitsap UGA	5,562	5,406	5,967	5,582	5,582	(156)	405	20	20
Kingston UGA	1,070	1,070	1,212	1,145	1,145	0	142	75	75
Port Orchard UGA	3,810	2,907	3,059	3,077	3,094	(904)	(751)	(734)	(717)
Poulsbo UTA	428	428	428	428	428	0	0	0	0
Silverdale	5,516	5,541	6,221	5,455	5,654	25	705	(61)	138
TOTAL	18,949	18,167	19,702	18,745	18,961	(783)	753	(205)	11

Source: Kitsap County GIS; BERK Consulting 2015

Because UGA capacity analysis is at a planning level, Kitsap County has established a margin of error of 5%. UGA land capacity results within +/-5% of the growth allocation are considered in balance. Reviewing the capacities of the unincorporated UGAs using standard assumptions for land capacity, the unincorporated UGAs are within -5% of the population target and just 1% above employment targets. Thus, regarding UGA sizing, the Final Plan is sized within planning tolerances.

¹ See the three documents where the margin of tolerance is discussed: 1. Kitsap County 10-Year Comprehensive Plan Update – Integrated Plan and Environmental Impact Statement (EIS), Volume II: Final EIS, December 2006. 2. Kitsap County Urban Growth Area (UGA) Sizing and Composition Remand, Final Supplemental EIS, August 10, 2012. 3. Kitsap County 2016 Comprehensive Plan Update, Final Supplemental EIS, April 2016.

²The Growth Management Hearings Board has recognized that “it is an unrealistic expectation of any county, in creating the right combination of parcel sizes to accommodate the allocated population that every UGA must be exactly the right size (not too large and not too small) to accommodate only the number of people allocated to it.” *Stalheim et al. v. Whatcom County*, WWGMHB No. 10-2-0016c, FDO (4/11/2011).

Update Clarifications and Corrections

This section provides some clarifications and corrections to the Final SEIS that do not change the programmatic analysis of the studied alternatives or the relative difference among alternatives.

Amend final paragraph on page 2-30 to reference the Preferred Alternative rather than Alternative 2.

Unincorporated UGAs are evaluated based on growth capacity in Exhibit 2.6-20. The Preferred Alternative shows that UGAs would be below population targets by 5-6% and essentially in balance for employment capacity and targets. Under Alternative 2, the unincorporated UGAs would be below population targets by 7% and above employment targets by about 17%. However, because Silverdale’s employment growth is essentially occurring in present UGA boundaries (with a less than 1% UGA change for industrial lands), growth would largely occur in the existing urban footprint of the Silverdale RGC. If the Silverdale employment growth is excluded, the percentage above employment targets across the County would drop to 3%.

Amend the second paragraph under Section 3.2.2.1 on page 3-8 to identify the employment capacity of the Preferred Alternative rather than Alternative 2.

Countywide, Alternative 2 assumes employment growth above targets by ~~1812%~~, as described in Chapter 2. Unincorporated UGA employment capacity would be ~~17% above~~ within balance of the target requirements for these areas. ~~Much of the greater supply in employment is based on an intensification of retail and office uses in the Silverdale RGC. If that employment were reduced to a more moderate level, the employment levels would be within 5% of the target for UGAs and considered in balance within a reasonable margin of tolerance.~~

Amend Exhibit 3.3-34 on page 3-38 to address more current sewer capital costs included in the Bremerton Comprehensive Plan, 2016.

Exhibit 3.3-34 Sewer Cost Comparison by Provider and Alternative 2016-2036 (All Amounts in \$1,000)

UGA	No Action	Alternative 2	Alternative 3	Preferred Alternative
Bremerton (City)	\$334,969 \$304,633	\$334,969 \$304,633	\$334,969 \$304,633	\$334,969 \$304,633
Port Orchard (City)	\$7,470	\$7,470	\$7,470	\$7,470
WSUD*	\$31,685	\$27,085	\$27,085	\$27,835
Poulsbo (City)	\$11,655	\$11,655	\$11,655	\$11,655
Kitsap County	\$338,404	\$333,004	\$354,004	\$341,263

Note: *WSUD confirmed Capital Facility Plan estimates for No Action and Alternatives 2 and 3, and these are reflected in the table with a similar relative difference. The Preferred Alternative adds one pump station in the District’s Capital Facility Plan on Bethel Road SE as it is retained in the UGA compared to Alternatives 2 and 3.

Source: WSUD 2015; BHC 2015 and 2016

ATTACHMENT A: Revised Land Capacity Analysis Worksheets

SILVERDALE
URBAN GROWTH AREA
Comment 2.47C
Land Capacity - Final

UNDERUTILIZED LANDS	URBAN LOW 6.0 DU/AC	SENIOR LIVING HOMESTEAD 6.0 DU/AC	URBAN MEDIUM 12 DU/AC	URBAN HIGH 21.75 DU/AC	URBAN RESTRICTED 2.5 DU/AC	URBAN CLUSTER 7.6 DU/AC	GREENBELT 2 DU/AC	REGIONAL CENTER 10 DU/AC	GRAND TOTAL
Gross Residential Acres (Developed Parcels only)									
Total Gross Acres	1967.59	0.00	89.35	186.32	201.47	0.00	0.00	0.00	2444.73
Multi - Family Dwelling (-)	44.75	0.00	25.64	80.04	5.99	0.00	0.00	0.00	156.42
Parcels < 0.50 Acres (-)	740.52	0.00	27.48	35.90	6.57	0.00	0.00	0.00	810.47
Public/Current Use/Utilities (-)	516.91	0.00	18.63	42.67	52.82	0.00	0.00	0.00	631.03
Shoreline Parcel < 1.00 Acre (-)	0.82	0.00	0.00	0.00	2.38	0.00	0.00	0.00	3.20
Subtotal	664.59	0.00	17.60	27.71	133.71	0.00	0.00	0.00	843.61
Redevelopable Acres									
<i>Median Value \$159712</i>									
Density < 2.5 (-)	49.77	0.00	0.00	0.00	44.41	0.00	0.00	0.00	94.18
Density >= 2.5 - < 4.00 Value > (-)	123.00	0.00	0.00	0.00	6.21	0.00	0.00	0.00	129.21
Density >= 4.0 - < 5.00 Value > (-)	142.32	0.00	0.00	0.00	7.11	0.00	0.00	0.00	149.43
Density >= 5.0 - < 10.00 Value > (-)	34.57	0.00	0.00	0.00	6.40	0.00	0.00	0.00	40.97
Subtotal	314.93	0.00	17.60	27.71	69.58	0.00	0.00	0.00	429.82
Remove 100% of Platted lots (acres)	69.14	0.00	0.00	1.35	0.00	0.00	0.00	0.00	70.49
Critical Areas									
Total Redevelopable Acres	245.79	0.00	17.60	26.36	69.58	0.00	0.00	0.00	359.33
Unencumbered Acres	201.78	0.00	16.02	19.80	37.72	0.00	0.00	0.00	275.32
Acres within Critical Areas	32.92	0.00	1.57	0.52	27.25	0.00	0.00	0.00	62.26
Critical Areas reduction 75% (-)	8.23	0.00	0.39	0.13	6.81	0.00	0.00	0.00	15.57
Acres within Area of Concern	11.06	0.00	0.00	6.03	4.59	0.00	0.00	0.00	21.68
Area of Concern reduction 50% (-)	5.53	0.00	0.00	3.02	2.30	0.00	0.00	0.00	10.84
Subtotal	215.54	0.00	16.41	22.95	46.83	0.00	0.00	0.00	301.73
Roads/Right-of-Way (Future)									
20% (-)	172.43	0.00	13.13	18.36	37.46	0.00	0.00	0.00	241.38
Public Facility (Future)									
20% (-)	137.95	0.00	10.50	14.68	29.97	0.00	0.00	0.00	193.10
Unavailable Lands									
15% (-)	117.25	0.00	8.93	12.48	25.47	0.00	0.00	0.00	164.14
Commercial Split									
50% (-)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Platted Lots									
Underutilized Platted Lots 25% (+)	17.29	0.00	0.00	0.34	0.00	0.00	0.00	0.00	17.62
Platted Lot existing dwelling unit (count)	46	0	0	2	0	0	0	0	48
Dwelling Unit count (-75%)	12	0	0	1	0	0	0	0	12
Existing Dwelling Units									
Existing Dwelling Units (non platted lots) (-)	141	0	14	25	22	0	0	0	202
Total Dwelling Units (-)	153	0	14	26	22	0	0	0	214
TOTALS									
Net Developable Acres	134.54	0.00	8.93	12.82	25.47	0.00	0.00	0.00	181.76
	6.0 DU/AC	6.0 DU/AC	12 DU/AC	21.75 DU/AC	2.5 DU/AC	7.6 DU/AC	2 DU/AC	10 DU/AC	
Dwelling Unit Capacity	655	0	93	253	42	0	0	0	1043
	2.5 pph	1.8 pph	2.5 pph	1.8 pph	2.5 pph	2.5 pph	2.5 pph	1.8 pph	
Population Capacity	1637	0	233	456	104	0	0	0	2430

SILVERDALE
 Comment 2.47C
 URBAN GROWTH AREA
 Land Capacity - Final

VACANT LAND	URBAN LOW <i>6.0 DU/AC</i>	SENIOR LIVING HOMESTEAD <i>6.0 DU/AC</i>	URBAN MEDIUM <i>12 DU/AC</i>	URBAN HIGH <i>21.75 DU/AC</i>	URBAN RESTRICTED <i>2.5 DU/AC</i>	URBAN CLUSTER <i>7.6 DU/AC</i>	GREENBELT <i>2 DU/AC</i>	REGIONAL CENTER <i>10 DU/AC</i>	GRAND TOTAL
Gross Residential Zoned Acres									
Total Gross Vacant Acres	392.48	0.00	64.41	14.86	98.08	0.00	0.00	0.00	569.83
Subtotal	392.48	0.00	64.41	14.86	98.08	0.00	0.00	0.00	569.83
Redevelopable Acres									
Subtotal	392.48	0.00	64.41	14.86	98.08	0.00	0.00	0.00	569.83
Vacant Platted Lots (-)	34.72	0.00	3.87	0.50	0.00	0.00	0.00	0.00	39.09
Critical Areas									
Total Redevelopable Acres	357.76	0.00	60.54	14.36	98.08	0.00	0.00	0.00	530.74
Unencumbered Acres	299.08	0.00	45.50	5.33	23.49	0.00	0.00	0.00	373.40
Acres within Critical Areas	20.45	0.00	8.33	2.50	54.89	0.00	0.00	0.00	86.17
Critical Areas reduction 75% (-)	5.11	0.00	2.08	0.63	13.72	0.00	0.00	0.00	21.54
Acres within Area of Concern	38.23	0.00	6.69	6.53	19.69	0.00	0.00	0.00	71.14
Area of Concern reduction 50% (-)	19.12	0.00	3.35	3.27	9.85	0.00	0.00	0.00	35.57
Subtotal	323.31	0.00	50.93	9.22	47.06	0.00	0.00	0.00	430.51
Roads/Right-of-Way (Future)									
20% (-)	258.65	0.00	40.74	7.38	37.65	0.00	0.00	0.00	344.41
Public Facility (Future)									
20% (-)	206.92	0.00	32.59	5.90	30.12	0.00	0.00	0.00	275.53
Unavailable Lands									
5% (-)	196.57	0.00	30.96	5.61	28.61	0.00	0.00	0.00	261.75
Commercial Split									
50% (-)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Platted Lots									
Vacant Platted Lots	298	0	40	2	0	0	0	0	340
Net Developable Acres									
	196.57	0.00	30.96	5.61	28.61	0.00	0.00	0.00	261.75
	<i>6.0 DU/AC</i>	<i>6.0 DU/AC</i>	<i>12 DU/AC</i>	<i>21.75 DU/AC</i>	<i>2.5 DU/AC</i>	<i>7.6 DU/AC</i>	<i>2 DU/AC</i>	<i>10 DU/AC</i>	
Dwelling Unit Capacity	1477	0	412	124	72	0	0	666	2750
	<i>2.5 pph</i>	<i>1.8 pph</i>	<i>2.5 pph</i>	<i>1.8 pph</i>	<i>2.5 pph</i>	<i>2.5 pph</i>	<i>2.5 pph</i>	<i>1.8 pph</i>	
Population Capacity	3694	0	1029	223	179	0	0	1199	6323

SILVERDALE
 Comment 2.4C
 URBAN GROWTH AREA
 Land Capacity - Final

UNDERUTILIZED LANDS	URBAN LOW <i>6.0 DU/AC</i>	SENIOR LIVING HOMESTEAD <i>6.0 DU/AC</i>	URBAN MEDIUM <i>12 DU/AC</i>	URBAN HIGH <i>21.75 DU/AC</i>	URBAN RESTRICTED <i>2.5 DU/AC</i>	URBAN CLUSTER <i>7.6 DU/AC</i>	GREENBELT <i>2 DU/AC</i>	REGIONAL CENTER <i>12 DU/AC</i>	GRAND TOTAL
Net Developable Acres	134.54	0.00	8.93	12.82	25.47	0.00	0.00	0.00	181.76
Dwelling Unit Capacity	655	0	93	253	42	0	0	0	1043
	<i>2.5 pph</i>	<i>1.8 pph</i>	<i>2.5 pph</i>	<i>1.8 pph</i>	<i>2.5 pph</i>	<i>2.5 pph</i>	<i>2.5 pph</i>	<i>1.8 pph</i>	
Population Capacity	1637	0	233	456	104	0	0	0	2430

VACANT LAND	URBAN LOW <i>6.0 DU/AC</i>	SENIOR LIVING HOMESTEAD <i>6.0 DU/AC</i>	URBAN MEDIUM <i>12 DU/AC</i>	URBAN HIGH <i>21.75 DU/AC</i>	URBAN RESTRICTED <i>2.5 DU/AC</i>	URBAN CLUSTER <i>7.6 DU/AC</i>	GREENBELT <i>2 DU/AC</i>	REGIONAL CENTER <i>12 DU/AC</i>	GRAND TOTAL
Net Developable Acres	196.57	0.00	30.96	5.61	28.61	0.00	0.00	0.00	261.75
Dwelling Unit Capacity	1477	0	412	124	72	0	0	666	2750
	<i>2.5 pph</i>	<i>1.8 pph</i>	<i>2.5 pph</i>	<i>1.8 pph</i>	<i>2.5 pph</i>	<i>2.5 pph</i>	<i>2.5 pph</i>	<i>1.8 pph</i>	
Population Capacity	3694	0	1029	223	179	0	0	1199	6323

GRAND TOTAL	URBAN LOW <i>6.0 DU/AC</i>	SENIOR LIVING HOMESTEAD <i>6.0 DU/AC</i>	URBAN MEDIUM <i>12 DU/AC</i>	URBAN HIGH <i>21.75 DU/AC</i>	URBAN RESTRICTED <i>2.5 DU/AC</i>	URBAN CLUSTER <i>7.6 DU/AC</i>	GREENBELT <i>2 DU/AC</i>	REGIONAL CENTER <i>12 DU/AC</i>	GRAND TOTAL
Net Developable Acres	331.11	0.00	39.89	18.43	54.09	0.00	0.00	0.00	443.51
Dwelling Unit Capacity	2132	0	505	377	113	0	0	666	3793
	<i>2.5 pph</i>	<i>1.8 pph</i>	<i>2.5 pph</i>	<i>1.8 pph</i>	<i>2.5 pph</i>	<i>2.5 pph</i>	<i>2.5 pph</i>	<i>1.8 pph</i>	
Population Capacity	5330	0	1262	679	283	0	0	1199	8753

SILVERDALE
Comment 2.47C
 URBAN GROWTH AREA
 Commercial Land Capacity
 Final

UNDERUTILIZED LANDS	INDUSTRIAL	BUSINESS CENTER	BUSINESS PARK	NEIGHBORHOOD COMMERCIAL	COMMERCIAL	REGIONAL CENTER	URBAN VILLAGE CENTER	GRAND TOTAL
Gross Acres (Developed Parcels only)								See Residential Sheet for split
Total Gross Acres	373.86	27.46	4.76	41.46	166.63	523.14	0.00	1137.31
Multi - Family Dwelling (-)	12.80	0.00	0.00	4.20	0.00	40.13	0.00	57.13
Public/Current Use/Utilities (-)	278.43	11.53	4.76	31.63	55.82	431.56	0.00	813.73
Subtotal	82.63	15.93	0.00	5.63	110.81	51.45	0.00	266.45
Remove 100% Planned Lots	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Remove 100% Platted	10.65	0.00	0.00	3.63	1.39	3.77	0.00	19.44
Subtotal	71.98	15.93	0.00	2.00	109.42	47.68	0.00	247.01
Critical Areas								
Total Redevelopable Acres	71.98	15.93	0.00	2.00	109.42	47.68	0.00	247.01
Unencumbered Acres	41.00	4.75	0.00	1.54	59.47	31.81	0.00	138.57
Acres within Critical Areas	21.24	5.06	0.00	0.11	25.12	15.18	0.00	66.71
Critical Areas reduction 75% (-)	5.31	1.27	0.00	0.03	6.28	3.80	0.00	16.68
Acres within Area of Concern	9.73	6.11	0.00	0.33	24.82	0.66	0.00	41.65
Area of Concern reduction 50% (-)	4.87	3.06	0.00	0.17	12.41	0.33	0.00	20.83
Subtotal	51.18	9.07	0.00	1.73	78.16	35.94	0.00	176.07
Roads/Right-of-Way (Future)								
20% (-)	40.94	7.26	0.00	1.39	62.53	28.75	0.00	140.86
Public Facility (Future)								
20% (-)	32.75	5.80	0.00	1.11	50.02	23.00	0.00	112.69
Unavailable Lands								
25% (-)	24.56	4.35	0.00	0.83	37.52	17.25	0.00	84.51
Underutilized Acres	35.21	4.35	0.00	4.46	38.91	21.02	0.00	103.95
VACANT LAND								
Gross Acres								See Residential Sheet for split
Total Gross Acres	260.41	19.58	0.00	1.08	82.46	37.50	0.00	401.03
Remove 100% Planned Development	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Remove 100% Platted Lots	15.91	0.00	0.00	1.08	0.00	3.01	0.00	20.00
Subtotal	244.50	19.58	0.00	0.00	82.46	34.49	0.00	381.03
Critical Areas								
Total Redevelopable Acres	244.50	19.58	0.00	0.00	82.46	34.49	0.00	381.03
Unencumbered Acres	149.29	14.78	0.00	0.00	49.12	23.26	0.00	236.45
Acres within Critical Areas	26.56	0.53	0.00	0.00	21.28	11.22	0.00	59.59
Critical Areas reduction 75% (-)	6.64	0.13	0.00	0.00	5.32	2.81	0.00	14.90
Acres within Area of Concern	68.64	4.27	0.00	0.00	12.06	0.00	0.00	84.97
Area of Concern reduction 50% (-)	34.32	2.14	0.00	0.00	6.03	0.00	0.00	42.49
Subtotal	190.25	17.05	0.00	0.00	60.47	26.07	0.00	293.83
Roads/Right-of-Way (Future)								
20% (-)	152.20	13.64	0.00	0.00	48.38	20.85	0.00	235.07
Public Facility (Future)								
20% (-)	121.76	10.91	0.00	0.00	38.70	16.68	0.00	188.05
Unavailable Lands								
20% (-)	97.41	8.73	0.00	0.00	30.96	13.35	0.00	150.44
Vacant Acres	113.32	8.73	0.00	1.08	30.96	16.36	0.00	170.44
							37.37	
Total Net Developable Acres	148.53	13.08	0.00	5.54	69.87	46.72	0.00	283.74
Total Square Footage Planned Developments	36300	0	0	0	0	36629	0	72929

SILVERDALE
 Comment 2.47C
 URBAN GROWTH AREA
 Employment Capacity
 Final

	INDUSTRIAL	BUSINESS CENTER	BUSINESS PARK	NEIGHBORHOOD COMMERCIAL	COMMERCIAL	REGIONAL CENTER	URBAN VILLAGE CENTER	GRAND TOTAL
Underutilized Acres	35.21	4.35	0.00	4.46	38.91	26.27	0.00	109.21
Vacant Acres	113.32	8.73	0.00	1.08	30.96	20.44	0.00	174.53
Total Net Developable Acres	148.53	13.08	0.00	5.54	69.87	46.71	0.00	283.74
Underutilized Square Feet	1533922	189643	0	194347	1694780	1144321	0	4757013
Vacant Square Feet	4936132	380206	0	47045	1348645	890541	0	7602569
Total Square Feet	6470054	569848	0	241392	3043426	2034862	0	12359582
Conversion to Building Square Footage								
Conversion to Building Square Footage								
Industrial (38%) underutilized	582890	72064	0					654955
Industrial (38%) vacant supply	1875730	144478	0					2020208
Industrial Planned lots	36300	0	0					36300
<i>Subtotal (vacant)</i>	<i>1912030</i>	<i>144478</i>	<i>0</i>					<i>2056508</i>
Total	2494920	216542	0					2711463
Commercial (32%) underutilized				62191	542330	1144321	0	1748842
Commercial (32%) vacant supply				15054	431567	890541	0	1337162
Commercial Planned lots				0	0	36629	0	36629
<i>Subtotal (vacant)</i>				<i>15054</i>	<i>431567</i>	<i>927170</i>	<i>0</i>	<i>1373791</i>
Total				77245	973896	2071491	0	3122633
Total Building Square Footage	2494920	216542	0	77245	973896	2071491	0	5834095

SILVERDALE
 URBAN GROWTH AREA
 Resulting Jobs
 Final

	Industrial Zones	Commercial Zones	TOTAL
Net Developable Acres	161.61	122.12	283.74
Building Area Estimates (Square Footage)	2,711,462.89	3,122,632.53	5,834,095.43
Vacancy Rate Adjustment (5%)	2,575,889.75	2,966,500.90	5,542,390.65
Employment Rate - Square Feet Per Employee	969	500	
Resulting Employment Capacity - Jobs	2658	5933	8592



KITSAP COUNTY DEPARTMENT OF COMMUNITY DEVELOPMENT

614 DIVISION STREET MS-36, PORT ORCHARD WASHINGTON 98366-4682 LOUISA GARBO, DIRECTOR
(360) 337-5777 HOME PAGE - www.kitsapgov.com/dcd/

DETERMINATION OF NONSIGNIFICANCE

Description of Proposal: A proposed Kitsap County Comprehensive Plan amendment to the Kingston Urban Village Center (UVC). The proposed non-project action includes the following amendments to the UVC zone:

1. Kingston Subarea Plan in Chapter 8 of the 2016 Kitsap County Comprehensive Plan.
 - Remove the mixed-use requirement in the Urban Village Center (UVC) zone
 - Clarify incentive-based parking programs
 - Remove completed Subarea Plan policies
2. Appendix B in Chapter 11 of the 2016 Kitsap County Comprehensive Plan.
 - Revise the allowed density in the UVC zone
3. Kitsap County Code.
 - Update Chapter 17.260 (Urban Village Center), Section 010.B. (Purpose) to revise the allowed density in the UVC zone
 - Update Chapter 17.420 (Density, Dimensions, and Design), Section 060 (Footnotes for tables) to revise the allowed density and remove the mixed-use requirement in the UVC zone

The Kitsap County Comprehensive Plan, including the Kingston Subarea Plan, and the Kitsap County Code is subject to continuous review and may be amended in future years. It is anticipated that additional amendments to the Kitsap County Code regarding the Urban Village Zone may be proposed in the future. Any future amendments to the Kitsap County Code will be consistent with and facilitate implementation of the goals and policies in the Kingston Subarea Plan and will be subject to applicable environmental review.

Staff contact: Liz Williams, Senior Planner, Kitsap DCD, Planning and Environmental Programs Div.

Lead Agency: Kitsap County

Location of proposal, including street address, if any: The proposed action covers the geographical jurisdiction of the Kingston UVC zone, an approximate 44.72 acres located adjacent to the Port of Kingston and the Washington State ferry terminal in the Kingston Urban Growth Area. Kitsap County.

The lead agency for this proposal has determined that it does not have a probable significant adverse impact on the environment. An environmental impact statement (EIS) is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information on file with the lead agency. This information is available to the public on request.

This DNS is issued under 197-11-340(2); the lead agency will not act on this proposal for 14 days from the date below. Comments must be submitted by August 7, 2018.

COMMENTS:

This is a nonproject action per WAC 197-11-704(2)(b). Adoption by ordinance of the amendment by the Kitsap County Board of Commissioners is expected in December of 2018.

Responsible Official: Scott Diener

SEPA Coordinator: Steve Heacock

Position/Title: SEPA Coordinator, Dept. of Community Development Phone: (360) 337-5777

Address: 619 Division Street, Port Orchard, WA 98366

DATE: 6/29/2018

Signature: 



Kitsap County Department of Community Development

Staff Report and Recommendation Annual Comprehensive Plan Amendment Process for 2018 Kingston Urban Village Center (UVC)

Report Date 6/25/18; Revised 10/1/2018

Hearing Date 7/17/18, 7/31/2018, and 10/29/2018

Amendment Type County-sponsored Amendment

Description The proposed amendment includes the following changes to:

- Kingston Subarea Plan in Chapter 8 of the 2016 Kitsap County Comprehensive Plan
 - Remove the mixed-use requirement in the Urban Village Center (UVC) zone
 - Clarify incentive-based parking programs
 - Remove completed Subarea Plan policies
- Appendix B in Chapter 11 of the 2016 Kitsap County Comprehensive Plan
 - Revise the allowed density in the UVC zone
- Kitsap County Code
 - Update Chapter 17.260 (Urban Village Center), Section 010.B. (Purpose) to revise the allowed density in the UVC zone
 - Update Chapter 17.420 (Density, Dimensions, and Design), Section 060 (Footnotes for tables) to revise the allowed density and remove the mixed-use requirement in the UVC zone

Geographic Area Affected [Unincorporated Kitsap County](#)

SEPA Determination of Non-Significance

Department Recommendation to Planning Commission Adopt as proposed

Planning Commission Recommendation Adopt as proposed

Department Recommendation to Board of County Commissioners Adopt as proposed

This report and recommendation are based on information available at the time of publication. If new relevant facts are discovered, this staff report will be revised, and the department recommendation may change.

Revision History

<u>No.</u>	<u>Date</u>	<u>Description</u>
1	10/1/2018	Incorporates the Planning Commission record and recommendation.

1. Background

The Kitsap County Comprehensive Plan describes the 20-year vision for unincorporated Kitsap County and how that vision will be achieved. The plan covers land use, economic development, environment, housing and human services, transportation, capital facilities and utilities as well as parks, recreation, and open space. The Comprehensive Plan is mandated by the Washington State Growth Management Act (GMA, RCW 36.70A).

A. Authority

The GMA mandates that Kitsap County's Comprehensive Plan and development regulations be reviewed and, if needed, revised at least every 8-years [RCW 90.70A.130(5)]. The most recent Kitsap County 8-year update concluded with the adoption of the 2016 Kitsap County Comprehensive Plan on June 27, 2016 by Ordinance 534-2016. The GMA also mandates that Kitsap County's Comprehensive Plan and development regulations be subject to continuing review and evaluation, allowing for annual amendments to the Comprehensive Plan and development regulations [RCW 36.70A.130(1)].

Kitsap County Code sets forth a process and criteria for making amendments to the Comprehensive Plan [KCC 21.08]. In making amendments, the County must consider:

- Whether the proposed amendment are consistent with and supports other plan elements and or development regulations, and if not, what additional amendments to the plan and/or development regulations will be required to maintain consistency;
- Whether the proposed amendment to the plan and/or regulation will more closely reflect the goals and policies of the Comprehensive Plan;
- Whether the proposed amendment is consistent with the Kitsap County-wide Planning Policies; and
- Whether the proposed amendment complies with the requirements of the GMA.

The final docket adopted by the Board of County Commissioners on April 4, 2018 (Resolution No. 064-2018) allows for consideration of this amendment during Kitsap County's annual Comprehensive Plan amendment process for 2018.

B. Proposed Amendment

The proposed amendment is provided in Attachment A and includes the following changes to:

- Kingston Subarea Plan in Chapter 8 of the 2016 Kitsap County Comprehensive Plan
 - Remove the mixed-use requirement in the Urban Village Center (UVC) zone
 - Clarify incentive-based parking programs
 - Remove completed Subarea Plan policies
- Appendix B in Chapter 11 of the 2016 Kitsap County Comprehensive Plan
 - Revise the allowed density in the UVC zone
- Kitsap County Code
 - Update Chapter 17.260 (Urban Village Center), Section 010.B. (Purpose) to revise the allowed density in the UVC zone
 - Update Chapter 17.420 (Density, Dimensions, and Design), Section 060 (Footnotes for tables) to revise the allowed density and remove the mixed-use requirement in the UVC zone

The proposed amendment was prepared at the request of the Kitsap County Board of Commissioners to “review the Comprehensive Plan policies and development regulations related to the Kingston Urban Village Center (UVC).” Staff from the Department of Community Development worked with the Kingston UVC Workgroup to identify priority barriers to achieving the existing vision and planned growth in the Kingston UVC zone. The proposed amendment is consistent with the Kingston UVC Workgroup’s preferred alternatives.

C. Geographic Description

The proposed amendment affects the Urban Village Center (UVC) zone. The UVC zone contains approximately 44.72 acres and is located in unincorporated Kitsap County adjacent to the Port of Kingston and the Washington State ferry terminal in the Kingston Urban Growth Area (see Attachment A - Map 1 - Vicinity).

2. Department Recommendation

Having analyzed the proposed amendment and other alternatives, if applicable, public comment received to date, and the Planning Commission’s recommendation, the Department recommends:

- Adoption of the amendment:
 - as proposed above
 - as described in Alternative ___ below
 - with revisions described below

- with conditions described below
- Deferral of the amendment to a future docket
- Denial of the amendment

A. Revisions

None.

B. Conditions

None.

C. Rational

- The Kitsap County Board of Commissioners requested a review of the Comprehensive Plan policies and development regulations related to the Kingston Urban Village Center (UVC) zone.
- The Department of Community Development worked with the Kingston UVC Workgroup to identify priority barriers to achieve the existing vision and planned growth in the UVC zone.
- The Kingston UVC Workgroup reached consensus on the proposed amendment.
- The proposed amendment addresses policy and regulatory barriers identified by the Kingston UVC Workgroup and supports the community's ability to achieve the existing vision for development and planned growth in the Kingston UVC zone.

3. Other Alternatives Considered

None.

4. Analysis

Amendments to the Comprehensive Plan must be consistent with the criteria outlined in Kitsap County Code (KCC) Chapter 21.08. Applicable criteria are analyzed below. A summary of the State Environmental Policy Act (SEPA) review of this amendment is located at the end of this section.

A. General Decision Criteria (KCC 21.08.070.A)

For each proposed amendment to the Comprehensive Plan, the Planning Commission in reaching its recommendation, and the Board of Commissioners in making its decision, shall develop findings and conclusions, which demonstrate:

1. *How circumstances related to the proposed amendment and/or the area in which the property affected by the proposed amendment is located have substantially changed since the adoption of the Comprehensive Plan or applicable development regulations;*

Staff Analysis: Policy and regulatory barriers were identified by the Kingston UVC Workgroup that diminish the ability to achieve the existing vision for development and planned growth in the UVC zone.

2. *How the assumptions upon which the Comprehensive Plan is based are no longer valid, or there is new information available which was not considered during the adoption of, or during the last annual amendment to, the Comprehensive Plan or development regulations; and*

Staff Analysis: Policy and regulatory barriers were identified by the Kingston UVC Workgroup that diminish the ability to achieve the existing vision for development and planned growth in the UVC zone.

3. *How the requested redesignation is in the public interest and the proposal is consistent with the Kitsap County Comprehensive Plan.*

Staff Analysis: Not applicable – this is not a redesignation request.

B. Additional Decision Criteria (KCC 21.08.070.B)

In addition to the findings and conclusions above, for each proposed text amendment, the Planning Commission in reaching its recommendation, and the Board of Commissioners in making its decision, shall develop findings and conclusions which consider:

1. *Whether the proposed amendment is consistent with and supports other plan elements and/or development regulations and, if not, what additional amendments to the plan and/or development regulations will be required to maintain consistency;*

Staff Analysis: The proposed amendment to the 2016 Comprehensive Plan and Kingston Subarea Plan is consistent with and support the following plan elements:

- Kingston Policy 47 encourages the review of mixed-use standards for the UVC zone and directs it to be amended as necessary.
- Kingston Policy 48 encourages increasing the density allowance in the UVC zone.
- Transportation Goal 5 encourages the use of incentive-based parking programs when opportunities for people to make choices among alternative modes of travel with an emphasis on moving people rather than vehicles and maximize opportunities for non-motorized travel.

The proposed amendment is needed to maintain consistency between the Kitsap County Code and 2016 Comprehensive Plan and Kingston Subarea Plan.

2. *Whether the proposed amendment to the plan and/or regulation(s) will more closely reflect the goals, objectives and policies of the Comprehensive Plan and reflect the local circumstances of the county;*

Staff Analysis: The proposed amendment addresses policy and regulatory barriers identified by the Kingston UVC Workgroup to achieve the vision for development and planned growth in the UVC zone. The proposed amendment to the 2016 Comprehensive Plan more closely aligns with the purpose statement of the Kingston UVC zone, which is:

- To foster a development pattern offering direct, convenient pedestrian, bicycle, and vehicular access between residences and businesses, in order to facilitate pedestrian and bicycle travel and reduce the number and length of automobile trips.
- To provide for a compatible mix of single-family, multifamily housing and neighborhood commercial businesses and services, with an emphasis on promoting multi-story structures with commercial uses generally located on the lower floors and residential housing generally located on upper floors.
- To promote a compact growth pattern to efficiently use developable land within UGAs, to enable the cost-effective extension of utilities, services and streets, to enable frequent and efficient transit service, and to help sustain neighborhood businesses.
- To foster the development of mixed use areas that are arranged, scaled and designed to be compatible with surrounding land.

The proposed amendment will also ensure consistency between the Kitsap County Code and 2016 Comprehensive Plan.

3. *Whether the proposed amendment is consistent with the Kitsap County-wide Planning Policy;*

Staff Analysis: The proposed amendment is consistent with the [Kitsap County-wide Planning Policies](#) (CPPs; adopted on 5/11/2015 by Kitsap County Ordinance 522-2015). The proposed amendment is consistent with Element B, F, H of the CPPs, including:

- Higher density residential development within walking or bicycling distance of jobs, transit, schools and parks;
- Promotion of in-fill or redevelopment of existing urban areas;
- Encourage development that reflects unique local qualities and provides an economic benefit to the community;

- The County shall actively seek opportunities to share facilities, expertise, and transportation resources, such as multiple use park & ride/parking lots or shared traffic signal maintenance responsibility.
 - The County shall provide both infra-structure and policy incentives to increase the use of non-single occupancy vehicle modes of travel.
4. *Whether the proposed amendment complies with the requirements of GMA, state and local laws and other applicable inter-jurisdictional policies or agreements; and*

Staff Analysis: The proposed amendment will ensure implementation of the community's vision for development and planned growth in the Kingston UVC zone and is consistent with the requirements of GMA, state and local laws and other applicable inter-jurisdictional policies and agreements.

5. *An explanation of why language should be added to the Comprehensive Plan or why existing language should be modified or deleted.*

Staff Analysis: The proposed amendment should be added to the 2016 Comprehensive Plan and Kingston Subarea Plan for the following reasons:

- Kingston Policy 47 encourages the review of mixed use standards for the UVC zone and directs it to be amended as necessary.
- Kingston Policy 48 encourages an increase in the density allowance for the UVC zone.
- Transportation Goal 5 encourages the use of parking incentives to provide opportunities for people to make choices among alternative modes of travel with an emphasis on moving people rather than vehicles and maximize opportunities for non-motorized travel.
- The proposed amendment includes updates to the Kitsap County Code to maintain consistency with the 2016 Comprehensive Plan and Kingston Subarea Plan.

C. State Environmental Policy Act (SEPA)

The Kitsap County SEPA official issued a SEPA threshold determination of non-significance (DNS; Attachment B1) for this amendment after having reviewed the SEPA environmental checklists prepared for this amendment (Attachment B2) and all of the other proposed Comprehensive Plan amendments. The SEPA official's review found that this amendment is not related to or dependent on any of the other amendments and therefore an independent SEPA threshold determination was made regarding this amendment.

Notice of this SEPA threshold determination was:

- Filed with the Washington State Department of Ecology [SEPA Register](#);
- Published in the Kitsap Sun newspaper (6/29/2018);

- Mailed to property owners within 800 feet of the Kingston Urban Village Center (UVC) zone; and
- Integrated with other public announcements described in Section 5 below.

The SEPA threshold determination and environmental checklist was also distributed to agencies with jurisdiction, the Department of Ecology, affected tribes, and each local agency or political subdivision whose public services would be changed as a result of implementation of the proposal.

D. Public Comment

Nine public comments generally supportive of the amendment were received. One comment questioned an increase in maximum density and felt it may alter the small-town character and be inconsistent with the Kingston design standards. Public comments and staff responses are provided in Attachment D1.

Staff Analysis: The Kingston Subarea Plan includes a policy to increase the density allowed in the UVC zone. In addition, building height, site, and community character will continue to be governed by the Kingston design standards.

E. Planning Commission Recommendation

The Planning Commission concurred with the staff recommendation to adopt the amendment as proposed.

Staff Analysis: None.

5. Public Involvement and Outreach

Kitsap County's public involvement and outreach in support of this amendment has exceeded the requirements of the Growth Management Act (RCW 36.70A) and Kitsap County Code (KCC 21.08).

Public involvement and outreach in support of this amendment has included the following:

- An [Online Open House](#) with information about previous, current, and upcoming phases of the 2018 amendment process.
- A public comment period (11/27/2017 – 12/15/2017) and a public hearing by the Kitsap County Board of Commissioners (12/11/2017) while setting the initial docket of amendments. Based on public comments, the Board of Commissioners added a review of affordable housing policies (i.e. the subject of this amendment) to the docket of amendments. Notifications and announcements regarding this comment period and public hearing included the following:
 - Legal notice published in the Kitsap Sun newspaper (11/27/2017);

- Broadcast announcements via email, text message, Facebook.com, Twitter.com, and Nextdoor.com; and
- Formal letters to Tribes with usual and accustomed area in Kitsap County.
- Legal notice announcing the docket of amendments was published in the Kitsap Sun newspaper (1/8/2018).
- A public comment period (6/28/2018 – 8/7/2018) and public hearings by the Kitsap County Planning Commission (7/17/2018 and 7/31/2018) regarding the proposed amendment, staff report, and SEPA determination. Notifications and announcements regarding this comment period and public hearing included the following:
 - Legal notice published in the Kitsap Sun newspaper;
 - Broadcast announcements via email, text message, Facebook.com, Twitter.com, and Nextdoor.com;
 - Notice signs posted on site-specific amendment properties;
 - Notices mailed to property owners near site-specific amendments and other geographically specific amendments; and
 - Formal letters to Tribes with usual and accustomed area in Kitsap County.
- Open house meetings were held on 7/10/2018 in Kingston, 7/11/2018 in Port Orchard, and 7/12/2018 in Silverdale.
- Presentations to various Kitsap County advisory groups and community groups.
- The proposed amendment was developed by the Kingston UVC Workgroup, an appointed citizen group supported by Kitsap County planning staff.

Comment Period and Public Hearing

A new comment period regarding the proposed amendment and this staff report will run through Wednesday, October 31, 2018.

- During this public comment period, the public may learn more about this and other amendments by:
 - Visiting an [Online Open House](http://tinyurl.com/kitsap2018cpa) (<http://tinyurl.com/kitsap2018cpa>);
 - Attending applicable Board of County Commissioner meetings; or
 - Contacting the staff listed in Section 6 below.
- To be included in the official record, written comments must be submitted to the Department of Community Development by 11:59 PM on Wednesday, October 31, 2018 using one of the following methods:
 - Entered [online via computer or mobile device](#);
 - Emailed to CompPlan@co.kitsap.wa.us;
 - Mailed to 614 Division St - MS36, Port Orchard, WA 98366;
 - Dropped off at the Permit Center at 619 Division St, Port Orchard; or
 - Dropped off at one of the open houses listed above.
- Oral and written testimony may also be made to the Board of County Commissioners during a public hearings on October 29, 2018 in the Commissioner's Chambers on the 3rd Floor of the Kitsap County Administration Building (619 Division St, Port Orchard).

- Notifications and announcements regarding this comment period and public hearing include the following:
 - Legal notice published in the Kitsap Sun newspaper;
 - Broadcast announcements via email, text message, Facebook.com, Twitter.com, and Nextdoor.com;
 - Notice signs posted on site-specific amendment properties;
 - Notices mailed to property owners near site-specific amendments and other geographically specific amendments; and
 - Formal letters to Tribes with usual and accustomed area in Kitsap County.

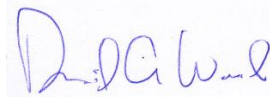
6. Staff Contact

Report prepared by:



Liz Williams, Planner
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LWilliam@co.kitsap.wa.us

Report approved by:



Dave Ward, Manager
Planning and Environmental Programs
Department of Community Development



Peter Best, Senior Planner
(360) 337-5777
PBest@co.kitsap.wa.us

7. Attachments

- A. Proposed Amendment
- B. State Environmental Policy Act (SEPA)
 - 1. SEPA Determination
 - 2. SEPA Checklist
- C. Maps
 - 1. Vicinity
 - 2. Aerial Photo
 - 3A. Critical Areas
 - 3B. Critical Aquifer Recharge Areas
 - 4. Current Land Use Designation
 - 5. Current Zoning Classification
 - Map Legends
- D. Supplemental Materials
 - 1. Public Comment and Staff Response

SEPA ENVIRONMENTAL CHECKLIST

Purpose of checklist:

Governmental agencies use this checklist to help determine whether the environmental impacts of your proposal are significant. This information is also helpful to determine if available avoidance, minimization or compensatory mitigation measures will address the probable significant impacts or if an environmental impact statement will be prepared to further analyze the proposal.

Instructions for applicants:

This environmental checklist asks you to describe some basic information about your proposal. Please answer each question accurately and carefully, to the best of your knowledge. You may need to consult with an agency specialist or private consultant for some questions. You may use "not applicable" or "does not apply" only when you can explain why it does not apply and not when the answer is unknown. You may also attach or incorporate by reference additional studies reports. Complete and accurate answers to these questions often avoid delays with the SEPA process as well as later in the decision-making process.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will help describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

Instructions for Lead Agencies:

Please adjust the format of this template as needed. Additional information may be necessary to evaluate the existing environment, all interrelated aspects of the proposal and an analysis of adverse impacts. The checklist is considered the first but not necessarily the only source of information needed to make an adequate threshold determination. Once a threshold determination is made, the lead agency is responsible for the completeness and accuracy of the checklist and other supporting documents.

Use of checklist for nonproject proposals: [\[help\]](#)

For nonproject proposals (such as ordinances, regulations, plans and programs), complete the applicable parts of sections A and B plus the [SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS \(part D\)](#). Please completely answer all questions that apply and note that the words "project," "applicant," and "property or site" should be read as "proposal," "proponent," and "affected geographic area," respectively. The lead agency may exclude (for non-projects) questions in Part B - Environmental Elements –that do not contribute meaningfully to the analysis of the proposal.

A. Background [\[help\]](#)

1. Name of proposed project, if applicable: [\[help\]](#)

2018 Kitsap County Comprehensive Plan Kingston Urban Village Center Amendment (a non-project action)

2. Name of applicant: [\[help\]](#)

Kitsap County Department of Community Development

3. Address and phone number of applicant and contact person: [\[help\]](#)

Liz Williams, Planner

lwilliam@co.kitsap.wa.us

Peter Best, Senior Planner

PBest@co.kitsap.wa.us

360-337-5777

614 Division St – MS36

Port Orchard, WA 98366

4. Date checklist prepared: [\[help\]](#)

June 20, 2018

5. Agency requesting checklist: [\[help\]](#)

Kitsap County

6. Proposed timing or schedule (including phasing, if applicable): [\[help\]](#)

Amendment is expected to be adopted in December 2018.

7. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain. [\[help\]](#)

The Kitsap County Comprehensive Plan, including the Kingston Subarea Plan, and the Kitsap County Code is subject to continuous review and may be amended in future years. It is anticipated that additional amendments to the Kitsap County Code regarding the Urban Village Zone may be proposed in the future. Any future amendments to the Kitsap County Code will be consistent with and facilitate implementation of the goals and policies in the Kingston Subarea Plan and will be subject to applicable environmental review.

8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal. [\[help\]](#)

None.

9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain. [\[help\]](#)

Not applicable for this non-project action.

10. List any government approvals or permits that will be needed for your proposal, if known. [\[help\]](#)

This amendment will need to be adopted by Kitsap County ordinance.

11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. (Lead agencies may modify this form to include additional specific information on project description.) [\[help\]](#)

The proposed non-project action includes the following amendments to:

- Kingston Subarea Plan in Chapter 8 of the 2016 Kitsap County Comprehensive Plan
 - Remove the mixed-use requirement in the Urban Village Center (UVC) zone
 - Clarify incentive-based parking programs
 - Remove completed Subarea Plan policies
- Appendix B in Chapter 11 of the 2016 Kitsap County Comprehensive Plan
 - Revise the allowed density in the UVC zone
- Kitsap County Code
 - Update Chapter 17.260 (Urban Village Center), Section 010.B. (Purpose) to revise the allowed density in the UVC zone
 - Update Chapter 17.420 (Density, Dimensions, and Design), Section 060 (Footnotes for tables) to revise the allowed density and remove the mixed-use requirement in the UVC zone

12. Location of the proposal. Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township, and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit applications related to this checklist. [\[help\]](#)

This is a non-project action that affects the Urban Village Center (UVC) zone in Kingston, WA. The UVC zone contains approximately 44.72 acres and is located in unincorporated Kitsap County adjacent to the Port of Kingston and the Washington State ferry terminal in the Kingston Urban Growth Area (see Map 1 – Vicinity below).



B. Environmental Elements [\[help\]](#)**1. Earth** [\[help\]](#)

a. General description of the site: [\[help\]](#)

(circle one): Flat, rolling, hilly, steep slopes, mountainous, other _____

b. What is the steepest slope on the site (approximate percent slope)? [\[help\]](#)

Not applicable for this non-project action

c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any agricultural land of long-term commercial significance and whether the proposal results in removing any of these soils. [\[help\]](#)

Not applicable for this non-project action

d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe. [\[help\]](#)

None known.

e. Describe the purpose, type, total area, and approximate quantities and total affected area of any filling, excavation, and grading proposed. Indicate source of fill. [\[help\]](#)

Not applicable for this non-project action

f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe. [\[help\]](#)

Not applicable for this non-project action

g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)? [\[help\]](#)

Not applicable for this non-project action

h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

2. Air [\[help\]](#)

- a. What types of emissions to the air would result from the proposal during construction, operation, and maintenance when the project is completed? If any, generally describe and give approximate quantities if known. [\[help\]](#)

Not applicable for this non-project action

- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe. [\[help\]](#)

Not applicable for this non-project action

- c. Proposed measures to reduce or control emissions or other impacts to air, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

3. Water [\[help\]](#)

- a. Surface Water:

- 1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into. [\[help\]](#)

Marine waters include Appletree Cove and Puget Sound. A non-fish bearing stream is piped under the UVC zone. No wetlands are currently mapped by Kitsap County in the UVC zone.

- 2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans. [\[help\]](#)

Not applicable for this non-project action

- 3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material. [\[help\]](#)

Not applicable for this non-project action

- 4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known. [\[help\]](#)

Not applicable for this non-project action

- 5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan. [\[help\]](#)

No

- 6) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge. [\[help\]](#)

Not applicable for this non-project action

b. Ground Water:

- 1) Will groundwater be withdrawn from a well for drinking water or other purposes? If so, give a general description of the well, proposed uses and approximate quantities withdrawn from the well. Will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known. [\[help\]](#)

Not applicable for this non-project action

- 2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals. . . ; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve. [\[help\]](#)

Not applicable for this non-project action

c. Water runoff (including stormwater):

- 1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe. [\[help\]](#)

Not applicable for this non-project action

- 2) Could waste materials enter ground or surface waters? If so, generally describe. [\[help\]](#)

Not applicable for this non-project action

- 3) Does the proposal alter or otherwise affect drainage patterns in the vicinity of the site? If so, describe. [\[help\]](#)

Not applicable for this non-project action

d. Proposed measures to reduce or control surface, ground, and runoff water, and drainage pattern impacts, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

4. Plants [\[help\]](#)

- a. Check the types of vegetation found on the site: [\[help\]](#)

various deciduous tree: alder, maple, aspen, other
 various evergreen tree: fir, cedar, pine, other
 various shrubs
 x grass
 pasture
 crop or grain
 Orchards, vineyards or other permanent crops.
 wet soil plants: cattail, buttercup, bullrush, skunk cabbage, other
 water plants: water lily, eelgrass, milfoil, other
 other types of vegetation

- b. What kind and amount of vegetation will be removed or altered? [\[help\]](#)

Not applicable for this non-project action

- c. List threatened and endangered species known to be on or near the site. [\[help\]](#)

None known.

- d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

- e. List all noxious weeds and invasive species known to be on or near the site. [\[help\]](#)

None known.

5. Animals [\[help\]](#)

- a. List any birds and other animals which have been observed on or near the site or are known to be on or near the site. [\[help\]](#)

Examples include:

birds: hawk, heron, eagle, songbirds, other:
mammals: deer, bear, elk, beaver, other:
fish: bass, salmon, trout, herring, shellfish, other _____

Unknown.

- b. List any threatened and endangered species known to be on or near the site. [\[help\]](#)

Chinook Salmon, Steelhead Trout, Southern Resident Killer Whale.

- c. Is the site part of a migration route? If so, explain. [\[help\]](#)

Not applicable for this non-project action

- d. Proposed measures to preserve or enhance wildlife, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

- e. List any invasive animal species known to be on or near the site. [\[help\]](#)

None known.

6. Energy and Natural Resources [\[help\]](#)

- a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc. [\[help\]](#)

Not applicable for this non-project action

- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe. [\[help\]](#)

Not applicable for this non-project action

- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any: [\[help\]](#)

Not applicable for this non-project action

7. Environmental Health [\[help\]](#)

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste, that could occur as a result of this proposal? If so, describe. [\[help\]](#)

Not applicable for this non-project action

- 1) Describe any known or possible contamination at the site from present or past uses. [\[help\]](#)

None known

- 2) Describe existing hazardous chemicals/conditions that might affect project development and design. This includes underground hazardous liquid and gas transmission pipelines located within the project area and in the vicinity. [\[help\]](#)

Not applicable for this non-project action

- 3) Describe any toxic or hazardous chemicals that might be stored, used, or produced during the project's development or construction, or at any time during the operating life of the project. [\[help\]](#)

Not applicable for this non-project action

- 4) Describe special emergency services that might be required. [\[help\]](#)

Not applicable for this non-project action

- 5) Proposed measures to reduce or control environmental health hazards, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

b. Noise [\[help\]](#)

- 1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)? [\[help\]](#)

None

- 2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site. [\[help\]](#)

Not applicable for this non-project action. Future development may generate noise from traffic, construction, and operations.

- 3) Proposed measures to reduce or control noise impacts, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

8. Land and Shoreline Use [\[help\]](#)

- a. What is the current use of the site and adjacent properties? Will the proposal affect current land uses on nearby or adjacent properties? If so, describe. [\[help\]](#)

The UVC zone includes a mix of commercial and residential uses. The proposed amendment will not affect current land uses in or adjacent to the UVC zone.

- b. Has the project site been used as working farmlands or working forest lands? If so, describe. How much agricultural or forest land of long-term commercial significance will be converted to other uses as a result of the proposal, if any? If resource lands have not been designated, how many acres in farmland or forest land tax status will be converted to nonfarm or nonforest use? [\[help\]](#)

No

- 1) Will the proposal affect or be affected by surrounding working farm or forest land normal business operations, such as oversize equipment access, the application of pesticides, tilling, and harvesting? If so, how: [\[help\]](#)

Not applicable for this non-project action

- c. Describe any structures on the site. [\[help\]](#)

The UVC zone contains a mix of one, two and three story residential and commercial structures.

- d. Will any structures be demolished? If so, what? [\[help\]](#)

This non-project action will not demolish existing structures but is expected to stimulate development within the UVC zone consistent with the existing vision and planned growth for the area in the Kitsap County Comprehensive Plan, including the Kinston Subarea Plan. Existing structures may be demolished in the future when parcels are redeveloped, which would be subject to applicable permitting and environmental review.

- e. What is the current zoning classification of the site? [\[help\]](#)

The site has an Urban Village Center (UVC) zoning classification. This non-project action will not change the zoning classification. The general purpose of the zone is:

1. To foster a development pattern offering direct, convenient pedestrian, bicycle, and vehicular access between residences and businesses, in order to facilitate pedestrian and bicycle travel and reduce the number and length of automobile trips.
2. To provide for a compatible mix of single-family, multifamily housing and neighborhood commercial businesses and services, with an emphasis on promoting multi-story structures with commercial uses generally located on the lower floors and residential housing generally located on upper floors.
3. To promote a compact growth pattern to efficiently use developable land within UGAs, to enable the cost-effective extension of utilities, services and streets, to enable frequent and efficient transit service, and to help sustain neighborhood businesses.

4. To foster the development of mixed use areas that are arranged, scaled and designed to be compatible with surrounding land.

f. What is the current comprehensive plan designation of the site? [\[help\]](#)

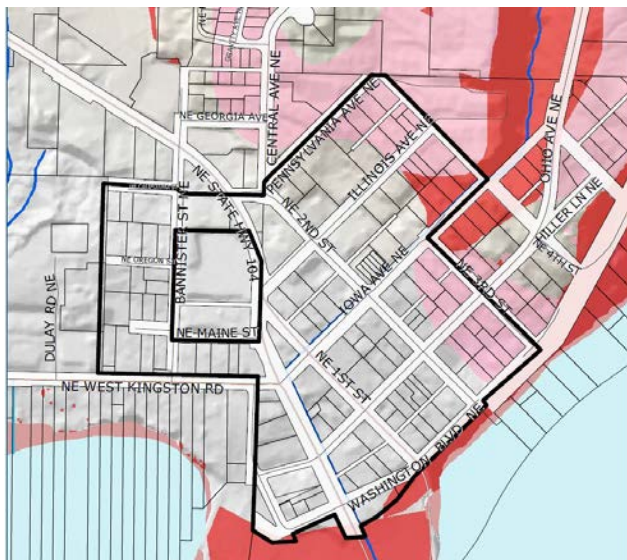
The site has an Urban Low-Intensity Commercial comprehensive plan land use designation. This amendment will not change the land use designation.

g. If applicable, what is the current shoreline master program designation of the site? [\[help\]](#)

The UVC zone does not extend down to the ordinary high water mark, but small portions of the UVC zone are covered by High Intensity, Shoreline Residential, and Urban Conservancy shoreline designations.

h. Has any part of the site been classified as a critical area by the city or county? If so, specify. [\[help\]](#)

The northeast portion of the UVC zone contains high (red areas in map) and moderate (pink areas in map) geologically hazardous areas.



i. Approximately how many people would reside or work in the completed project? [\[help\]](#)

This non-project action is expected to stimulate development within the UVC zone consistent with the existing vision and planned growth for the area in the Kitsap County Comprehensive Plan, including the Kingston Subarea Plan.

j. Approximately how many people would the completed project displace? [\[help\]](#)

This non-project action is expected to increase residential and commercial capacity within the UVC zone and is not expected to cause general displacement. Limited or temporary displacement may occur in the future when parcels are redeveloped.

k. Proposed measures to avoid or reduce displacement impacts, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any: [\[help\]](#)

Not applicable for this non-project action. The proposed amendment addresses policy and regulatory barriers to achieving the existing vision for development and planned growth for the Kingston UVC zone in the Kitsap County Comprehensive Plan, including the Kingston Subarea Plan.

m. Proposed measures to reduce or control impacts to agricultural and forest lands of long-term commercial significance, if any: [\[help\]](#)

Not applicable for this non-project action

9. **Housing** [\[help\]](#)

a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing. [\[help\]](#)

This non-project action is expected to stimulate new housing development within the UVC zone consistent with the planned growth for the area envisioned in the Kitsap County Comprehensive plan, including the Kingston Subarea Plan.

b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing. [\[help\]](#)

This non-project action is expected to stimulate new housing development within the UVC zone consistent with the planned growth for the area envisioned in the Kitsap County Comprehensive plan, including the Kingston Subarea Plan.

c. Proposed measures to reduce or control housing impacts, if any: [\[help\]](#)

Not applicable for this non-project action. This non-project action is expected to stimulate new housing development within UVC zone consistent with the planned growth area envisioned in the Kitsap County Comprehensive Plan, including the Kingston Subarea Plan. Any future development will be subject to applicable permitting and environmental review.

10. **Aesthetics** [\[help\]](#)

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed? [\[help\]](#)

Not applicable for this non-project action. This non-project action will not change existing requirements in Kitsap County Code regarding building height and exterior materials.

- b. What views in the immediate vicinity would be altered or obstructed? [\[help\]](#)

Not applicable for this non-project action. This non-project action will not change existing requirements in Kitsap County Code regarding building height and exterior materials.

- b. Proposed measures to reduce or control aesthetic impacts, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

11. Light and Glare [\[help\]](#)

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur? [\[help\]](#)

Not applicable for this non-project action. This non-project action will not change existing requirements in Kitsap County regarding light and glare.

- b. Could light or glare from the finished project be a safety hazard or interfere with views? [\[help\]](#)

Not applicable for this non-project action. This non-project action will not change existing requirements in Kitsap County regarding light and glare.

- c. What existing off-site sources of light or glare may affect your proposal? [\[help\]](#)

Not applicable for this non-project action.

- d. Proposed measures to reduce or control light and glare impacts, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

12. Recreation [\[help\]](#)

- a. What designated and informal recreational opportunities are in the immediate vicinity? [\[help\]](#)

The Village Green Park and Community Center, Kola Kole Park, Washington Boulevard Park, and the Port of Kingston (park, marina, boat ramp) are in or adjacent to the UVC zone.

- b. Would the proposed project displace any existing recreational uses? If so, describe. [\[help\]](#)

Not applicable for this non-project action. This non-project action will not affect existing recreational opportunities in the UVC zone or the vicinity.

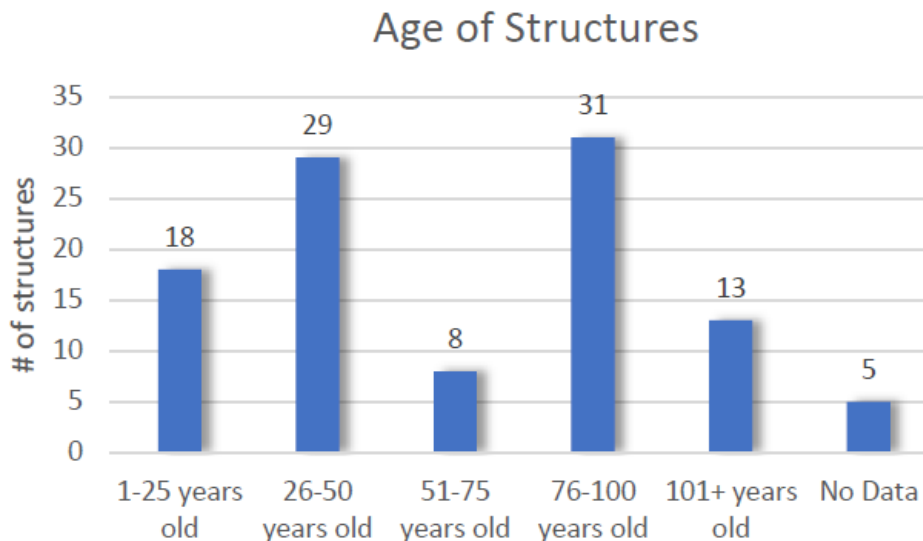
- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any: [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

13. Historic and cultural preservation [\[help\]](#)

- a. Are there any buildings, structures, or sites, located on or near the site that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers ? If so, specifically describe. [\[help\]](#)

Unknown. A distribution of existing structure age is shown below.



- b. Are there any landmarks, features, or other evidence of Indian or historic use or occupation? This may include human burials or old cemeteries. Are there any material evidence, artifacts, or areas of cultural importance on or near the site? Please list any professional studies conducted at the site to identify such resources. [\[help\]](#)

Unknown

- c. Describe the methods used to assess the potential impacts to cultural and historic resources on or near the project site. Examples include consultation with tribes and the department of archeology and historic preservation, archaeological surveys, historic maps, GIS data, etc. [\[help\]](#)

Not applicable for this non-project action

- d. Proposed measures to avoid, minimize, or compensate for loss, changes to, and disturbance to resources. Please include plans for the above and any permits that may be required. [\[help\]](#)

Not applicable for this non-project action. Any future development will be subject to applicable permitting and environmental review.

14. Transportation [\[help\]](#)

- a. Identify public streets and highways serving the site or affected geographic area and describe proposed access to the existing street system. Show on site plans, if any. [\[help\]](#)

The entire UVC zone is accessed by public streets and highways.

- b. Is the site or affected geographic area currently served by public transit? If so, generally describe. If not, what is the approximate distance to the nearest transit stop? [\[help\]](#)

The UVC zone is located adjacent to the Washington State ferry terminal. Kitsap Transit provides routed and non-routed bus service to the UVC zone and is expected to begin passenger-only fast ferry service later in 2018 from the adjacent Port of Kingston to downtown Seattle.

- c. How many additional parking spaces would the completed project or non-project proposal have? How many would the project or proposal eliminate? [\[help\]](#)

Not applicable for this non-project action. This non-project action encourages the development of parking incentives, but does not change existing requirements in Kitsap County Code regarding parking. Any future proposals to modify parking requirements in Kitsap County Code will be subject to applicable environmental review.

- d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities, not including driveways? If so, generally describe (indicate whether public or private). [\[help\]](#)

This non-project action is expected to stimulate development within the UVC zone consistent with the assumptions used for the Kitsap County Capital Facilities Plan and will not require new improvements beyond those already planned or as would be required through the applicable permitting and environmental review of any development projects.

- e. Will the project or proposal use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe. [\[help\]](#)

The UVC zone is located adjacent to a Washington State ferry terminal and the future terminal for the Kitsap Transit passenger-only fast ferry service.

- f. How many vehicular trips per day would be generated by the completed project or proposal? If known, indicate when peak volumes would occur and what percentage of the volume would

be trucks (such as commercial and nonpassenger vehicles). What data or transportation models were used to make these estimates? [\[help\]](#)

This non-project action is expected to stimulate development within the UVC zone consistent with the assumptions used for the Kitsap County Capital Facilities Plan and will not require new improvements beyond those already planned or as would be required through the applicable permitting and environmental review of any future development projects.

g. Will the proposal interfere with, affect or be affected by the movement of agricultural and forest products on roads or streets in the area? If so, generally describe. [\[help\]](#)

Not applicable for this non-project action

h. Proposed measures to reduce or control transportation impacts, if any: [\[help\]](#)

Not applicable for this non-project action. This non-project action is expected to stimulate development within the UVC zone consistent with the assumptions used for the Kitsap County Capital Facilities Plan and will not require new improvements beyond those already planned or as would be required through the applicable permitting and environmental review of any future development projects.

15. **Public Services** [\[help\]](#)

a. Would the project result in an increased need for public services (for example: fire protection, police protection, public transit, health care, schools, other)? If so, generally describe. [\[help\]](#)

The proposed amendment to the Comprehensive Plan policy related to parking incentives has the potential to increase demand on public transit over time. This non-project action is expected to stimulate development within the UVC zone consistent with the assumptions used for the Kitsap County Capital Facilities Plan and will not increase demand on other public services.

b. Proposed measures to reduce or control direct impacts on public services, if any. [\[help\]](#)

Not applicable for this non-project action. This non-project action is expected to stimulate development within the UVC zone consistent with the assumptions used for the Kitsap County Capital Facilities Plan and will not require new improvements beyond those already planned or as would be required through the applicable permitting and environmental review of future development projects.

16. **Utilities** [\[help\]](#)

a. Circle utilities currently available at the site: [\[help\]](#)
 electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system,
 other _____

- b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed. [\[help\]](#)

Not applicable for this non-project action. This non-project action is expected to stimulate development within the UVC zone consistent with the assumptions used for the Kitsap County Capital Facilities Plan and will not require new improvements beyond those already planned or as would be required through the applicable permitting and environmental review of future development projects.

C. Signature [\[help\]](#)

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision.

Signature:  _____

Name of signee: Peter Best

Position and Agency/Organization: Senior Planner, Kitsap County Department of Community Development

Date Submitted: 6/20/18

D. Supplemental sheet for nonproject actions [\[help\]](#)

(IT IS NOT NECESSARY to use this sheet for project actions)

Because these questions are very general, it may be helpful to read them in conjunction with the list of the elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage, or release of toxic or hazardous substances; or production of noise?

This non-project action is not likely to increase discharge to water; emissions to air; production storage, or release of toxic or hazardous substances; or production of noise.

Proposed measures to avoid or reduce such increases are:

None. Any future development will be subject to applicable permitting and environmental review.

2. How would the proposal be likely to affect plants, animals, fish, or marine life?

This non-project action is not likely to affect plants, animals, fish, or marine life.

Proposed measures to protect or conserve plants, animals, fish, or marine life are:

None. Any future development will be subject to applicable permitting and environmental review.

3. How would the proposal be likely to deplete energy or natural resources?

This non-project action is not likely to deplete energy or natural resources.

Proposed measures to protect or conserve energy and natural resources are:

None. Any future development will be subject to applicable permitting and environmental review.

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection; such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, floodplains, or prime farmlands?

This non-project action is not likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection.

Proposed measures to protect such resources or to avoid or reduce impacts are:

None. Any future development will be subject to applicable permitting and environmental review.

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans?

This non-project action is not likely to affect land and shoreline use.

Proposed measures to avoid or reduce shoreline and land use impacts are:

None. Any future development will be subject to applicable permitting and environmental review.

6. How would the proposal be likely to increase demands on transportation or public services and utilities?

The proposed amendment to the Comprehensive Plan policy related to parking incentives has the potential to increase demand on public transit over time. The combination of removing the mixed-use requirement and revising allowed density is expected to stimulate development within the UVC zone consistent with the assumptions used for the County's 20-year land capacity analysis and Capital Facilities Plan for the Kingston Urban Growth Area. Based on the proposed amendment, increased demand on other public services are not anticipated.

Proposed measures to reduce or respond to such demand(s) are:

None. Any future development will be subject to applicable permitting and environmental review.

7. Identify, if possible, whether the proposal may conflict with local, state, or federal laws or requirements for the protection of the environment.

This non-project action does not conflict with local, state, or federal laws or requirements for the protection of the environment.



Apex Airpark is located in Kitsap County, 2 miles northwest of Silverdale. Apex Airpark airport has 55 based aircraft, including 54 single-engine and 1 helicopter. The latest available data indicate that Apex Airpark had a total of 19,600 annual operations. Runway 17-35, the Airport's only runway, is 2,500 feet long, 28 feet wide, has an asphalt surface, and is equipped with low intensity runway lights.

NOTE: Data on this page comes from the WA Airport Information System Database (AIS).

AIRPORT CHARACTERISTICS

Location	Service Classification	Approach
Legislative Dist:	Federal:	Airport Elevation:
Associated City: Silverdale	State:	Approach Category:
County:		
Organizational Structure	Runway(s)	Type of Airport
Ownership Type:	Number: 0	FAA:
Owner:	Type(s):	Description:

AIRPORT ACTIVITY

Activities	Based Aircraft		Cargo
	Based	Transient	
AIS Last Updated: 12/14/2011			
Agricultural Spraying	<input type="checkbox"/>	<input type="checkbox"/>	Jet 0
Air Ambulance	<input type="checkbox"/>	<input type="checkbox"/>	Multi-Engine 3
Medical Transport	<input type="checkbox"/>	<input type="checkbox"/>	Single-Engine 70
Airplane Parts Manufacturing	<input type="checkbox"/>	<input type="checkbox"/>	Rotor Based 0
Aerial Surveying	<input type="checkbox"/>	<input type="checkbox"/>	Glider 1
Wildland Firefighting	<input type="checkbox"/>	<input type="checkbox"/>	Military 0
Skydiving/Parachute Drops	<input type="checkbox"/>	<input type="checkbox"/>	Ultralight 2
Aerial Tours	<input type="checkbox"/>	<input type="checkbox"/>	Seaplane 0
Civil Air Patrol	<input type="checkbox"/>	<input type="checkbox"/>	Total 76
Cargo Activity	<input type="checkbox"/>		Fixed Based Operators
Flight Training	<input type="checkbox"/>		AIS Last Updated:
Commercial Carrier Activity	<input type="checkbox"/>		No. of FBOs
			Number of Cargo Carriers -
			Total Cargo Volume (Tons) -
			Ground Transportation
			AIS Last Updated: 12/15/2011
			Bus Service <input type="checkbox"/>
			Taxi Service <input checked="" type="checkbox"/>
			Marine Service <input type="checkbox"/>
			Rail Service <input type="checkbox"/>
			Shuttle Service <input type="checkbox"/>
			Limo Town Car <input type="checkbox"/>
			Other Ground Transportation <input checked="" type="checkbox"/>

Comparison by State Classification Take Offs and Landings (Operations)

Airport	Classification		Operations
	Low	High	
Based Aircraft	76	-	-
Operations	14,800	-	-
Commercial Enplanements*			
2010			-
2009			-
2008			-
*Enplanements are passengers boarding a commercial aircraft. Does not include disembarking passengers.			
Fuel Service			
80 LL	<input type="checkbox"/>		
100 LL	<input type="checkbox"/>		
MoGas	<input type="checkbox"/>		
Jet A	<input type="checkbox"/>		
Helicopter Fuel	<input type="checkbox"/>		

	2005	2006	2007	2008	2009	2010
Military Itinerant	0	0	0	0	0	0
Military Local	0	0	0	0	0	0
Commercial Air Taxi	0	0	0	6844	0	0
Commercial Air Carrier	0	0	0	0	0	0
General Itinerant	0	0	0	25000	7400	7400
General Local	0	0	0	17288	7400	7400

Airport Businesses and Visitors

Economic and Fiscal impacts calculated for each airport start with activity that can be directly associated with the airport, namely the businesses operating at the airport and the visitors traveling through the airport. For economic impacts, multiplier effects are estimated from this initial activity as portions of wages and business and visitor spending are re-spent within the local economy. Impacts of airport businesses are analyzed within the defined economic impact region, visitor spending is analyzed statewide, since once visitors land they may spend their dollars throughout the state. For fiscal impacts, taxes paid to various types of jurisdictions from this business and visitor activity are estimated.



NOTE: All impacts are shown in 2010 dollars.

ECONOMIC IMPACTS

AIRPORT BUSINESSES

- Counties in Impact Region:** Kitsap
- Direct Jobs:** Estimated jobs on the airport footprint (excluding businesses that are not aviation-dependent).
- Direct Labor Income:** Estimated income paid to the Direct Jobs located on the airport footprint.
- Direct Output:** Estimated value of original business activity that remains in the economic impact region (some business activity will be exported outside of the region).
- Indirect/Induced Impacts:** Increases in regional impacts from the local re-spending of direct dollars.
- Total Impacts:** The sum of Direct, Indirect, and Induced Impacts, for a total regional impact.

Estimated Regional Impact from Airport Businesses

Estimated Economic Impact	Direct	Indirect/Induced	Total Impact
Jobs	6	3	9
Labor Income	\$ 314,000	\$ 112,000	\$ 426,000
Output	\$ 576,000	\$ 314,000	\$ 890,000

VISITOR SPENDING

- Impact Region:** Washington State (once visitors land they may spend their money throughout the state).
- Total Visitor Spending:** Estimated total annual spending by visitors traveling through this airport.
- Direct Jobs:** Estimated jobs supported by the total estimated visitor expenditures.
- Direct Labor Income:** Estimated income paid to the Direct Jobs supported by visitor expenditures.
- Direct Output:** Estimated value of original visitor spending that remains in the state (some visitor spending dollars paid to businesses will be exported out of the state).
- Indirect/Induced Impacts:** Increases in regional impacts from the local re-spending of direct dollars.
- Total Impacts:** The sum of Direct, Indirect, and Induced Impacts, for a total regional impact.

Estimated Regional Impacts from Visitor Spending

Total Estimated Visitor Spending:	\$ 342,700				
	Direct	Indirect/Induced	Total Impact	All State Impacts	% State Impact
Jobs	3	2	5	94,000	0.01%
Labor Income	\$ 96,000	\$ 82,000	\$ 178,000	\$ 3,311,700,000	0.01%
Output	\$ 292,000	\$ 254,000	\$ 546,000	\$ 10,160,600,000	0.01%

FISCAL IMPACTS

Estimated Taxes Paid to Each Jurisdiction Type

	Cities	Counties	Special Districts	State	Total Taxes
Airport Businesses	\$ -	\$ 15,000	\$ 115,000	\$ 67,000	\$ 197,000
Visitors	\$ 2,500	\$ 2,600	\$ 2,600	\$ 17,000	\$ 24,700
Total	\$ 2,500	\$ 17,600	\$ 117,600	\$ 84,000	\$ 221,700

NOTE: Tax estimates include Aircraft Excise Tax, Property Tax, Business & Occupation Tax, Sales Tax, Aviation Fuel Tax, State and Local Utility Taxes, Rental Car Tax, and Lodging Tax.
Special Districts include Transit, Schools, Hospitals, Fire, EMS, Parks, Ports, Utilities, and others.

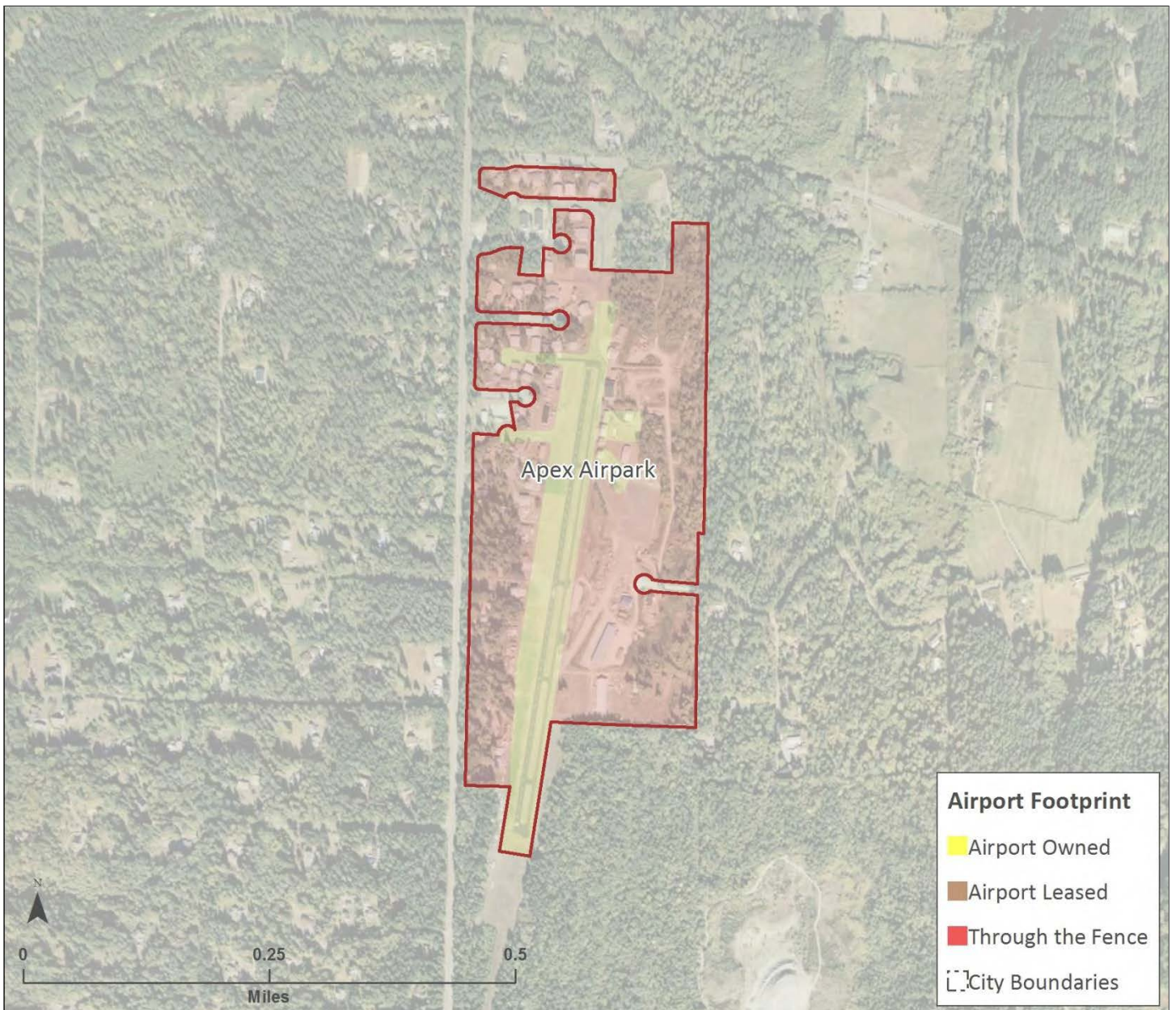
Data Sheet A: Airport Footprint Map

The analysis of economic activity on each airport is based on an airport footprint boundary. The airport boundaries are composed of property owned or leased by the airport.

Through-the-fence Connections. In rare cases, additional properties with physical connections to the airport and aviation-dependent activity are included in the footprint. These properties are considered “through-the-fence” connections and are indicated on footprint maps shaded in red. Examples of these connections include Boeing’s aircraft manufacturing operations at some airports and rural airparks that have direct connections to an airport.

When reviewing your airport footprint map, keep in mind that some footprints will show rights-of-way and other irregularities that do not affect the underlying analysis.

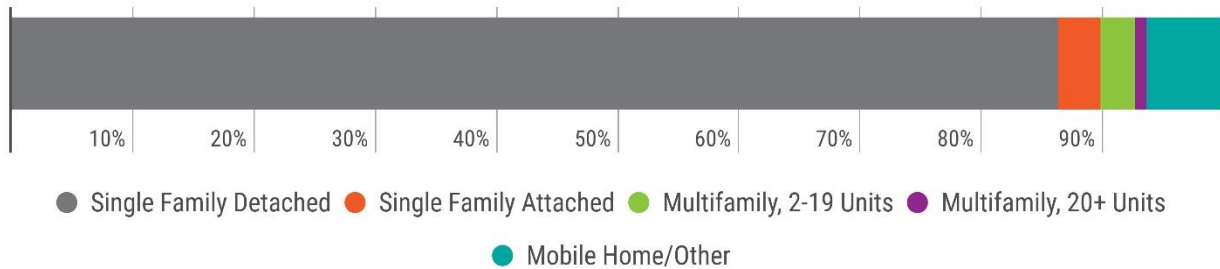
**Exhibit 1
Airport Footprint Map**



Comment 2.47D

Draft VISION 2050 | GMPB Recommendation – December 5, 2019

Figure 27 – Ownership Housing Stock by Housing Type, Central Puget Sound Region



Source: 2017 American Community Survey

Moderate density housing, ranging from duplexes to townhomes to garden apartments, bridge a gap between single-family housing and more intense multifamily and commercial areas and provide opportunities for housing types that are inclusive to people of different ages, life stages, and income ranges. Regional and local tools can help to promote and incentivize the development and preservation of more moderate density housing to give people greater housing choices, and produce urban densities that support walkable communities, local retail and commercial services, and efficient public transit.

Affordability

With increasing incomes for some and a surge in demand for housing, the region is experiencing an affordability crisis. Rising housing costs can be particularly devastating for low-income renters, particularly renters of color, many who pay more than 50% of their income on housing. These households are often the most at risk of losing their housing and experiencing homelessness. Many middle- and lower-income households struggle to find housing that fits their income in an increasingly competitive and expensive housing market due, in part, to zoning practices that have prevented the development of more affordable, smaller homes, and apartments. Home ownership may seem like less of a reality for potential first-time buyers as home prices continue to climb. This is especially true for people of color, who have been historically excluded from homeownership opportunities.

Low- to middle-wage workers – such as teachers, health care professionals, retail workers, administrative personnel, police officers, and firefighters – who are essential to the economic and social vitality of a community, often cannot afford to live in the places where they work. As affordable housing options become scarce, households are forced to move farther from their jobs and communities, resulting in increased traffic congestion and transportation costs and fragmentation of communities. This spatial mismatch also leads to an inability of certain segments of the labor market to fill positions.

Affordable Housing is commonly defined in terms of housing costs as a percentage of household income. Housing is considered unaffordable when a household's monthly housing costs exceed a certain threshold – most commonly 30% of gross income – thereby reducing the budget available for basic necessities.

Housing Affordability refers to the balance (or imbalance) between incomes and housing costs within a community or region. A common measurement compares the number of households in certain income categories to the number of units in the market that are affordable at 30% of gross income.





Search Options

- Parcel No
- Site Address
- Mail Address

Parcel Search

Show results in a table, not the map.

Map Scale: 1 : 2,400



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- Pan
- Draw
- Measure

- Layers
- Results
- Print

Zoom to Location >

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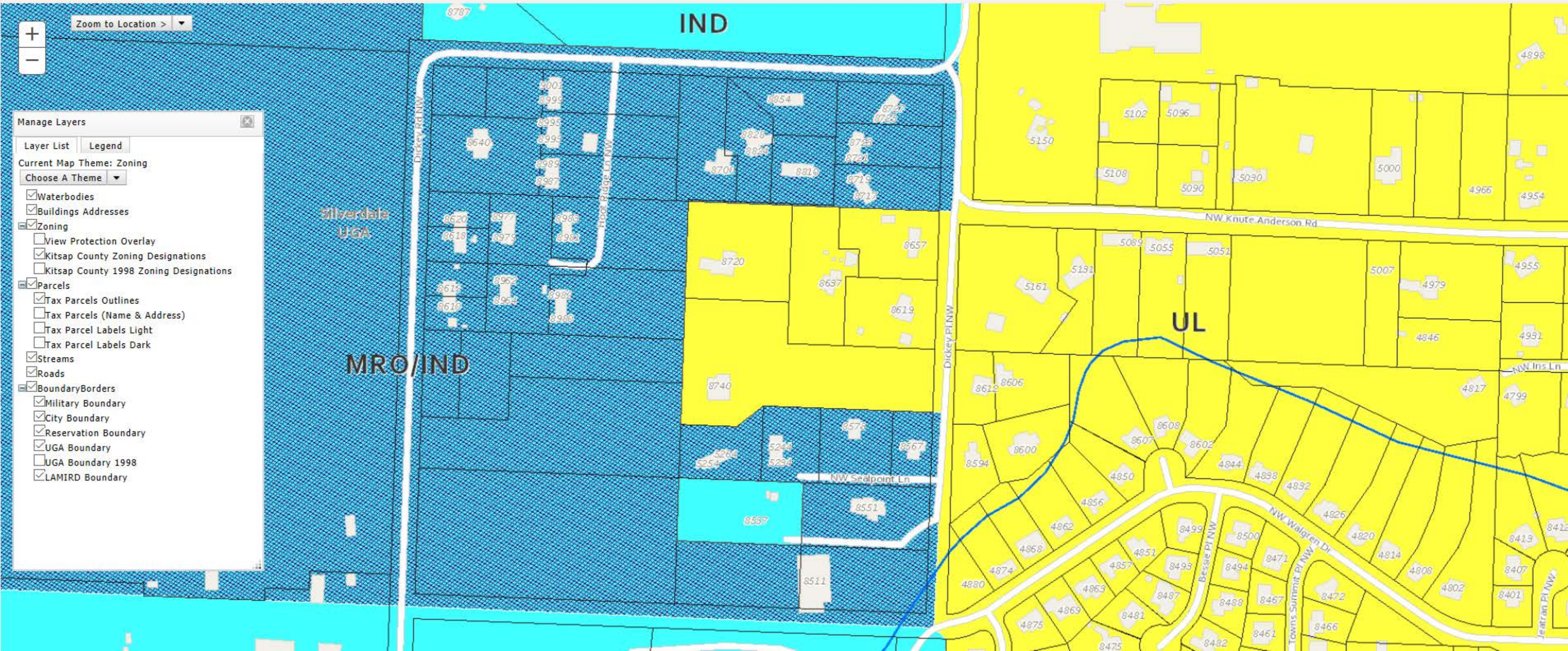
Manage Layers

Layer List Legend

Current Map Theme: Zoning

Choose A Theme

- Waterbodies
- Buildings Addresses
- Zoning
 - View Protection Overlay
 - Kitsap County Zoning Designations
 - Kitsap County 1998 Zoning Designations
- Parcels
 - Tax Parcels Outlines
 - Tax Parcels (Name & Address)
 - Tax Parcel Labels Light
 - Tax Parcel Labels Dark
- Streams
- Roads
- BoundaryBorders
 - Military Boundary
 - City Boundary
 - Reservation Boundary
 - UGA Boundary
 - UGA Boundary 1998
 - LAMIRD Boundary





Search Options

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- Site Address
- Mail Address

Parcel Search

Show results in a table, not the map.

Map Scale: 1 : 2,400

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- Results
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Zoom to Location >

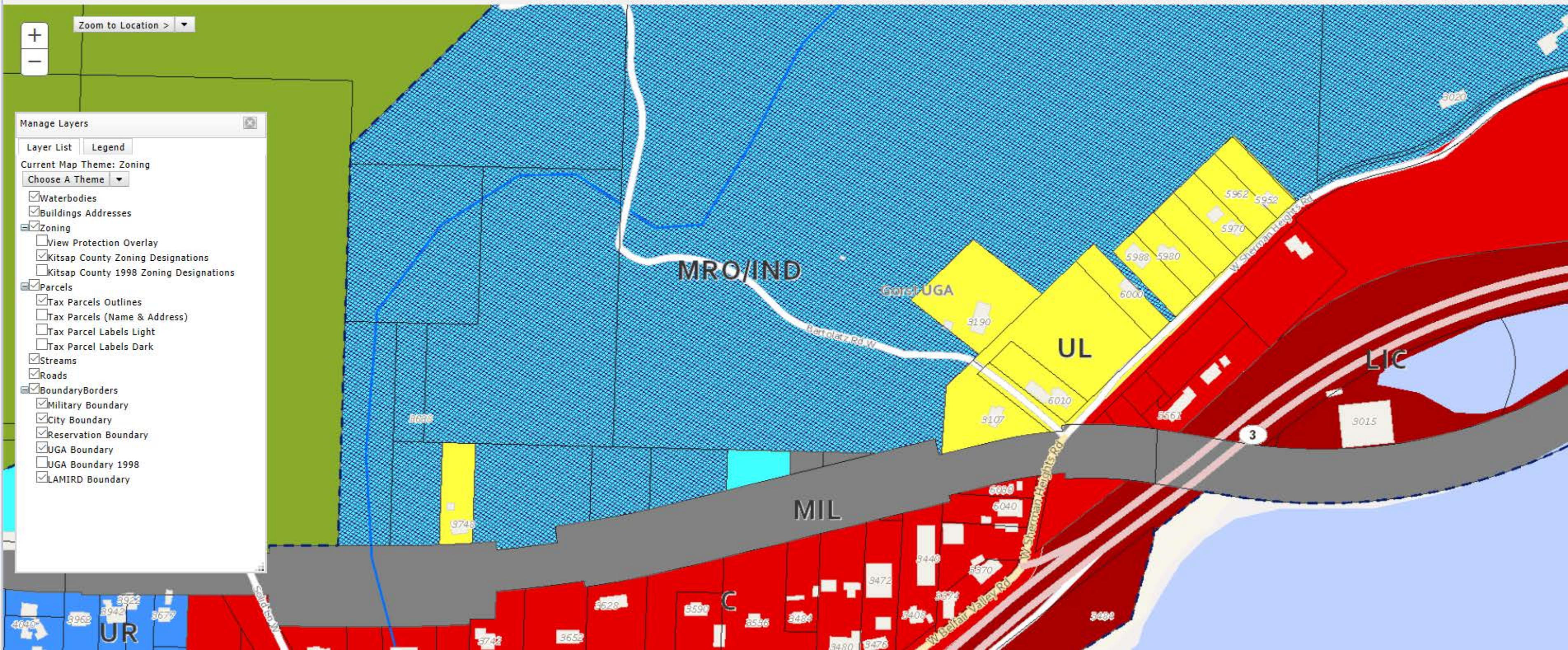
Manage Layers

Layer List Legend

Current Map Theme: Zoning

Choose A Theme

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 - UGA Boundary 1998
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Search Options

- Parcel No
- Site Address
- Mail Address

Parcel Search

Comment 2.47D

Show results in a table, not the map.

Map Scale: 1 : 4,800



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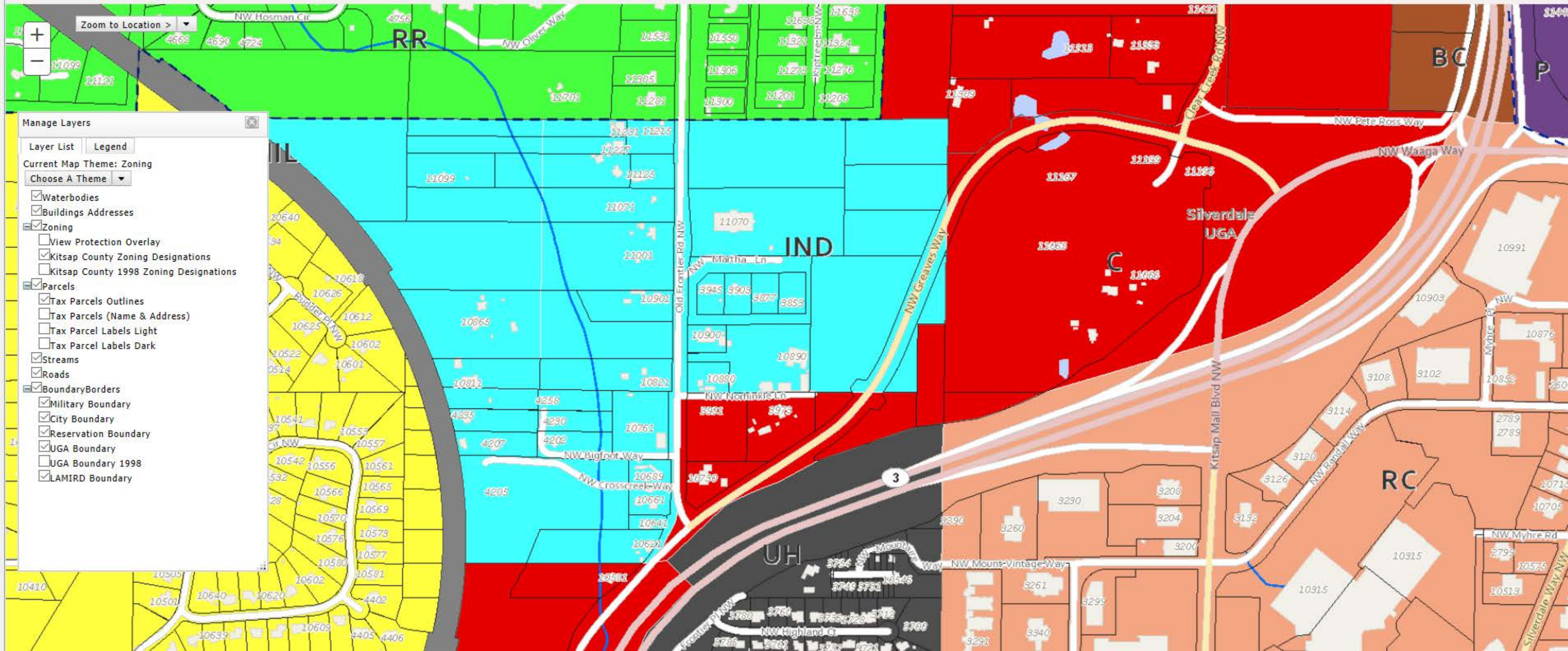


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- Zoom In
- Zoom Out
- Full Extent
- Prev Extent
- Next Extent
- Pan
- Draw
- Measure
- Layers
- Results
- Print



Manage Layers

Layer List Legend

Current Map Theme: Zoning

Choose A Theme

- Waterbodies
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 - UGA Boundary
 - UGA Boundary 1998
 - LAMIRD Boundary



Search Options

- Parcel No
- Site Address
- Mail Address

Parcel Search

Comment 2.47D

Show results in a table, not the map.

Contact GIS

Disclaimer

Help

Map Scale: 1 : 4,800

- Zoom In
- Zoom Out
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Zoom to Location >

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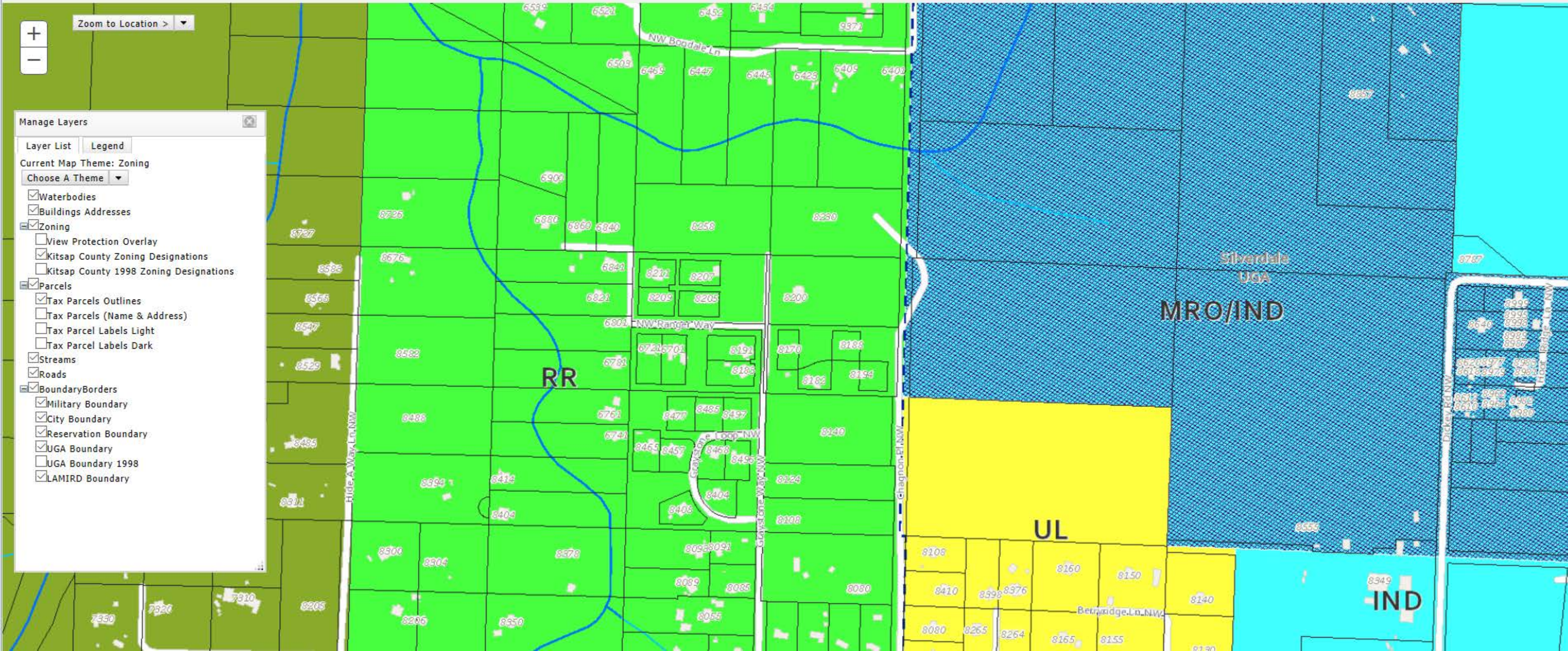
Manage Layers

Layer List Legend

Current Map Theme: Zoning

Choose A Theme ▾

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- Parcels
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 - LAMIRD Boundary





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Parcel Search

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Map Scale: 1 : 4,800

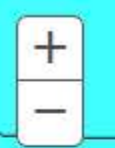
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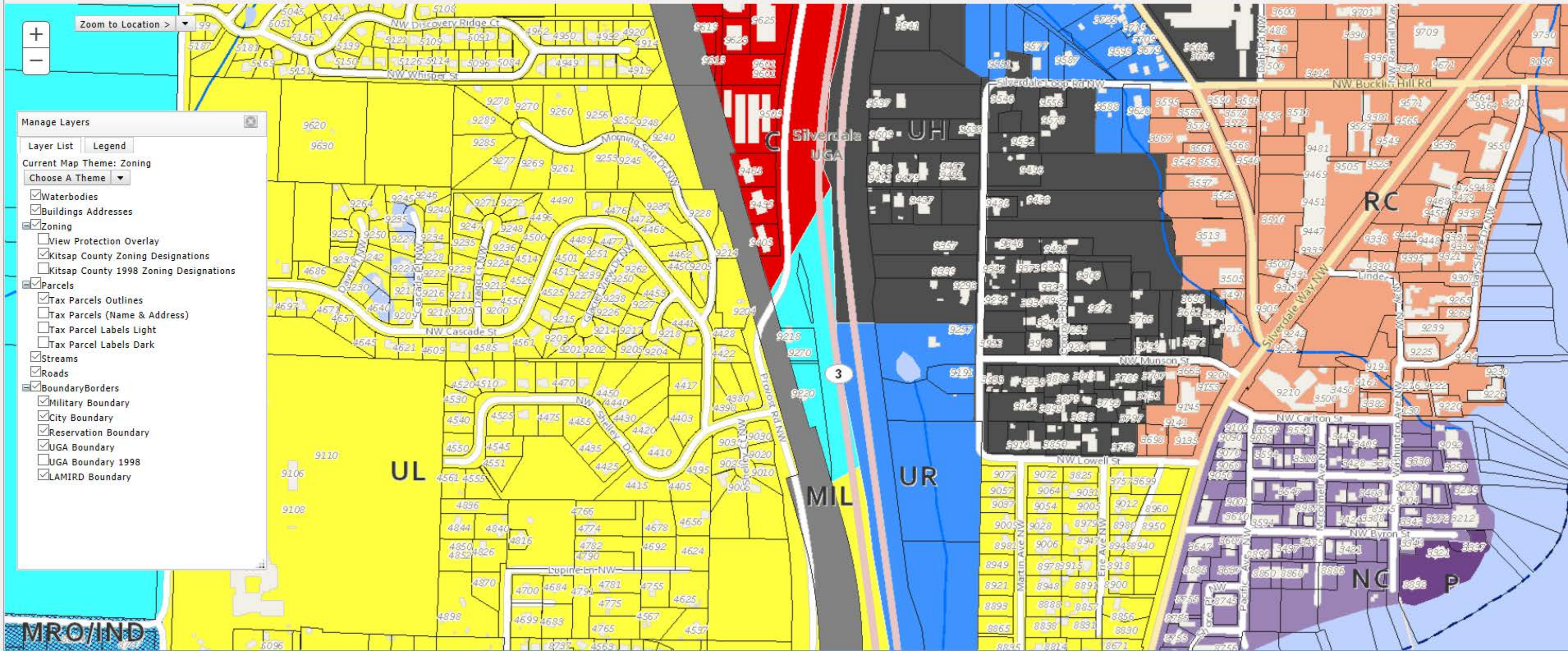
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Layer List Legend

Current Map Theme: Zoning

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- Waterbodies
- Buildings Addresses
- Zoning
 - View Protection Overlay
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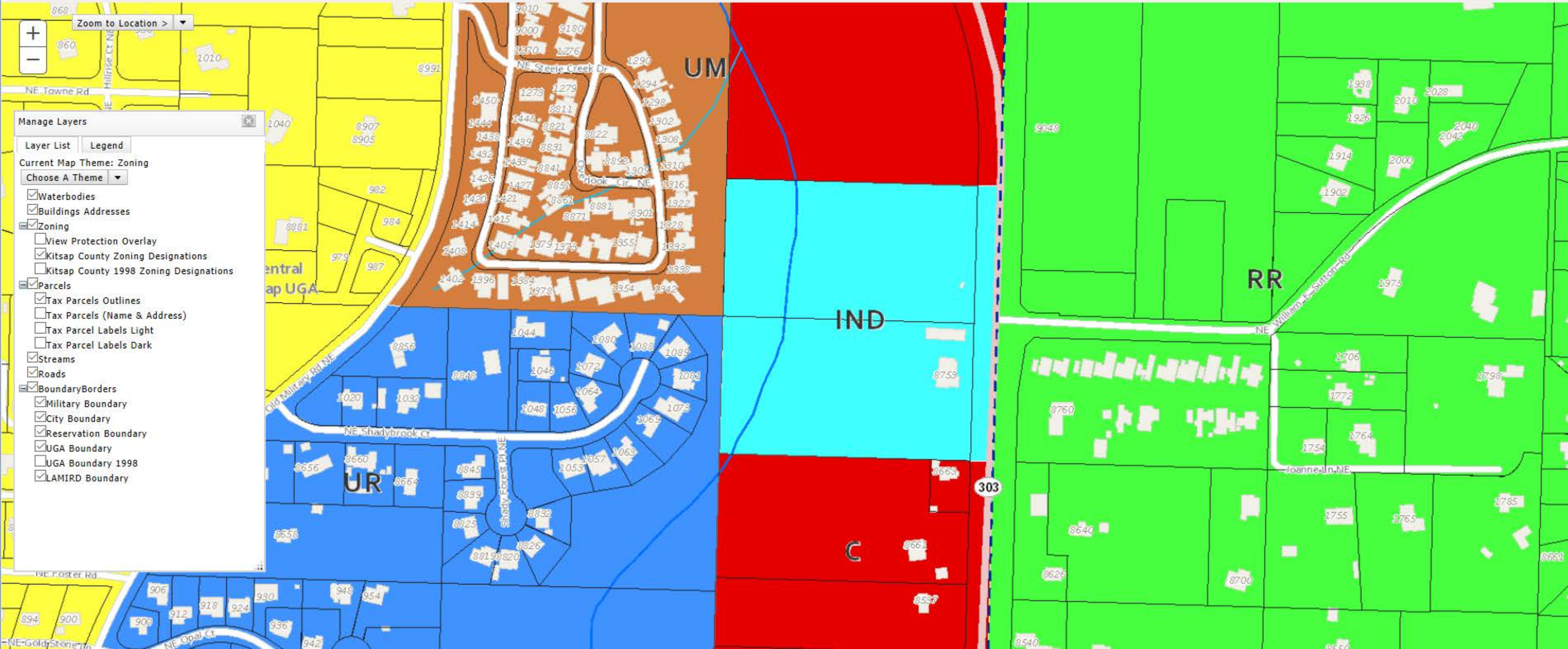
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Map Scale: 1 : 2,400

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Search Options

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Parcel Search

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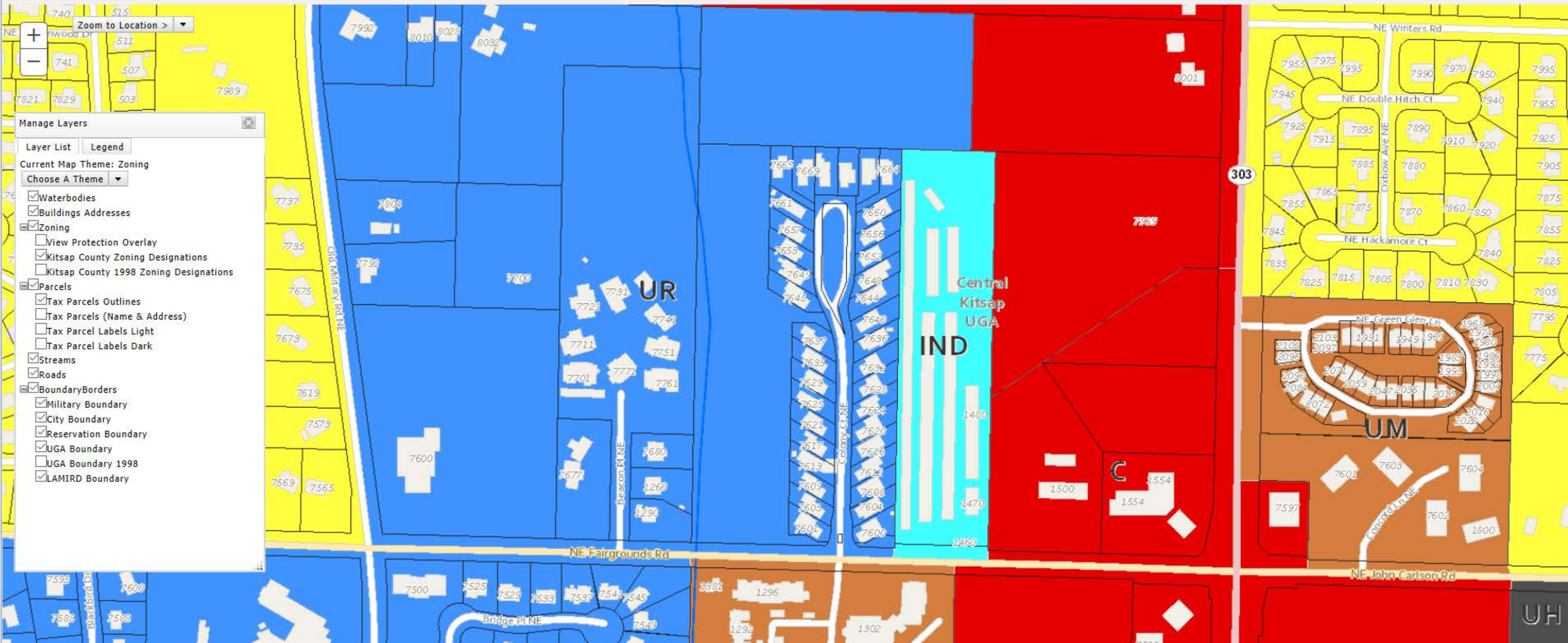
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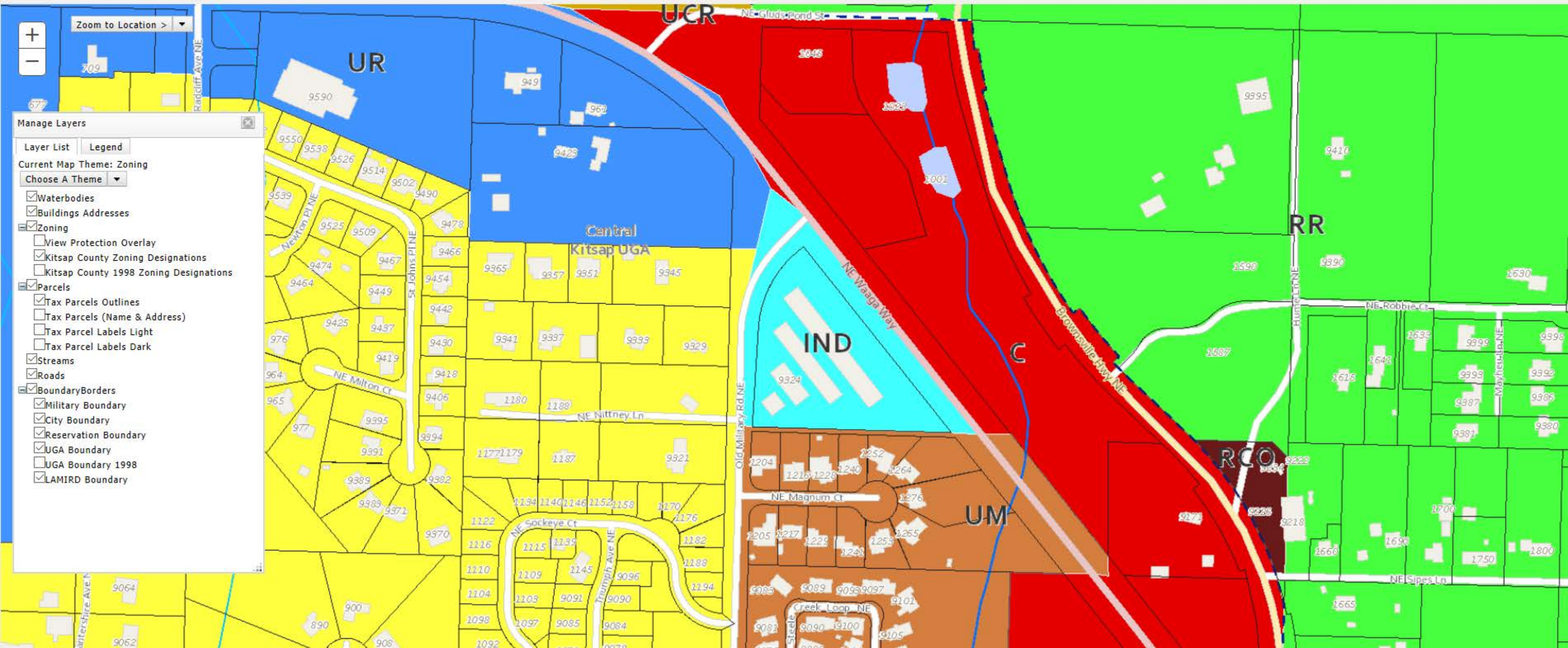
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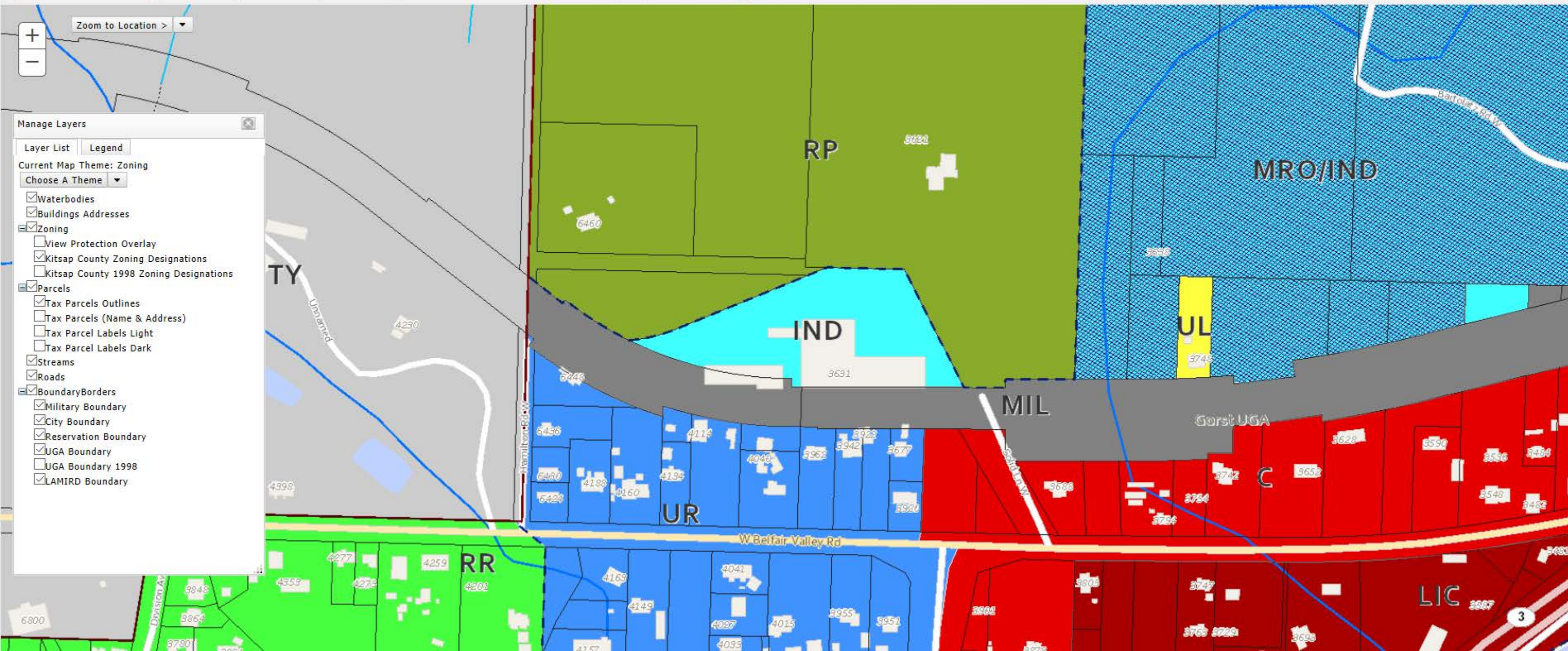
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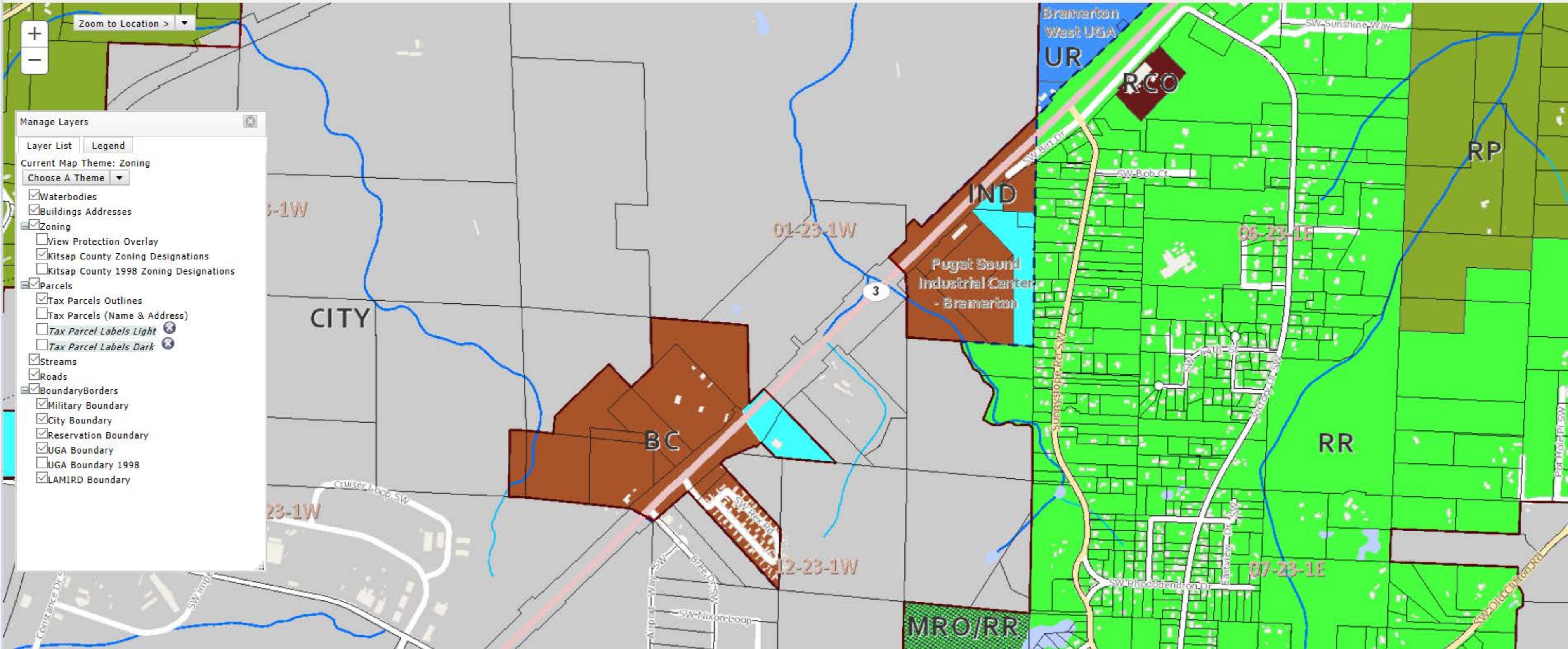
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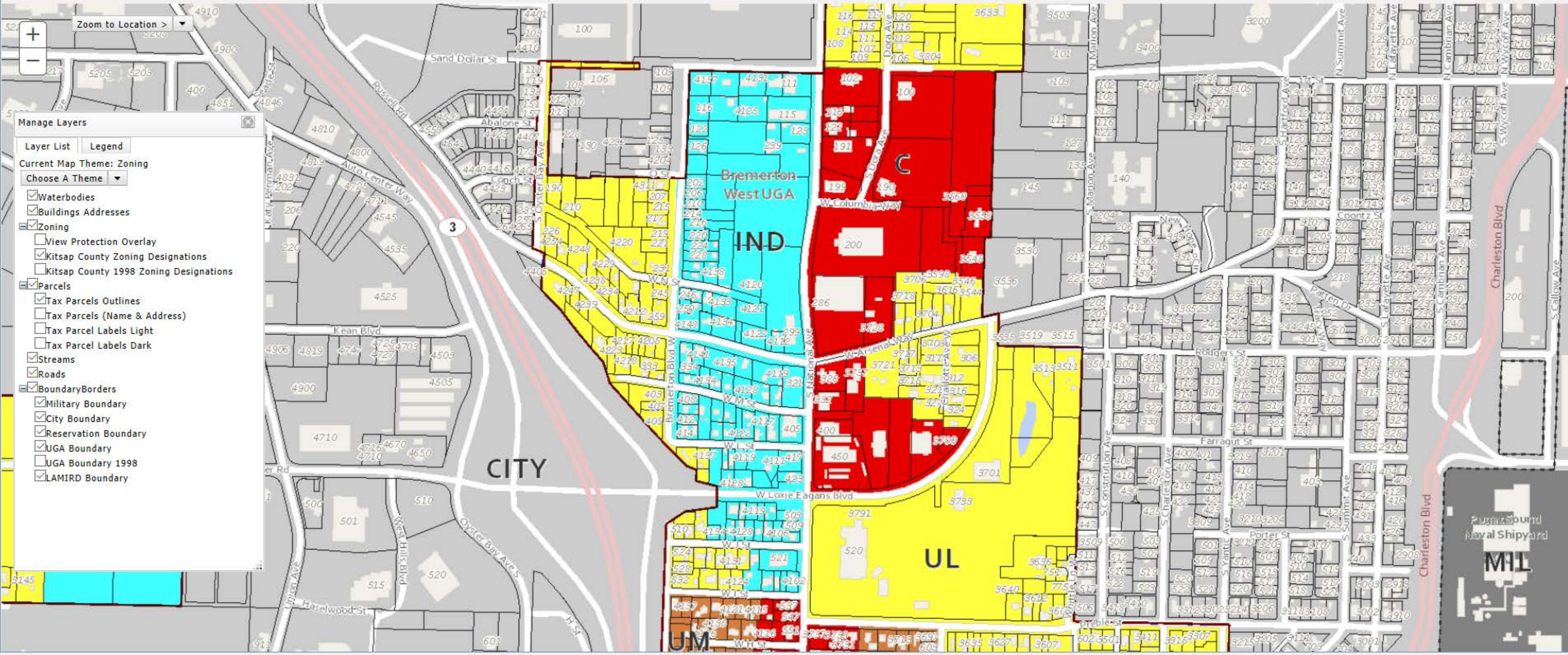
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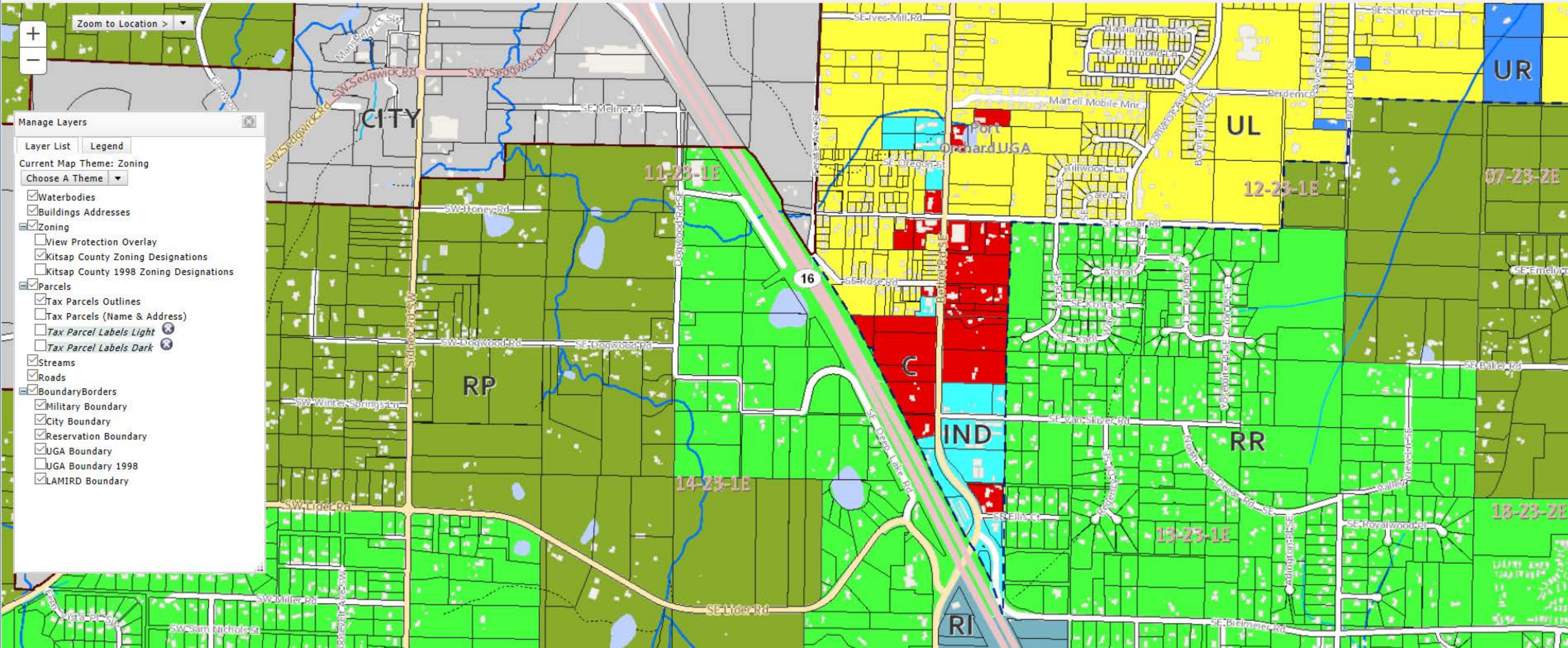
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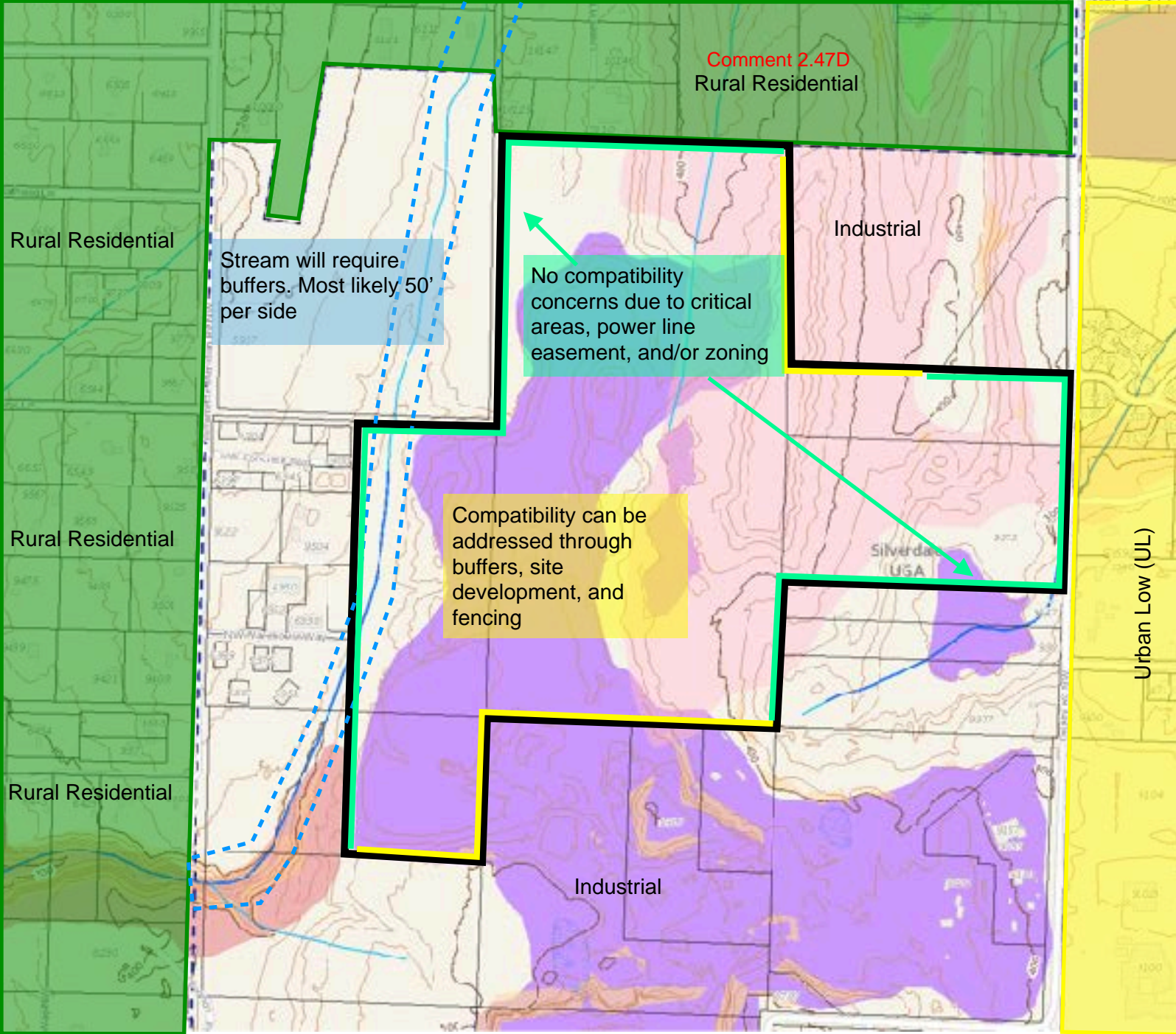
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Comment 2.47D
Rural Residential

Rural Residential

Stream will require buffers. Most likely 50' per side

Industrial

No compatibility concerns due to critical areas, power line easement, and/or zoning

Rural Residential

Compatibility can be addressed through buffers, site development, and fencing

Silverdale, USA

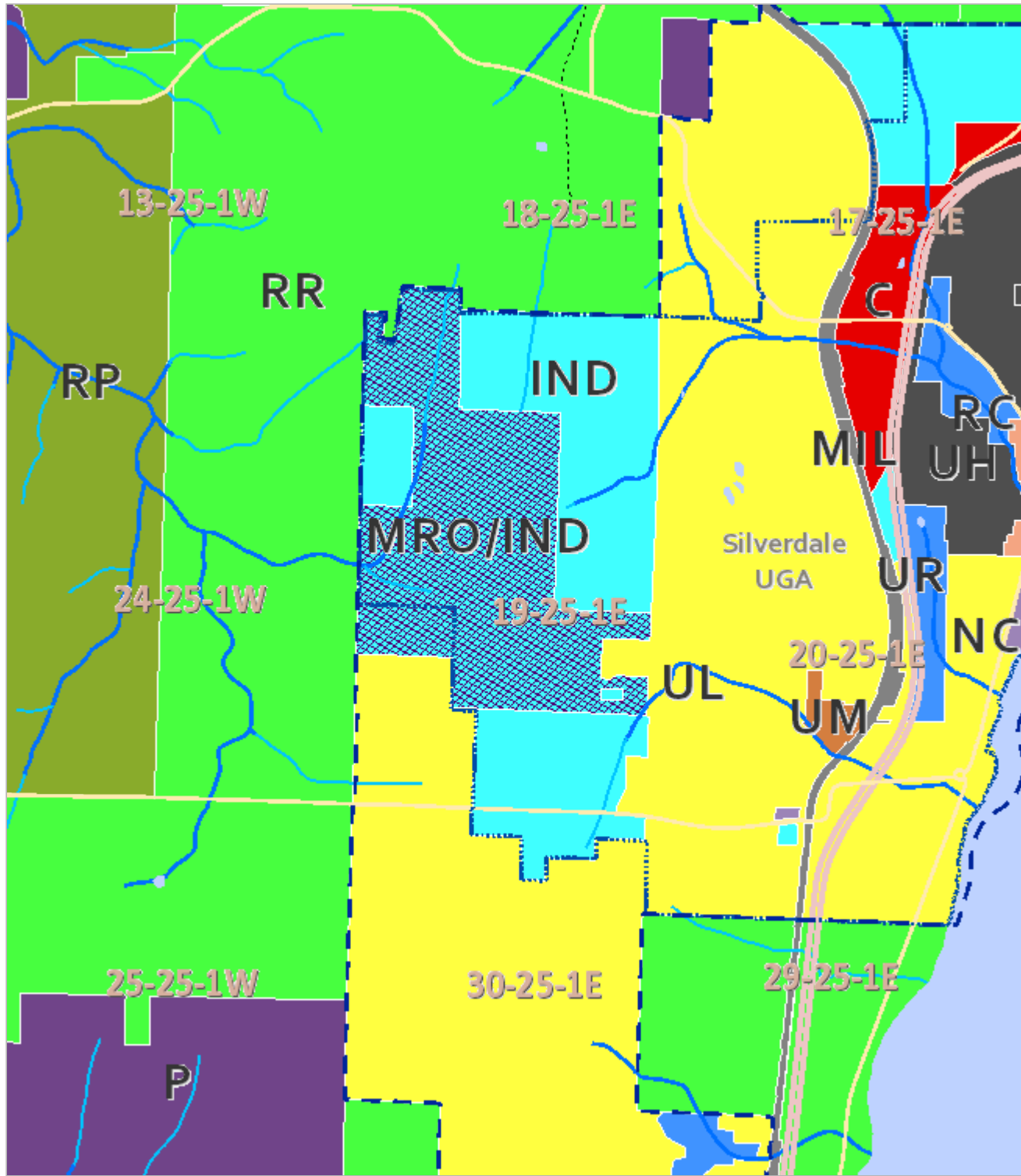
Urban Low (UL)

Rural Residential

Industrial

Map Scale: 1 : 24,000

Printed: Tuesday, Jan 21, 2020



** This map is not a substitute for field survey **

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Existing Surrounding Uses/Zoning Exhibit



Staff Report Rational and/or Analysis	Applicant Response
<p>Non-project specific 2016 Comprehensive Plan, Countywide Planning Policies, and Multi-county planning policies seek to absorb growth into the Silverdale Regional Growth Center and other designated centers. The proposed site is outside of these designated centers and would likely reduce demand for housing and employment within the centers.</p>	<p><i>The goals and policies below show that there are many goals and policies in place that support the Comprehensive Plan Amendment.</i></p> <p>Silverdale Sub-Area Plan</p> <p>Silverdale Land Use Goal 1 – Provide sufficient capacity within the Urban Growth Area to properly accommodate a mix of residential, commercial and industrial development to meet the extended population and employment projects for Silverdale.</p> <p>Silverdale Policy 1 – Monitor land supply over time to ensure a continued adequate supply of residential, commercial and industrial designated land to meet Silverdale’s population and employment targets and to meet the needs of unexpected growth.</p> <p>Silverdale Policy 2 - In establishing and modifying land use designations, provide for a balanced and complete community.</p> <p>Silverdale Policy 3 - Incorporate reasonable measures that are appropriate to the Silverdale area to help focus growth in the urban growth area. These measures could be incentives, standards, policies, and/or regulations. In this Sub-Area Plan, incorporated reasonable measures include:</p> <ul style="list-style-type: none"> • Inclusion of an economic development plan element intended to encourage employment and related housing/population growth in the Urban Growth Area; • Support the Silverdale Regional Plan goals and Policies. <p>Silverdale Goal 5 - Support and coordinate economic expansion through efficient use of land and provision of capital facilities.</p> <p>Silverdale Policy 14 - Encourage full use and development of designated commercial and industrial areas prior to expanding those areas. Promote revitalization within existing developed areas to take advantage of the investment in existing buildings and infrastructure.</p> <p>Silverdale Regional Center</p> <p>The number 1 policy for the Silverdale Regional Center is to maintain the designation. While policy exists (see below) to drive the “majority” of growth to the center, maintaining or achieving a particular density is not necessary to maintain the designation. See the Puget Sound Regional Council Regional Centers Framework Update page 10 under “Center criteria” adopted 3/22/18 that states “existing centers will remain designated if they do not meet the new center density criteria, provided that the center is consistent with other criteria identified in this section.” Additionally, the “majority” is not defined nor quantified.</p>

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Staff Report Rational and/or Analysis	Applicant Response
	<p>SRC Policy 1 - Meet the Puget Sound Regional Council requirements necessary to maintain the Regional Growth Center Designation for Silverdale.</p> <p>SRC Economic Development Goal 16: Maintain Silverdale’s economic engine by accommodating and attracting a majority of anticipated job and housing growth for the Silverdale Urban Growth Area in the Regional Growth Center.</p> <p>SRC Housing Goal 18: Locate a majority of Silverdale Urban Growth Area housing growth in the Silverdale Regional Growth Center.</p> <p><i>The change is insignificant.</i> The Land Capacity Analysis (Staff Report Exhibit C15) assumes only 151 additional population would be created from the proposal compared to the population capacity of 8,753 in the Silverdale UGA (less than 4.5% change) and population capacity of 31,837 in the Countywide UGA (less than 1.5% change). The Land Capacity Analysis (Staff Report Exhibit C15) assumes only 18 additional jobs would be created from the proposal compared to the employment capacity of 8,592 in the Silverdale UGA (less than 0.5% change) and employment capacity of 14,200 in the Countywide UGA (less than 0.2% change).</p> <p><i>Not all citizens want to live in high density, stacked housing.</i> Over 85% of Central Puget Sound Residents live in single family detached homes (Applicant Exhibit - Figure 27 Vision 2050). The Silverdale Regional Center only allows multifamily and single-family attached housing which are both conditional uses. The Neighborhood Commercial and Urban Low Residential zones allow for Duplex, Multifamily, Single-Family Attached, Single-Family Detached, Accessory Dwelling Units, Accessory Dwelling Quarters and Cottage Housing. All of these uses are permitted with the exception of Cottage Housing which is a conditional use.</p> <p>The Land Capacity Analysis (LCA) projects capacity for only 666 units in the Regional Center over the entire planning period (24 years). Development will be necessary outside of the Regional Center to adequately provide for housing needs.</p>
<p>Project Specific The proposed UL residential and mix-use NC zones will be located adjacent to a general aviation airport (including under incompatible flight zones), a surface mine (with planned</p>	<p><u>Airport Concerns</u> <i>Apex Airport offers minimal economic contribution.</i> It offers no services (such as fuel, repair services, or supplies), no access to public roads, public transit, rail, or marina. Apex Airport is a Private Public Airport. This means that anyone from the Public can land and takeoff from the runway however they cannot access any of the surrounding properties or community</p>

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Staff Report Rational and/or Analysis	Applicant Response
<p>expansions and operations likely to continue approximately 40-50 years), and other industrial uses as well as additional undeveloped industrial lands. In total, 82% of the proposed site would border the IND zone.</p>	<p>without permission of the private ownership. Any potential amenities are located "through the fence" which WSDOT describes as "In rare cases, additional properties with physical connections to the airport and aviation-dependent activity are included in the footprint." These properties are not part of the airport and not available for public use (Applicant Apex Airport Exhibit). These lack of services limits the use and significance of the airport as compared to other airports in the region.</p> <p><u>Air traffic is minimal.</u> It is primarily reduced to the surrounding "through the fence" homeowners. WSDOT describes "through the fence" as "In rare cases, additional properties with physical connections to the airport and aviation-dependent activity are included in the footprint." These properties are not part of the airport and not available for public use (Applicant Apex Airport Exhibit). On review of the County's Parcel Search Map it appears that only three remaining parcels abutting the runway are undeveloped further supporting the argument that air traffic is not likely to increase.</p> <p><u>Expansion is unlikely.</u> The majority of adjacent properties (including the private ownership) are zoned Rural Residential which does not allow for support services associated with other general aviation airports.</p> <p><u>Applicant can record a notice to title regarding the Airport.</u> This is an acceptable mitigation in the WSDOT Airports and Compatible Land Use Guidebook.</p> <p>WSDOT Airports and Compatible Land Use Guidebook Page V. states "<u>WSDOT does not have regulatory authority over land use decisions; however, we offer a technical assistance program to help towns, cities and counties address aviation issues.</u>"</p> <p>WSDOT Airports and Compatible Land Use Guidebook – Table 1-2 Compatibility Concerns Represented by Particular Land Uses – The following table shows that there are concerns even for the existing land use designation of Industrial.</p>

Staff Report Rational and/or Analysis	Applicant Response								
	<p style="text-align: right;"><i>Table 1-2 Compatibility Concerns Represented by Particular Land Uses</i></p> <table border="1" data-bbox="789 269 1486 548"> <thead> <tr> <th>Land Use Type</th> <th>Compatibility Concerns</th> </tr> </thead> <tbody> <tr> <td>Single-Family Residential</td> <td> <ul style="list-style-type: none"> Noise can be disruptive in outdoor areas as well as indoors with open windows. Aircraft overflight can be annoying, especially where ambient noise levels are low such as in suburban or rural areas. </td> </tr> <tr> <td>Multi-Family Residential</td> <td> <ul style="list-style-type: none"> Noise can be disruptive in outdoor areas as well as indoors with open windows, although less sensitive than for single-family residential. High density presents concern for safety of residents in areas exposed to significant risk of aircraft accidents. </td> </tr> <tr> <td>Industrial Uses</td> <td> <ul style="list-style-type: none"> Smoke, steam, and thermal plumes can be hazards to flight. Tall structures can be airspace obstructions. Possible release of hazardous materials if damaged during an accident. </td> </tr> </tbody> </table> <p>Chapter 2 lists “Retail/Service Uses/Mixed Use” as “compatible” uses within the Turning Zone (Zone 3) and Beneath Traffic Patterns (Zone 6) area. Residential uses are listed as “incompatible” in the Turning Zone and “generally incompatible” Beneath Traffic Patterns.</p> <p>Chapter 3 provides tools for enhancing compatibility including:</p> <ul style="list-style-type: none"> Clustering of development within Zone 6 which most of the subject property falls within. Aviation Disclosure Notices Aviation Easements Real Estate Disclosure Statements <p>Appendix F – Compatibility Criteria provides guidelines for maximum residential densities. It suggests that in urban areas (within the urban growth boundary) Zone 3 should promote “infill development up to average of surrounding residential area is allowed, but is appropriate only if nonresidential uses are not feasible.”</p> <p>It suggests that Zone 6 should “promote high density and intensity mixed-use development (15 or more d.u. per acre).”</p> <p>Table F-2 Airport Land Use Matrix – D. Urban Development – 1. Residential lists single-family dwelling: up to 12 dwelling units/acre as incompatible with Zone 3, however it suggests that in Zone 6 they are “Limited” as “uses or activities that may be compatible with airport operations depending on their location, size, bulk, height, density and intensity of use”. Multi-family dwellings are listed in Zone 3 as “Limited with Special conditions – development should be moved away from the extended runway centerline. Open space should be devoted to areas that experience elevated risk” however multi-family is outrightly “permitted” within Zone 6.</p>	Land Use Type	Compatibility Concerns	Single-Family Residential	<ul style="list-style-type: none"> Noise can be disruptive in outdoor areas as well as indoors with open windows. Aircraft overflight can be annoying, especially where ambient noise levels are low such as in suburban or rural areas. 	Multi-Family Residential	<ul style="list-style-type: none"> Noise can be disruptive in outdoor areas as well as indoors with open windows, although less sensitive than for single-family residential. High density presents concern for safety of residents in areas exposed to significant risk of aircraft accidents. 	Industrial Uses	<ul style="list-style-type: none"> Smoke, steam, and thermal plumes can be hazards to flight. Tall structures can be airspace obstructions. Possible release of hazardous materials if damaged during an accident.
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Staff Report Rational and/or Analysis	Applicant Response
	<p>Olson and Associates, (a professional engineering and survey company) has prepared an Applicant Airport Overlay Exhibit which represents the entire airport overlay, not just as it pertains to the subject properties. As shown on the exhibit, there are a variety of existing uses, most of which are residential, within the various airport compatibility zones. These uses were not considered significant enough in past comprehensive plans as to warrant non-project requirements or mitigation such as rezoning of the parcels and/or restrictions on uses. They were deemed compatible with WSDOT guidelines and any potential impacts could be mitigated during project specific permit review.</p> <p>Additional options and considerations are given to a variety of other uses supporting the conclusion that the final determination regarding site specific uses should be achieved at the project-specific permit level. The County maintains these tools and others after the Comprehensive Plan Amendment to ensure that proposed developments are consistent with guidelines and regulations to safeguard compatibility with surrounding uses.</p> <p>Compatibility of the surrounding areas: The Staff Report contends that 82% of the subject properties would boarder the Industrial (IND) zone and that industrial zoned property abutting residentially zoned properties are incompatible, however currently 100% of this entire industrially zoned area is surrounded by either Urban Low (UL) or Rural Residential (RR) zoning and residential uses (Applicant Existing Surrounding Uses/Zoning Exhibit). The current comprehensive plan has numerous instances of industrial lands adjacent to residential and commercially zoned properties and uses. Several examples are documented in the applicant’s original attachments for Staff Report Exhibit C4 and submitted to the Planning Commission as Applicant Exhibit 6 – Compatible Uses. These examples include industrial land completely surrounded by residential and commercial, industrial surrounded on three sides by residential, and residential surrounded on three sides by industrial.</p> <p>Surface mine concerns and adjacent uses: Kitsap County Code (KCC) has existing provisions in place to mitigate surrounding incompatible uses. Mitigation requirements fall as much on the development of the subject parcels as they do the potential expansion of neighboring uses. KCC 17.500.025 requires that a minimum of 15% of the site be landscaped. Additionally, KCC 17.500.027 directs buffer</p>

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Staff Report Rational and/or Analysis	Applicant Response
	<p>sizes based on neighboring uses and zoning. A standard subdivision would be required to provide a Solid Screening Buffer (KCC 17.500.027.B) which requires a minimum width of 50-feet when residential zoning abuts industrial zoning or uses. The Director has the authority to increase this requirement depending on the proposed use of the site and adjacent zones and/or uses (KCC 17.500.027).</p> <p>If a Performance Based Development (PBD) were proposed the standards for development on the subject property increase with requirements for both common and recreational open space (KCC 17.450.040B. & C.). Common open space requirements for PBD's is an additional 15% and additional recreational open space of 5%. This means that if the subject properties were developed utilizing a PBD, a minimum of 35% of the site could be in various types of open space or landscaping. Critical area buffers would also be required around much of the perimeter of the subject parcels. After accounting for critical areas and power easements, only 37% of the boundary is shared with industrial zoned property (See Applicant Adjacent Land uses Exhibit). Strategically placing landscaping and open space would be a top priority to the development of this site as it would increase the marketability to the end user.</p> <p>Again, the County maintains these tools and others after the Comprehensive Plan Amendment at a project specific level to ensure that proposed developments are consistent with guidelines and regulations to safeguard compatibility with surrounding uses.</p>
<p>Non-Project Specific The circumstances and assumptions regarding the area have not substantially changed since the adoption of the 2016 Comprehensive Plan amendment, which is a blueprint for how the county should develop in the next 20-year planning horizon. Assumptions may be revised during the required 8-year Comprehensive Plan review due by 2024.</p> <p>Staff Lists the following headings under the Staff Analysis section of the Staff Report:</p>	<p><u>Status and Capacity of Mine Site:</u> During the 2016 Comprehensive Plan update this property was not considered "available" due to active mining permits. The mine resources have been depleted and the mine is ready for reclamation and re-development and therefore needs to be added to the available inventory and the uses need to be reconsidered. This is a significant change in circumstances since the 2016 Comprehensive Plan update. The staff report argues that because there are still active mining permits with the State Department of Natural Resources the parcels were excluded from the Land Capacity Analysis (LCA) for the 2016 Comprehensive Plan Update. Consequently, when the properties are reclaimed and considered available for redevelopment, the Counties LCA balances better after the amendment than when it was completed in 2016 (3.8% below the target for jobs vs. 1% above the target). If the proposed amendment is adopted, the LCA is still in balance at 1.2% above the adopted employment target which is still within the accepted margin of +/-5%. While the LCA balances regardless</p>

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Staff Report Rational and/or Analysis	Applicant Response
<ul style="list-style-type: none"> • <u>Status and Capacity of Mine Site</u> • <u>Housing Supply & Population Capacity</u> • <u>Supply & Demand for Industrial Land</u> • <u>Liquefaction Hazards</u> 	<p>of the amendment, the LCA is more favorably balanced after the Comprehensive Plan Amendment.</p> <p>ZERO jobs will be created if the property is not redeveloped and the cost of reclamation makes developing the property as an industrial project financially infeasible.</p> <p><u>Housing Supply & Population Capacity:</u></p> <p>The staff report argues that because the 2016 Comprehensive Plan planned for population growth for a 20-year horizon and that because population numbers show slower growth than anticipate that there is not a housing shortage, however, Kitsap County is experiencing a housing supply crisis. The 2016 Comp Plan projected housing trends based on data sets from the worst housing recession the region, state, and country has ever experienced. The Comp Plan reduced the UGA boundary, effectively limiting potential urban supply. Furthermore, due to the rapid expansion of large employers in Seattle, the Puget Sound is experiencing massive population increase and housing demand. The following information was obtained from the Northwest Multiple Listings Service (NWMLS) and attached as an Applicant exhibit:</p> <p>A) Median days on market is down to 10 days as opposed to pre-recession of over 35 days (see Applicant Exhibit – Housing Market Demand)</p> <p>B) The Median house price is over \$375,000 (equal to Pierce County and Snohomish County) as opposed to pre-recession of \$250,000 (see Applicant Exhibit – Housing Market Demand).</p> <p>C) Months' supply of housing units is less than 2 months. 6-7 months is considered a balanced market (see Applicant Exhibit – Housing Market Demand).</p> <p>D) Median Price per square foot is well over \$200/ft as opposed to pre-recession of \$150/ft (see Applicant Exhibit – Housing Market Demand).</p> <p>The County Staff Report Attachment C11 “KEDA Consultation Notes” page 2, KEDA offered the following comment:</p> <p>“While we, as economic developers, have a proclivity to protect limited industrial sites as future homes to living wage manufacturing type jobs – we certainly recognize that now is an historic situation relative to lack of inventory of affordable – workforce housing units in Kitsap – indeed the entire region – which may provide a solid reason to convert this industrial land at this time – with an awareness that this should not set a precedent relative</p>

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	<p>to rezoning – down-zoning other industrial properties.”</p> <p>The Business Major Industries Summary found on KEDA’s website indicates that only 11.1% of Kitsap Counties Major Industries is made up of industrial type uses (see Applicant Exhibit – Industrial Demand).</p> <p><u>Supply & Demand for Industrial Land:</u></p> <p>The Staff Report contends that the proposed amendment would reduce the availability of vacant industrial land within the UGA by 50% and Countywide by 36% outside of the Puget Sound Industrial Center – Bremerton, however the Staff Report, pages 7-8 explain that the subject properties we assumed developed during the 2016 Comprehensive Plan update and were therefore never considered “available” industrial properties to begin with.</p> <p>Additionally, it is incomplete and misleading for the County to simply remove the available properties within the Puget Sound Industrial Center (PSIC) to support their claims. Of the Counties four exhibits (C10-C13) that note the supply and demand for industrial lands, only Exhibit C13 was prepared as part of a study or broader publication prepared by a 3rd party. The other exhibits were prepared by County Staff and are lacking market data from credible sources such as the Northwest Multiple Listing Service, the Commercial Brokers Association or the National Association of Realtors. Staff Report Exhibit C13, the only broader publication, states the PSIC has a surplus of land beyond that required to accommodate forecasts and that Dispersed Kitsap County had adequate capacity to absorb employment forecasts, provided industrial and non-industrial growth occurs with moderate employment and building densities. As noted throughout the Staff Report the subject properties were already considered “developed” and not counted as available industrial lands.</p> <p>There is little demand for Industrial property outside of Puget Sound Industrial Center (PSIC) (See Applicant Exhibit Industrial Demand).</p> <p>Staff gives an incomplete picture by leaving the PSIC out of its analysis. Since the adoption of the 2016 Comprehensive Plan, there has been very little development of industrial properties. Furthermore, many of the properties that have been developed are utilizing low intensity uses which are allowed most commercial zones including Neighborhood</p>

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	<p>Commercial such as Public Facilities, Storage, Veterinary Clinic/Animal Hospital, Kennels, General Office, Engineering and Construction offices, Automotive service Station, and Automotive repair and car washes (see Staff Report Exhibit A (should be C1)).</p> <p>Liquefaction Hazards: The subject properties contain areas of liquefaction hazards. The Staff Report states that the Applicant has provided a geotechnical report demonstrating that reasonable development methods would mitigate the hazard at a project specific level and therefore can be reviewed and addressed at the project-specific permit level.</p>
<p>Non-Project Specific There is not a shortage or surplus of land capacity for population or employment within the Silverdale UGA or in Kitsap County overall currently or anticipated in the 20-year planning period. Available capacity and growth assumptions may be revised during the required 8-year Comprehensive Plan review due by 2024.</p>	<p>The proposed changes are insignificant when compared to the entire UGA however they error on the side of surplus. The Land Capacity Analysis (Staff Report Exhibit C15) assumes only 151 additional population would be created from the proposal compared to the population capacity of 8,753 in the Silverdale UGA (less than 4.5% change) and population capacity of 31,837 in the Countywide UGA (less than 1.5% change). The Land Capacity Analysis (Staff Report Exhibit C15) assumes only 18 additional jobs would be created from the proposal compared to the employment capacity of 8,592 in the Silverdale UGA (less than 0.5% change) and employment capacity of 14,200 in the Countywide UGA (less than 0.2% change).</p>
<p><i>The proposed amendment meets concurrency requirements for transportation, sewer and water, and will not result in significant adverse impacts on adopted level of service standards for other public facilities and services, such as police, fire and emergency medical services, park services, and general government services;</i></p>	
<p>Project Specific Staff Analysis: A preliminary analysis of public facilities completed by the Department and by the service providers identified numerous existing capacity deficiencies and anticipated deficiencies over the 20-year planning horizon (2016-2036), which are generally planned to be addressed over that 20-year period (see Attachment C9). Based on current land capacity methodology, the Department and providers estimate that for the proposed</p>	<p>All service providers have been given an opportunity to address any potential deficiencies. The majority of the providers commented and all of those who commented stated that any potential deficiencies could be managed at a project specific level. Furthermore, the subject property is already located inside the Urban Growth Boundary, and the current zoning allows for the most intensive uses. It is possible that many of the capital facility providers will see a reduction in demand due to the change of zoning.</p> <p>Kitsap County utilizes general assumptions for planning purposes. For instance, the Land Capacity Analysis (LCA) does not attribute any population to the following commercial zones:</p> <ul style="list-style-type: none"> • Urban Village Center (10 units per acre minimum with no max) • Regional Center (10-60 units per acre)

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<p>amendment (see Attachment C15) there are additional service deficiencies for several public facilities over and above the existing and anticipated deficiencies. <u>Providers anticipate, however, that these deficiencies can be addressed through future project-level mitigation as noted below in Table 1.</u> Should a future development proposal for this site significantly exceed the current land capacity methodology estimates for population and employment (see Attachment C15), there is a risk that:</p> <ul style="list-style-type: none"> • Additional public facilities may become deficient; • Deficiencies may extend farther from the proposed site than currently anticipated; • The developer may not be able to pay for its proportionate share of the necessary infrastructure improvements; and • The site (or portions of it) could remain undeveloped. 	<ul style="list-style-type: none"> • Commercial (10-30 units per acre) <p>The Regional Center is only calculated at 10 units per acre even though the max density allows for 60 units per acre.</p> <p>The capital facility providers have based their general capital facility plans on these assumptions. Specific projects are required to provide mitigation for project specific impacts which accounts for any variations from the general assumptions.</p> <p>In 2018 the UVC went through a County sponsored comprehensive plan amendment. One of the changes was increasing the max density from 30 units per acre to unlimited. County Staff did not prepare a land capacity analysis, public facilities analysis, or compatibility analysis as part of their proposal or Staff Report and therefore deemed that neighborhood compatibility, public facilities and utility availability could be addressed at the project specific permit level. (See 2018 CPA-Kingston UVC Staff Report – Rev1Final and 2018 CPA-Kingston UVC SEPA Checklist and 2018 CPA Kingston UVC – Staff Report – Attachment B1 DNS FINAL).</p>
<p><i>The proposed amendment is consistent with the balance of the goals, policies and objectives of the Kitsap County Comprehensive Plan and reflects the local circumstances of the county;</i></p>	
<p>Non-Project Specific Staff Analysis: As explained elsewhere in this Staff Report, the proposed amendment does not appear consistent with the following goals and policies of the Kitsap County Comprehensive Plan and does not reflect the local circumstances of the county.</p>	<p>The Staff Report is chock-full of inconsistent conclusions supporting the basis of denial.</p>

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<p>Non-Project Specific Land Use Goal 10. Maintain sufficient industrial land area in the Urban Growth Areas for future industrial use.</p>	<p>This goal is a subjective policy statement. "Sufficient industrial land area" is not quantified or measured. The UGA was sized appropriately for jobs and population and aligns better with County goals after the Comprehensive Plan Amendment. Furthermore, the LCA assumed the majority of the project as developed or underutilized, as opposed to vacant, and therefore accounted for little future industrial growth on the proposed site.</p>
<p>Project Specific Transportation Policy 8. Preserve the county's existing aviation facilities such that they are able to retain and augment their role in the regional, national and international transportation system. Cooperate with entities within the county to establish an air transportation system appropriate to serve the residents, businesses and military activity within the community.</p>	<p>Air traffic is minimal. It is primarily reduced to the surrounding "through the fence" homeowners. WSDOT describes "through the fence" as "In rare cases, additional properties with physical connections to the airport and aviation-dependent activity are included in the footprint." These properties are not part of the airport and not available for public use (Applicant Apex Airport Exhibit). On review of the County's Parcel Search Map it appears that only three remaining parcels abutting the runway are undeveloped further supporting the argument that air traffic is not likely to increase.</p> <p>Expansion is unlikely. The majority of adjacent properties (including the private ownership) are zoned Rural Residential which does not allow for support services associated with other general aviation airports.</p> <p>Applicant can record a notice to title regarding the Airport. This is an acceptable mitigation in the WSDOT Airports and Compatible Land Use Guidebook.</p> <p>Any potential impacts to the airport can be mitigated at the project-specific permit review level.</p>
<p>Project Specific Transportation Policy 16. Ensure that necessary rights-of-way for transportation improvements will be obtained prior to or at the time of up-zoning property.</p>	<p>Kitsap County Code does not require a connection. The Fire Marshal may require the project to be fire sprinkled if a second fire access is not provided. The second access could be met by connecting to Willamette-Meridian, Dickey Rd, Chagnon Rd, or Apex Rd. These requirements will be reviewed at the project-specific permit level. Rights-of-way requirements are typically reviewed at the project specific level and KCC 16.24.040.3 addresses requirements for dedication of public rights-of-way during the subdivision process.</p> <p>Staff contends that the Applicant "proposed" a spine road. Applicant submitted documents included a potential emergency access road. No project specific proposals have been made by the Applicant. Project concepts and schematics were provided as visual reference only</p>

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	and determinations regarding roads and other project specific components will be determined at the project specific permitting level.
<p>Non-Project Specific Economic Development Policy 3. Provide a diverse mix and appropriate range of commercial, industrial and business land uses that will encourage economic activity capable of providing living-wage jobs and reasonably scaled to the needs of the community.</p>	The proposed Comprehensive Plan Amendment supports this policy. The amendment will provide a diverse mix of appropriate commercial and business land uses and encourage economic activity capable of providing living-wage jobs that are reasonably scaled to the needs of the community including walkable amenities to the residential neighborhoods on the subject property as well as the surrounding neighborhoods.
<p>Non-Project Specific Economic Development Policy 7. Encourage full utilization and development of industrially and commercially zoned areas.</p>	The proposed Comprehensive Plan Amendment supports this policy. The mine cannot be fully utilized, the resource has been depleted. The property must be reclaimed to be useable. The amendment will fully utilize the development of commercially zoned areas.
<p>Non-Project Specific Economic Development Policy 8. Promote revitalization within existing developed industrial and commercial areas.</p>	The proposed Comprehensive Plan Amendment supports this policy. The majority of the site was considered developed or underutilized in the Land Capacity Analysis and therefore is considered revitalization. This is a financially viable option to revitalize the existing developed area.
<p>Project Specific CapF and Utilities Policy 7. Apply the Urban Industrial designation in areas most conducive to industrial development; e.g., few or no natural limitations to development, reasonable accessibility to major streets and highways, available services and facilities.</p>	<p>See Staff Report page 14 which states: The site contains the following mapped environmental constraints (see Maps 3A and 3B in Attachment A and other maps as noted below):</p> <ul style="list-style-type: none"> • Wetlands • Note: Wetlands are not mapped in the County GIS system, but site assessment maps are provided in Attachment C3. • Streams (Type F and Type N) • Little Anderson Creek flows to Hood Canal and contains ESA listed summer chum and steelhead. • Strawberry Creek flows to Dyes Inlet and contains ESA listed steelhead. • Note: The stream flowing north actually extends south into the site approximately only one-third of the distance mapped. • Geohazards • High Hazard Area (Liquefaction) • Moderate Hazard Area (Erosion)

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	<ul style="list-style-type: none"> • Critical Aquifer Recharge Areas (Category I and II) <p>The Staff Report concurs that “...compliance with existing code is likely possible and would avoid or mitigate impacts, though future project-specific limitations may be necessary for certain types of industrial or commercial uses to minimize risks to the critical aquifer recharge areas.”</p> <p>Limitations and impacts can and will be reviewed and the project specific level.</p>
<p>Non-Project Specific Silverdale Goal 1. Provide sufficient capacity within the Urban Growth Area to properly accommodate a mix of residential, commercial, and industrial development to meet the extended population and employment projections for Silverdale.</p>	<p>The proposed Comprehensive Plan Amendment supports this policy. This amendment accommodates a mix of residential and commercial development to meet the extended population and employment projections for Silverdale. The Land Capacity Analysis better aligns with County targets after the Comprehensive Plan Amendment both in the UGA and Countywide. The proposal provides opportunity for walkable commercial amenities to the residential neighborhoods on the subject property as well as the surrounding neighborhoods.</p>
<p>Non-Project Specific Silverdale Policy 1. Monitor land supply over time to ensure a continued adequate supply of residential, commercial and industrial designated land to meet Silverdale’s population and employment targets and to meet the needs of unexpected growth.</p>	<p>The proposed Comprehensive Plan Amendment supports this policy. This amendment will help meet the needs of Silverdale’s population and employment targets (See page 21 of the Staff Report) by increasing the employment capacity from 3.8% below the adopted employment target to 1% above the adopted employment target and increasing the population target from 0.3% above the adopted target to 4.7% above the adopted target which is still within the acceptable error margin of +/-5%.</p> <p>Kitsap County, and the entire region, is experiencing a massive housing shortage and the land supply must be adjusted to provide relief. The Land Capacity Analysis calculates that all unincorporated UGA’s in Kitsap County have a deficit of population capacity except for Bremerton (+1%) and Silverdale (0%). Countywide there is a population deficit of 1714. With the increased demand and the current deficit, it is imperative that the County create some additional capacity. This proposal is a great place to start.</p> <p>The Silverdale UGA has an employment deficit of -4%. The increased jobs created by the proposal will create surplus capacity for the Silverdale area.</p>
<p>Non-Project Specific</p>	<p>The proposed Comprehensive Plan Amendment supports this policy. The amendment will provide for a balanced and complete community with a mix of homes and businesses and</p>

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Silverdale Policy 2. In establishing and modifying land use designations, provide for a balanced and complete community.	services to support the residents. The Land Capacity Analysis remains in balance both in the Silverdale UGA and Countywide UGA after the Comprehensive Plan Amendment.
<p>Non-Project Specific</p> <p>Silverdale Policy 14. Encourage full use and development of designated commercial and industrial areas prior to expanding those areas. Promote revitalization within existing developed areas to take advantage of the investment in existing buildings and infrastructure.</p>	<p>The proposed Comprehensive Plan Amendment supports this policy. This amendment seeks to promote revitalization within the existing developed areas. The applicant is not requesting to expand the UGA. The proposal promotes development within the existing UGA which is the number one doctrine of the Growth Management Act. RCW 36.70A.020 - Planning goals (1) Urban growth. Encourage development in urban areas where adequate public facilities and services exist or can be provided in an efficient manner.</p>
<p><i>The amendment will increase density and use urban land more efficiently within the Urban Growth area. The subject parcel(s) is suitable for the requested land use designation based upon, but not limited to, access, provision of utilities, consistency with existing and planned uses, environmental constraints and compatibility with the neighborhood;</i></p>	
<p>Non-Project Specific</p> <p>Staff Analysis: Based on the analysis below, the subject parcels appear better suited for the existing Industrial zoning than the proposed Urban Low Residential (UL; 5-9 DU/Ac) and Neighborhood Commercial (NC; 10-30 DU/Ac) zoning.</p>	<p>The Staff Report is chock-full of inconsistent conclusions supporting the basis of denial.</p>
<p>Non-Project Specific</p> <p><u>Density</u></p> <p>A change from industrial to residential and mixed-use zoning would increase the allowed density from zero dwelling units per acre (0 DU/Ac) across the entire site to 5-9 DU/Ac in the proposed Urban Low Residential (UL) zone and to 10-30 DU/Ac in the proposed Neighborhood Commercial (NC) zone (see Table 3 below). Per KCC 17.410, the minimum required number of dwellings would be 126</p>	<p>This is a non-project action. There are no specific uses or proposals at this time. Conceptual drawings were provided as visual aids to provide visual reference for what a development could look like. The concepts provided were not to scale, not vetted by engineers or surveyors, environmental consultants nor reviewed by County staff for compliance with applicable codes. The Comprehensive Plan Amendment must be reviewed as a non-project action.</p> <p>Making assumptions based on the maximum allowed density is irresponsible and misleading. The Land Capacity Analysis methodology must remain consistent across all applications. The Silverdale Regional Center is approximately 660 acres in size. It allows up to 60 units per acre. If analyzed in the same manner as this amendment then it has the potential to create</p>

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<p>units and the maximum allowed number of dwellings would be 2,904 units for the proposed zones (see Attachment C16). For comprehensive planning purposes, the County’s current land capacity methodology would estimate only 151 dwellings for the proposed UL zone and zero dwellings for the proposed NC zone (see Attachment C15). Given the applicants stated intent to develop mixed-use neighborhoods in the proposed NC zone and the large size of the proposed NC zone, the current land capacity methodology thus likely underestimates the number of future dwellings on the site.</p>	<p>39,600 units (71,280 population). If all of these units were developed, then they would exceed the entire Silverdale UGA planning capacity by 814%.</p> <p>Kitsap County utilizes general assumptions for planning purposes. For instance, the Land Capacity Analysis (LCA) does not attribute any population to the following commercial zones:</p> <ul style="list-style-type: none"> • Urban Village Center (10 units per acre minimum with no max) • Commercial (10-30 units per acre) • Neighborhood Commercial (10-30 units per acre) <p>The Regional Center is only calculated 10 units per acre even though the max density allows for 60 units per acre.</p> <p>The capital facility providers have based their general capital facility plans on these assumptions. Specific projects are required to provide mitigation for project specific impacts which accounts for any variations from the general assumptions.</p> <p>In 2018 the UVC went through a County sponsored comprehensive plan amendment. One of the changes was increasing the max density from 30 units per acre to unlimited. County Staff did not prepare a land capacity analysis, public facilities analysis, or compatibility analysis as part of their proposal or Staff Report and therefore deemed that neighborhood compatibility, public facilities and utility availability could be addressed at the project specific permit level. (See 2018 CPA-Kingston UVC Staff Report – Rev1Final and 2018 CPA-Kingston UVC SEPA Checklist and 2018 CPA Kingston UVC – Staff Report – Attachment B1 DNS FINAL).</p>
<p>Non-Project Specific <u>Employment Capacity</u> The current land capacity methodology estimates the proposed change from industrial zoning to the NC zone would increase the employment capacity of the site by 19 jobs (see Attachment Cx). This is due, in part, to the higher assumed number of jobs for commercial zoning than industrial zoning. This is also due, in part, to the current method’s</p>	<p>According to the County’s own methodology the employment figures better align with the County’s target goals after the proposed amendment. This is a non-project action. There are no specific uses or proposals at this time. Conceptual drawings were provided as visual aids to provide visual reference for what a development could look like. The concepts provided were not to scale, not vetted by engineers or surveyors, environmental consultants nor reviewed by County staff for compliance with applicable codes. The Comprehensive Plan Amendment must be reviewed as a non-project action.</p> <p>Kitsap County utilizes general assumptions for planning purposes. For instance, the Land Capacity Analysis (LCA) does not attribute any population to the following commercial zones:</p>

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<p>assumption that only commercial development will occur in the NC zone with no residential component. As discussed above, with the applicant’s stated intent for development, this is unlikely and therefore the employment estimate is likely overestimated.</p>	<ul style="list-style-type: none"> • Urban Village Center (10 units per acre minimum with no max) • Commercial (10-30 units per acre) • Neighborhood Commercial (10-30 units per acre) <p>The Regional Center is only calculated 10 units per acre even though the max density allows for 60 units per acre.</p> <p>The capital facility providers have based their general capital facility plans on these assumptions. Specific projects are required to provide mitigation for project specific impacts which accounts for any variations from the general assumptions.</p> <p>In 2018 the UVC went through a County sponsored comprehensive plan amendment. One of the changes was increasing the max density from 30 units per acre to unlimited. County Staff did not prepare a land capacity analysis, public facilities analysis, or compatibility analysis as part of their proposal or Staff Report and therefore deemed that neighborhood compatibility, public facilities and utility availability could be addressed at the project specific permit level. (See 2018 CPA-Kingston UVC Staff Report – Rev1Final and 2018 CPA-Kingston UVC SEPA Checklist and 2018 CPA Kingston UVC – Staff Report – Attachment B1 DNS FINAL).</p>
<p>Non-Project Specific <u>Efficient Land Use</u> The currently approved mine reclamation plan for the proposed site is very basic (consistent with the age of the current surface mining permit) and assumes a general future industrial development rather than anticipating a particular future development scenario consistent with contemporary mine reclamation planning. A revised and more specific reclamation plan, as required by KCC 17.170.065, would likely make any future development, whether the existing industrial use or the proposed residential and</p>	<p>The County contends that a revised more specific reclamation plan would likely make any future development, whether the existing industrial use or the proposed residential and commercial uses more efficient and less expensive, but provides no prepared budgets, market data or proformas to validate their statement. This is an anecdotal assumption by staff.</p>

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commercial uses, more efficient and less expensive.	
<p>Project Specific <u>Access</u> At the time of project-level application, the International Fire Code (IFC D106.1) and the Kitsap County Road Standards (Section 4.1.k) will require the future development of this site to have two access points because it will have more than 100 dwellings or more than 1,000 average daily trips. Alternatively, all structures would have to have sprinkler systems if only one access were provided. The Kitsap County Fire Marshall's office has stated one access may be gated for emergency vehicle use only provided a boulevard-style entrance is provided at the second access. The applicant has proposed a new spine road connection between Dickey Road and Willamette-Meridian Road, which was considered by the Kitsap County Public Works Department, Kitsap Transit, and the Central Kitsap Fire and Rescue District during review of this proposed amendment. The County Engineer has stated that the new spine road should be public to provide connectivity within the UGA consistent with the Comprehensive Plan¹. The applicant, however, has not yet been able to demonstrate that they can provide a second access at Willamette-Meridian Road that will comply with the access and connectivity requirements discussed above.</p>	<p>This is a non-project action. There are no specific uses or proposals at this time. Conceptual drawings were provided as visual aids to provide visual reference for what a development could look like. The concepts provided were not to scale, not vetted by engineers or surveyors, environmental consultants nor reviewed by County staff for compliance with applicable codes. The Comprehensive Plan Amendment must be reviewed as a non-project action.</p> <p>As stated in the Staff Report, access will be reviewed at the project specific level.</p>

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<p>Project Specific <u>Utilities</u> All utilities are available at the site, including sewer, water, power, and natural gas. As discussed above in Section 4.B.1.a and in Attachment C9, some of these utilities are expected to need capacity improvements in order to serve the estimated population and employment of the proposed site when developed.</p>	<p>As noted on page 9 of this document as well as pages 9-10 of the Staff Report, providers anticipate, that these deficiencies can be addressed through future project-level mitigation. These same deficiencies may be triggered if the project were developed as industrial because the Capital Facility plan would have considered the majority of this property developed or underdeveloped.</p>
<p>Non-Project Specific <u>Consistency with Existing and Planned Uses</u> Once the proposed site is reclaimed and the MRO designation is removed, there are no existing uses on the site that would be incompatible with the proposed future residential and commercial uses. The proposed site contains a power easement with high voltage powerlines as well as the following legacy uses:</p> <ul style="list-style-type: none"> • An exhausted sand and gravel mine that has not been reclaimed. • Two stormwater ponds associated with the mine. • Partially completed infrastructure (i.e. grading and utilities) for an abandoned industrial development off Dickey Rd. <p>Nevertheless, based on consultations with the Kitsap Economic Development Alliance, the County’s economic development organization, the existing Industrial zone appears to better</p>	<ul style="list-style-type: none"> • An exhausted sand and gravel mine that has not been reclaimed. <ul style="list-style-type: none"> • Gravel mine can be reclaimed per KCC 17.170.060 and 17.170.065 and is planned to be reclaimed with the development of the rezoned property. • Two stormwater ponds associated with the mine. <ul style="list-style-type: none"> • The ponds will be addressed in a reclamation plan and could be an asset to any development as an amenity as well as part of the storm water management methods. • Partially completed infrastructure (i.e. grading and utilities) for an abandoned industrial development off Dickey Rd <ul style="list-style-type: none"> • These will be addressed via a reclamation plan per Code and project specific permit review. <p>Staff Report Exhibit C11 are Staff’s notes from a meeting with KEDA. Additional comments/take-away’s from the meeting include:</p> <ul style="list-style-type: none"> • “Most typically, industrial business looking to locate or relocate in Kitsap County are looking for available space to lease in an existing facility. “ <ul style="list-style-type: none"> • There are no existing facilities on this site. • “The typical industrial business is looking for an existing 40k SF facility on 100k SF site” <ul style="list-style-type: none"> • There are no facilities on this site. • “A recent, but rare, example of a manufacturer looking for a location to develop a large new building with access to natural gas and sewer.” <ul style="list-style-type: none"> • This manufacturer has pursued a property located at PSIC.

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<p>support the suitability and uniqueness of the site for industrial development (see Attachment C11).</p>	<ul style="list-style-type: none"> • “Kitsap County has been slow to grow industrial sectors. Attracting/retaining larger scale operations has been limited.” <ul style="list-style-type: none"> • This further supports the lack of demand for industrially zoned properties, especially those that are undeveloped. <p>Staff ends it’s summary of discussions with the Kitsap Economic Development Alliance with the following quotation “While we, as economic developers, have a proclivity to protect limited industrial sites as future homes to living wage manufacturing type jobs – we certainly recognize that now is an historic situation relative to lack of inventory of affordable – workforce housing units in Kitsap – indeed the entire region – which may provide a solid reason to convert this industrial land at this time - with an awareness that this should not set a precedent relative to rezoning – down-zoning other industrial properties.”</p> <p>Lastly, the County fails to produce any viable market data to back up this claim.</p>
<p>Non-Project Specific Rezoning the proposed site for residential and commercial uses would be a significant change from this long-planned use and would reduce the amount of vacant land in the Industrial (IND) zone in the UGA by 50% and in unincorporated Kitsap County by 36% (see Attachment C12).</p>	<p>The Staff Report contends that the subject properties were assumed developed for the Land Capacity Analysis and were not considered available. This means that removal from the industrial supply would not affect future capacity. Additionally, the numbers are misleading and incomplete due to the lack of inclusion of the Puget Sound Industrial Center. The Land Capacity Analysis projects this property will have only 56 net developable acres as opposed to the 138 gross acres staff used in its Staff Report calculation. The Land Capacity Analysis methodology must remain consistent across all applications.</p>

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<p>Project Specific</p> <p><u>Neighborhood Compatibility</u></p> <p>Adjacent land uses are cataloged in Table 2 below and mapped in Attachment C1. Table 3 below provides a comparison of density and dimension requirements between the existing and proposed zones. Attachment C22 provides a full comparison of allowed uses between the existing and proposed zones. One of the neighboring uses is a general aviation airport. The County is required by state law to discourage the siting of incompatible uses adjacent to general aviation airports (RCW 36.70.547)2. According to the WSDOT Airports and Compatible Land Use Guidebook (January 2011) (see excerpts in Attachment C6):</p> <ul style="list-style-type: none"> • The proposed zoning in the southwest portion of the site is not compatible with the turning zone (compatibility zone 3 in Attachment C5) for aircraft taking off and landing at the Apex Airport. In this area the current IND zoning is more appropriate with some recommended limitations regarding specific types of incompatible uses. • The rest of the proposed UL zone is generally incompatible beneath the traffic pattern (compatibility zone 6 in Attachment C5) of aircraft approaching and departing the airport. Most industrial, commercial, and mixed-use development as well as urban residential development of at 	<p>Airport Concerns</p> <p><u>Apex Airport offers minimal economic contribution.</u> It offers no services (such as fuel, repair services, or supplies), no access to public roads, public transit, rail, or marina. Apex Airport is a Private Public Airport. This means that anyone from the Public can land and takeoff from the runway however they cannot access any of the surrounding properties or community without permission of the private ownership. Any potential amenities are located "through the fence" which WSDOT describes as "In rare cases, additional properties with physical connections to the airport and aviation-dependent activity are included in the footprint." These properties are not part of the airport and not available for public use (Applicant Apex Airport Exhibit). These lack of services limits the use and significance of the airport as compared to other airports in the region.</p> <p><u>Air traffic is minimal.</u> It is primarily reduced to the surrounding "through the fence" homeowners. WSDOT describes "through the fence" as "In rare cases, additional properties with physical connections to the airport and aviation-dependent activity are included in the footprint." These properties are not part of the airport and not available for public use (Applicant Apex Airport Exhibit). On review of the County's Parcel Search Map it appears that only three remaining parcels abutting the runway are undeveloped further supporting that air traffic is not likely to increase.</p> <p><u>Expansion is unlikely.</u> The majority of adjacent properties (including the private ownership) are zoned Rural Residential which does not allow for support services associated with other general aviation airports.</p> <p><u>Applicant can record a notice to title regarding the Airport.</u> This is an acceptable mitigation in the WSDOT Airports and Compatible Land Use Guidebook.</p> <p>WSDOT Airports and Compatible Land Use Guidebook Page v. states <u>"WSDOT does not have regulatory authority over land use decisions; however, we offer a technical assistance program to help towns, cities and counties address aviation issues."</u></p>

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<p>least 15 DU/Ac are considered compatible beneath the traffic pattern of the airport.</p> <ul style="list-style-type: none"> Besides the appropriate underlying zoning, a new zoning overlay is the recommended tool in the WSDOT guidance that would be most compatible with Kitsap County Code to further ensure compatibility with airports. 	<p>WSDOT Airports and Compatible Land Use Guidebook – Table 1-2 Compatibility Concerns Represented by Particular Land Uses – The following table shows that there are concerns even for the existing land use designation of Industrial.</p> <p style="text-align: center;"><small>Table 1-2 Compatibility Concerns Represented by Particular Land Uses</small></p> <table border="1" data-bbox="793 375 1486 656"> <thead> <tr> <th>Land Use Type</th> <th>Compatibility Concerns</th> </tr> </thead> <tbody> <tr> <td>Single-Family Residential</td> <td> <ul style="list-style-type: none"> Noise can be disruptive in outdoor areas as well as indoors with open windows. Aircraft overflight can be annoying, especially where ambient noise levels are low such as in suburban or rural areas. </td> </tr> <tr> <td>Multi-Family Residential</td> <td> <ul style="list-style-type: none"> Noise can be disruptive in outdoor areas as well as indoors with open windows, although less sensitive than for single-family residential. High density presents concern for safety of residents in areas exposed to significant risk of aircraft accidents. </td> </tr> <tr> <td>Industrial Uses</td> <td> <ul style="list-style-type: none"> Smoke, steam, and thermal plumes can be hazards to flight. Tall structures can be airspace obstructions. Possible release of hazardous materials if damaged during an accident. </td> </tr> </tbody> </table> <p>Chapter 2 lists “Retail/Service Uses/Mixed Use” as “compatible” uses within the Turning Zone and Beneath Traffic Patterns area. Residential uses are listed as “incompatible” in the Turning Zone and “generally incompatible” Beneath Traffic Patterns.</p> <p>Chapter 3 provides tools for enhancing compatibility including:</p> <ul style="list-style-type: none"> Clustering of development within zone 6 which most of the subject property falls within. Aviation Disclosure Notices Aviation Easements Real Estate Disclosure Statements <p>Appendix F – Compatibility Criteria provides guidelines for maximum residential densities. It suggests that in urban areas (within the urban growth boundary) Zone 3 should promote “infill development up to average of surrounding residential area is allowed, but is appropriate only if nonresidential uses are not feasible.” It suggests that Zone 6 should “promote high density and intensity mixed-use development (15 or more d.u. per acre).”</p> <p>Table F-2 Airport Land Use Matrix – D. Urban Development – 1. Residential lists single-family dwelling: up to 12 dwelling units/acre as incompatible with Zone 3, however it suggests that in Zone 6 they are “Limited” as “uses or activities that may be compatible with airport operations depending on their location, size, bulk, height, density and intensity of use”. Multi-family dwellings are listed in Zone 3 as “Limited with Special conditions – development should be moved away from the extended runway centerline. Open space</p>	Land Use Type	Compatibility Concerns	Single-Family Residential	<ul style="list-style-type: none"> Noise can be disruptive in outdoor areas as well as indoors with open windows. Aircraft overflight can be annoying, especially where ambient noise levels are low such as in suburban or rural areas. 	Multi-Family Residential	<ul style="list-style-type: none"> Noise can be disruptive in outdoor areas as well as indoors with open windows, although less sensitive than for single-family residential. High density presents concern for safety of residents in areas exposed to significant risk of aircraft accidents. 	Industrial Uses	<ul style="list-style-type: none"> Smoke, steam, and thermal plumes can be hazards to flight. Tall structures can be airspace obstructions. Possible release of hazardous materials if damaged during an accident.
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	<p>should be devoted to areas that experience elevated risk” however multi-family is outrightly “permitted” within Zone 6.</p> <p>In 2018 the UVC went through a County sponsored comprehensive plan amendment. One of the changes was increasing the max density from 30 units per acre to unlimited. County Staff did not prepare a land capacity analysis, public facilities analysis, or compatibility analysis as part of their proposal or Staff Report and therefore deemed that neighborhood compatibility, public facilities and utility availability could be addressed at the project specific permit level. (See 2018 CPA-Kingston UVC Staff Report – Rev1Final and 2018 CPA-Kingston UVC SEPA Checklist and 2018 CPA Kingston UVC – Staff Report – Attachment B1 DNS FINAL).</p> <p>Additional options and considerations are given to a variety of other uses supporting the conclusion that the final determination regarding site specific uses should be achieved at the project-specific permit level. The County maintains these tools and others after the Comprehensive Plan Amendment to ensure that proposed developments are consistent with guidelines and regulations to safeguard compatibility with surrounding uses.</p>
<p>Project Specific</p> <p>Other neighboring uses are industrial. The proposed amendment would thus result in urban residential and mixed-use densities of 5-9 DU/Ac and 10-30 DU/Ac:</p> <ul style="list-style-type: none"> • Adjacent to an existing heavy industrial use (a surface sand and gravel mine with rock crushing, heavy machinery, an asphalt batch plant, and occasional blasting which has a 	<p>Adjacent Industrial Zone Concerns</p> <ul style="list-style-type: none"> • Adjacent to an existing heavy industrial use (a surface sand and gravel mine with rock crushing, heavy machinery, an asphalt batch plant, and occasional blasting which has a planned expansion area with an anticipated operational life of an additional 40-50 years); <ul style="list-style-type: none"> • The existing neighboring uses and potential mitigation will be reviewed at the project specific level for this development. • Across a stream corridor from existing and developing light industrial uses; <ul style="list-style-type: none"> • The Critical Areas Ordinance will dictate how this is developed during a project specific permit review. • Adjacent to undeveloped industrial land with unknown future industrial uses; and

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<p>planned expansion area with an anticipated operational life of an additional 40-50 years);</p> <ul style="list-style-type: none"> • Across a stream corridor from existing and developing light industrial uses; • Adjacent to undeveloped industrial land with unknown future industrial uses; and • Adjacent to rural residential uses. 	<ul style="list-style-type: none"> • It is impossible for the County or the Applicant to mitigate for unknown future uses. Per the Staff Report, this property has been undeveloped since the 1990's which further substantiates that there is a lack of demand for industrial properties within Kitsap County. • Adjacent to rural residential uses. <ul style="list-style-type: none"> • UGA's abut Rural Residential uses and zoning throughout the County, including this entire industrially zoned area and associated UGA as it currently exists (See Applicant Existing Surrounding Uses/Zoning Exhibit and Applicant Exhibit 6-Compatable Uses).
<p>Project Specific</p> <p>The neighboring airport and industrial uses will generate noise and possibly other impacts (e.g. dust, vibrations, light, odors, etc) to the proposed residential and commercial zones. Noise complaints regarding existing industrial uses and airports commonly increase when new residential uses are developed nearby, even when the new residential development occurs on property with recorded notices disclosing the adjacent uses, potential impacts, and limitations on 17.170.0703 nuisance claims like that required by KCC or with avigation easements as discussed in Attachment C6. It is possible that some future industrial uses on industrial zoned parcels could not be feasible or could be denied because of their proximity to residential or commercial zones parcels. Increased setbacks, as required by KCC 17.420.060.A.27 (see below in Table 3), will also reduce the functional acreage of industrial land. Under current Kitsap County Code, development in the new residential and commercial zones</p>	<p><u>Noise Concerns and other impacts to neighboring industrially zoned properties</u></p> <p>The County fails to produce credible data to back-up their claim regarding noise complaints increasing after new residential uses are developed nearby.</p> <p>Staff contends that future industrial uses could be denied because of their proximity to residential or commercially zoned properties. Where does the Code support this statement?</p> <p>Noise concerns can and will be mitigated at the project specific permit review.</p> <p>The adjacent mine is more than 80' deep. This depth buffers a large majority of the noise and other impacts.</p> <p>Mitigation requirements fall as much on the development of the subject parcels as they do the potential expansion of neighboring uses. KCC 17.500.025 requires a minimum of 15% of the site be landscaped. Additionally, KCC 17.500.027 directs buffer sizes based on neighboring uses and zoning. A standard subdivision would be required to provide a Solid Screening Buffer (KCC 17.500.027.B) which requires a minimum of 50-feet when residential zoning abuts industrial zoning or uses. The Director has the authority to increase this requirement depending on the proposed use of the site and adjacent zones and/or uses (KCC 17.500.027).</p> <p>If a Performance Based Development (PBD) were proposed the standards for development on the subject property increase with requirements for both common and recreational open space (KCC 17.450.040B. & C.). Common open space requirements for PBD's is an additional</p>

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<p>would not be required to increase setbacks or implement other measures to improve compatibility with adjacent industrial uses. It should also be noted that a similar proposed amendment (see Attachment C18) in 2006 to rezone a large portion of the proposed site from the Industrial (IND; 0 DU/Ac) and Industrial with Mineral Resource Overlay (MRO/IND; 0 DU/Ac) zones to the Urban Low Residential (UL; 5-9 DU/Ac) zone was denied on the basis of neighborhood compatibility.</p>	<p>15% and additional recreational open space of 5%. This means that if the subject properties were developed utilizing a PBD, a minimum of 35% of the site could be in open space or landscaping. Critical area buffers would also be required around much of the perimeter of the subject parcels. Strategically placing landscaping and open space would be a top priority to the development of this site as it would increase the marketability to the end user.</p> <p>The 2006 submittal included approximately 15 acres of the current proposal (approx. 11%) and the entire proposal was approximately 40 acres. The 2006 proposal differed from the current proposal in the following ways:</p> <ol style="list-style-type: none"> 1) 2006 proposal has only 200' of Dickey Road frontage (this proposal has 1,000'). 2) 2006 proposal was a small sliver (700' wide) of Urban Low in the midst of an industrial area (this proposal is 3,200' wide). 3) 2006 proposal was a much smaller footprint and located on a very steep slope. The current proposal has much more land area and better topography to mitigate any potential compatibility issues. 4) In the 14 years since the denial there has been no development activity on the site which is further evidence of the lack of demand for Industrial property.
<p><i>The proposed amendment does not materially affect the land uses and growth projections which are the basis for the Comprehensive Plan, and reflects local circumstances in the county;</i></p>	
<p>Non-Project Specific Staff Analysis: Based on the County's current land capacity methodology estimates, the amendment will not materially affect the growth projections of the Comprehensive Plan.</p>	<p>Applicant agrees with Staff's analysis that the amendment will not materially affect the growth projects of the Comprehensive Plan.</p>
<p>Non-Project Specific Since several parcels in the proposed site still have an active surface mine permit from the Washington State Department of Natural Resources and have not been reclaimed, they were classified as "developed" in the land capacity analysis for the 2016 Comprehensive</p>	<p>The properties have active mining permits because the cost to maintain the permits vs. the cost of reclamation is considerably less expensive. Maintaining the permits is a cost-effective way to manage the property until a financially viable option becomes available.</p> <p>The subject properties were considered developed during the 2016 LCA which means they were excluded from the employment capacity calculations.</p>

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<p>Plan, which means those parcels were excluded from the calculation of employment capacity for the Silverdale UGA (see the “2016 Baseline” column in Attachment C15). When those parcels are reclaimed and considered available for redevelopment in the County’s land capacity calculations, the current methodology estimates they would increase the 2016 Comprehensive Plan’s employment capacity for the UGA from 3.8% below the adopted employment target to 1% above the adopted employment target (see the “2016 Adjusted Baseline (with Mining removed)” column in Attachment C15).</p>	<p>Staff is stating that the LCA aligns with County goals for employment better after the Comprehensive Plan Amendment. The calculations provided by staff are still within the acceptable margin of error of +/-5%.</p>
<p>Non-Project Specific If the proposed amendment were adopted, the current land capacity methodology estimates the employment capacity of the UGA would increase by 19 jobs to 1.2% above the adopted employment target and the population capacity of the UGA would increase by 377 people (151 homes) from 0.3% above the adopted population target to 4.7% above the adopted population target (see the “Alternative 1 (UL & NC)” column in Attachment C15). Capacity within +/- 5% of the adopted target is considered in balance by the County. Capacity outside this range would trigger the need for adjustments to rebalance the UGA, such as zoning amendments elsewhere inside the UGA, modifications to development regulations, changing the adopted target(s), or possibly resizing the</p>	<p>Staff is stating that the LCA aligns with County goals for employment better after the Comprehensive Plan Amendment. The calculations provided by staff are still within the acceptable margin of error of +/-5%.</p>

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<p>UGA, all of which are beyond the scope of any site-specific amendment and the adopted 2019 docket. In addition of ensuring that the growth capacity of a specific UGA remains in balance with its adopted target, the Comprehensive Plan must also ensure that growth capacity remains in balance Countywide. The current land capacity methodology estimates that Countywide population and employment capacity is in balance with adopted targets with or without the adoption of the proposed amendment (see Attachment C15).</p>	
<p>Non-Project Specific Per allowed densities in KCC 17.420.052 and .054 (see Table 3 above) for the proposed zones, a minimum of 126 dwellings (315 people) and a maximum of 2,904 dwellings (5,227 people) could be proposed for the site (see attachment C16). This is a Comprehensive Plan amendment and not a project-specific development proposal. Capacity estimates are not intended to predict the exact development of a specific site, but rather the general capacity of a large planning area. Since the current land capacity methodology assumes that no population capacity and only employment capacity will be developed in the NC zone, it is possible that this assumption may result in an underestimation of population capacity and an overestimation of employment capacity. The applicant has stated an intent to develop mixed use</p>	<p>Capacity calculations are based off an average of what is allowed in a zone. Making assumptions based on the maximum allowed density is irresponsible and misleading. The Land Capacity Analysis methodology must remain consistent across all applications. The Silverdale Regional Center is approximately 660 acres in size. It allows up to 60 units per acre. If analyzed in the same manner as this amendment then it has the potential to create 39,600 units (71,280 population). If all of these units were developed then they would exceed the entire Silverdale UGA planning capacity by 814%.</p> <p>Kitsap County utilizes general assumptions for planning purposes. For instance, the Land Capacity Analysis (LCA) does not attribute any population to the following commercial zones:</p> <ul style="list-style-type: none"> • Urban Village Center (10 units per acre minimum with no max) • Commercial (10-30 units per acre) • Neighborhood Commercial (10-30 units per acre) <p>The Regional Center is only calculated 10 units per acre even though the max density allows for 60 units per acre.</p> <p>The capital facility providers have based their general capital facility plans on these assumptions. Specific projects are required to provide mitigation for project specific impacts which accounts for any variations from the general assumptions.</p>

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<p>neighborhoods in the NC zone, but this could change in any future development of the site. Minimum and maximum density calculations define the legal limits of potential future development of a specific site, but also are not predictive. The actual level of development is rarely at maximum density, especially when mixed-use is developed.</p>	<p>In 2018 the UVC went through a County Sponsored comprehensive plan amendment. One of the changes was increasing the max density from 30 units per acre to unlimited. A land capacity analysis, public facilities analysis, and compatibility analysis were not prepared or part of the staff report. Based on the Staff Report and the SEPA checklist and Determination this analysis was unnecessary because they were non-project actions. Any specific concerns could be mitigated at a project specific level. (See 2018 CPA-Kingston UVC Staff Report – Rev1Final and 2018 CPA-Kingston UVC SEPA Checklist and 2018 CPA Kingston UVC – Staff Report – Attachment B1 DNS FINAL).</p> <p>This is a Comprehensive Plan Amendment. Conceptual drawings were provided as visual aids to provide visual reference for what a development could look like. The concepts provided were not to scale, not vetted by engineers or surveyors, environmental consultants nor reviewed by County staff for compliance with applicable codes. The Comprehensive Plan Amendment must be reviewed as a non-project action.</p>
<p><i>The proposed amendment does not materially affect the adequacy or availability of urban facilities and services to the immediate area or the overall area of the urban growth area; and</i></p>	

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<p>Project Specific Staff Analysis: A review of urban facilities and services based on the current land capacity methodology estimates found the proposed amendment does not materially affect the adequacy or availability of urban facilities and services to the immediate area or the overall area of the urban growth area. See analysis in Section 4.B.1.a regarding meeting concurrency requirements. Future improvements to address capacity deficiencies may be required during project level review for roads, transit, and schools (see Attachment C9).</p>	<p>The Staff Report states that “Future improvements to address capacity deficiencies may be required during project level review for roads, transit, and schools”.</p> <p>All service providers have been given an opportunity to address any potential deficiencies. The majority of the providers commented and all of those who commented stated that any potential deficiencies could be managed at a project specific level. Furthermore, the subject property is already located inside the Urban Growth Boundary, and the current zoning allows for the most intensive uses. It is possible that many of the capital facility providers will see a reduction in demand due to the change of zoning.</p>
<p><i>The proposed amendment is consistent with the GMA, Kitsap County-wide Planning Policy, state and local laws and other applicable inter-jurisdictional policies or agreements.</i></p>	
<p>Non-Project Specific Staff Analysis: As explained herein and elsewhere in this Staff Report, the proposed amendment does not appear consistent with the GMA industrial land designation criteria guidelines (RCW 365-196-310(4)(c)(iv) – see recital above) as well as the following Countywide Planning Policies. There are no known interjurisdictional agreements.</p>	<p>This requirement states the <i>proposed amendment</i> is consistent with GMA Kitsap County-wide Planning Policy, state and local laws and other applicable inter-jurisdictional policies or agreements.</p> <p>It <i>does not require</i> that the proposed amendment be consistent with the industrial land designation criteria and guidelines.</p> <p>We are not proposing to rezone to industrial, so the industrial guidelines do not apply.</p>
<p>Centers of Growth Policy C. 1: In decisions relating to population growth and resource allocation supporting growth, Centers have a high priority.</p>	<p>Other than the County’s statement that “The proposed site is outside of these designated centers and would likely reduce demand for housing and employment within the centers.” where is the data to support this statement?</p>

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	<p><u>The change is insignificant.</u> The Land Capacity Analysis (Staff Report Exhibit C15) assumes only 151 additional population would be created from the proposal compared to the population capacity of 8,753 in the Silverdale UGA (less than 4.5% change) and population capacity of 31,837 in the Countywide UGA (less than 1.5% change). The Land Capacity Analysis (Staff Report Exhibit C15) assumes only 18 additional jobs would be created from the proposal compared to the employment capacity of 8,592 in the Silverdale UGA (less than 0.5% change) and employment capacity of 14,200 in the Countywide UGA (less than 0.2% change).</p> <p><u>Not all citizens want to live in high density, stacked housing.</u> Over 85% of Central Puget Sound Residents live in single family detached homes (Figure 27 Vision 2050). The Silverdale Regional Center only allows multifamily and single-family attached housing which are both conditional uses. The Neighborhood Commercial and Urban Low Residential zones allow for Duplex, Multifamily, Single-Family Attached, Single-Family Detached, Accessory Dwelling Units, Accessory Dwelling Quarters and Cottage Housing. All of these uses are permitted with the exception of Cottage Housing which is a conditional use.</p> <p>The Land Capacity Analysis (LCA) projects capacity for only 666 units in the Regional Center over the entire planning period (24 years). Development will be necessary outside of the Regional Center to adequately provide for housing needs.</p>
<p>Project Specific Public Capital Facilities Policy 4: Air transportation facilities in Kitsap County: a. The Counties and the Cities shall recognize the importance of airports as essential public facilities and the preservation of access to the air transportation system. b. The County and the Cities shall ensure the safety of the community and airport users through compatible land use planning adjacent to airports and coordination of the airport with ground access. Examples would include not encouraging or supporting higher</p>	<p><u>Airport Concerns</u> <u>Apex Airport offers minimal economic contribution.</u> It offers no services (such as fuel, repair services, or supplies), no access to public roads, public transit, rail, or marina. Apex Airport is a Private Public Airport. This means that anyone from the Public can land and takeoff from the runway however they cannot access any of the surrounding properties or community without permission of the private ownership. Any potential amenities are located "through the fence" which WSDOT describes as "In rare cases, additional properties with physical connections to the airport and aviation-dependent activity are included in the footprint." These properties are not part of the airport and not available for public use (Applicant Apex Airport Exhibit). These lack of services limits the use and significance of the airport as compared to other airports in the region.</p>

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<p>residential densities, schools, or hospitals near airports or airport approach corridors.</p> <p>c. The County and the Cities shall plan for heliports throughout Kitsap County for emergency use.</p>	<p><u><i>Air traffic is minimal.</i></u> It is primarily reduced to the surrounding "through the fence" homeowners. WSDOT describes "through the fence" as "In rare cases, additional properties with physical connections to the airport and aviation-dependent activity are included in the footprint." These properties are not part of the airport and not available for public use (Applicant Apex Airport Exhibit). On review of the County's Parcel Search Map it appears that only three remaining parcels abutting the runway are undeveloped further supporting the argument that air traffic is not likely to increase.</p> <p><u><i>Expansion is unlikely.</i></u> The majority of adjacent properties (including the private ownership) are zoned Rural Residential which does not allow for support services associated with other general aviation airports.</p> <p><u><i>Applicant can record a notice to title regarding the Airport.</i></u> This is an acceptable mitigation in the WSDOT Airports and Compatible Land Use Guidebook.</p> <p><u>WSDOT Airports and Compatible Land Use Guidebook Page v. states "WSDOT does not have regulatory authority over land use decisions; however, we offer a technical assistance program to help towns, cities and counties address aviation issues."</u></p> <p>WSDOT Airports and Compatible Land Use Guidebook – Table 1-2 Compatibility Concerns Represented by Particular Land Uses – The following table shows that there are concerns even for the existing land use designation of Industrial.</p> <p style="text-align: right;"><small>Table 1-2 Compatibility Concerns Represented by Particular Land Uses</small></p> <table border="1" data-bbox="789 1052 1486 1331"> <thead> <tr> <th data-bbox="789 1052 1031 1073">Land Use Type</th> <th data-bbox="1037 1052 1486 1073">Compatibility Concerns</th> </tr> </thead> <tbody> <tr> <td data-bbox="789 1078 1031 1149">Single-Family Residential</td> <td data-bbox="1037 1078 1486 1149"> <ul style="list-style-type: none"> • Noise can be disruptive in outdoor areas as well as indoors with open windows. • Aircraft overflight can be annoying, especially where ambient noise levels are low such as in suburban or rural areas. </td> </tr> <tr> <td data-bbox="789 1154 1031 1243">Multi-Family Residential</td> <td data-bbox="1037 1154 1486 1243"> <ul style="list-style-type: none"> • Noise can be disruptive in outdoor areas as well as indoors with open windows, although less sensitive than for single-family residential. • High density presents concern for safety of residents in areas exposed to significant risk of aircraft accidents. </td> </tr> <tr> <td data-bbox="789 1248 1031 1331">Industrial Uses</td> <td data-bbox="1037 1248 1486 1331"> <ul style="list-style-type: none"> • Smoke, steam, and thermal plumes can be hazards to flight. • Tall structures can be airspace obstructions. • Possible release of hazardous materials if damaged during an accident. </td> </tr> </tbody> </table>	Land Use Type	Compatibility Concerns	Single-Family Residential	<ul style="list-style-type: none"> • Noise can be disruptive in outdoor areas as well as indoors with open windows. • Aircraft overflight can be annoying, especially where ambient noise levels are low such as in suburban or rural areas. 	Multi-Family Residential	<ul style="list-style-type: none"> • Noise can be disruptive in outdoor areas as well as indoors with open windows, although less sensitive than for single-family residential. • High density presents concern for safety of residents in areas exposed to significant risk of aircraft accidents. 	Industrial Uses	<ul style="list-style-type: none"> • Smoke, steam, and thermal plumes can be hazards to flight. • Tall structures can be airspace obstructions. • Possible release of hazardous materials if damaged during an accident.
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Staff Report Rational and/or Analysis	Applicant Response
	Additional options and considerations are given to a variety of other uses supporting the conclusion that the final determination regarding site specific uses should be achieved at the project-specific permit level. The County maintains these tools and others after the Comprehensive Plan Amendment to ensure that proposed developments are consistent with guidelines and regulations to safeguard compatibility with surrounding uses.
<p>Non-Project Specific Countywide Economic Development Policy J.2: The role of government agencies in assuring coordinated, consistent efforts to promote economic vitality and equity throughout Kitsap County:</p> <p>a. The County and the Cities shall promote Urban Growth Areas and existing industrial sites as centers for employment.</p>	More jobs are allocated to the Neighborhood Commercial (NC) zone than to the Industrial zone (IND). A change in zoning = an increase in jobs. How many jobs is this developed industrial site creating now? How many jobs has this developed industrial site created in the past 20-years?
<p>Non-Project Specific b. The County and the Cities shall encourage the full utilization/development of designated industrial and commercial areas. The County and the Cities shall promote revitalization within existing developed industrial and commercial areas to take advantage of the significant investments in existing buildings and infrastructure.</p>	This Comprehensive Plan Amendment proposes to fully utilize and develop commercial areas as required within County policy. The Amendment proposes to revitalize an existing developed site.
<p>Non-Project Specific e. The County and the Cities shall collaborate with the KEDA and the Ports to establish a common method to monitor the supply of designated commercial and industrial sites and to ensure adequate land supply for the expansion of existing enterprises and the establishment of new economic enterprises. The monitoring method shall indicate</p>	The Counties own Land Capacity Analysis (LCA) shows the proposed amendment achieves this policy. This rationale is included in the Staff Report and is an argument for, not against the proposed amendment.

Comment 2.47D

<i>Staff Report Rational and/or Analysis</i>	<i>Applicant Response</i>
environmental constraints, infrastructure availability and capacity, and shall use the Kitsap County Geographic Information System and Land Capacity Analysis as a regional database for this information.	

Peter Best

From: John Powers <powers@kitsapeda.org>
Sent: Tuesday, September 3, 2019 3:32 PM
To: Peter Best; Kathy Cocus
Subject: RE: Dickey Road Industrial Area & Long-Term Industrial Supply

Thanks for the nudge Peter.

In general your notes reflect our discussions and our comments; however, there are a couple of potential modifications I would note. A request for 50 plus acres for development is atypical (not common) we just happened to have received an inquire recently that is a mega-project looking for 50 plus acres to site a million square feet of facilities (a rare bird so to speak).

Typical industrial / manufacturing inquire is looking for an existing 40K sf facility on 100K sf site. While we, as economic developers, have a proclivity to protect limited industrial sites as future homes to living wage manufacturing type jobs – we certainly recognize that now is an **historic** situation relative to lack of inventory of affordable – workforce housing units in Kitsap – indeed the entire region – which may provide a solid reason to convert this industrial land at this time - with an awareness that this should not set a precedent relative to rezoning – down-zoning other industrial properties.

Best,

John

From: Peter Best <pbest@co.kitsap.wa.us>
Sent: Tuesday, September 3, 2019 3:15 PM
To: John Powers <powers@kitsapeda.org>; Kathy Cocus <cocus@kitsapeda.org>
Subject: RE: Dickey Road Industrial Area & Long-Term Industrial Supply

John and Kathy:

I didn't hear back from you and wanted to give you one last chance to review the attached notes.

Since I have confirmed that Cascade Natural Gas does have a gas pipeline running up Dickey Rd and serves much of the County (Poulsbo and areas south), including the Puget Sound Industrial Center. The have an office in Bremerton.

From: Peter Best
Sent: Tuesday, July 16, 2019 7:39 PM
To: John Powers <powers@kitsapeda.org>; Kathy Cocus <cocus@kitsapeda.org>
Subject: RE: Dickey Road Industrial Area & Long-Term Industrial Supply

John and Kathy:

Thanks again for taking the time to meet with me regarding the Dickey Rd industrial area. As promised, attached are my notes for your review. Please feel free to offer corrections/revisions/additions. It would be much appreciated if you could provide you feedback by Thursday (7/18) morning.

Since we were running short on time, I did not introduce you to the work we are beginning with the Cities, tribes, and Navy regarding buildable lands and land capacity analysis programs. We'll have to discuss that at a future date.

Sincerely,

Peter

Peter Best | Senior Planner
Kitsap County Department of Community Development
[Planning and Environmental Programs](#)
619 Division St, MS 36
Port Orchard, WA 98366
(360) 337-7098 | pbest@co.kitsap.wa.us

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From: John Powers <powers@kitsapeda.org>
Sent: Monday, July 15, 2019 1:16 PM
To: Peter Best <pbest@co.kitsap.wa.us>
Cc: Kathy Cocus <cocus@kitsapeda.org>
Subject: RE: Dickey Road Industrial Area & Long-Term Industrial Supply

Thanks, Peter – Kathy Cocus and I will see you tomorrow @ 3:00 PM @ our office.

John

From: Peter Best <pbest@co.kitsap.wa.us>
Sent: Monday, July 15, 2019 1:07 PM
To: John Powers <powers@kitsapeda.org>
Subject: RE: Dickey Road Industrial Area & Long-Term Industrial Supply

John,

Attached is another set of general maps for the site-specific amendment in the Dickey Road area. Looking forward to discussing this with you tomorrow at 3pm in your office.

Sincerely,
Peter

Peter Best | Senior Planner
Kitsap County Department of Community Development
[Planning and Environmental Programs](#)
619 Division St, MS 36
Port Orchard, WA 98366
(360) 337-7098 | pbest@co.kitsap.wa.us

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From: Peter Best
Sent: Friday, July 12, 2019 4:40 PM
To: John Powers <powers@kitsapeda.org>
Subject: Dickey Road Industrial Area & Long-Term Industrial Supply

John,

Thanks for the brief phone conversation today. I am available to have a more in-depth conversation Monday morning or anytime on Tuesday. Since my commute takes me through Silverdale, I could also drop into your office around 9am or 4:30pm on Tuesday.

As I said on the phone, there is a short-term item and a long-term item to discuss relative to industrial (and commercial) land supply. I have copied several relevant policies below and highlighted the policies most relevant to the interaction of our organizations.

The short-term issue is a site-specific application to redesignate 138 acres of land off Dickey Road from Urban Industrial to Urban Low-Density Residential (with 2 alternatives also having some mix of Neighborhood Commercial). Attached is a 1-page summary and some maps regarding this application. There has been recent industrial development along Willamette-Meridian Road and I noticed this area is identified in your [industrial park map](#). The Dickey Road area is the second largest industrially designated area in the unincorporated County. So I need to make sure I am looking carefully at compliance with our industrial land policies as I prepare a staff report regarding this application. My office is producing some updated analysis regarding land inventory and coordinating with service providers regarding capital facilities. I am hoping KEDA can provide some context regarding current and future demand (or lack of it) for industrial land in Kitsap generally and perhaps this type of land and this location more specifically.

The long-term issue is related to our buildable lands/land capacity analysis requirements under the Growth Management Act which are used as the basis for the 20-year Comprehensive Plans of the County and the cities. The County and Cities are beginning to update our program to implement these requirements (which were expanded by 2017 legislation) and related Countywide Planning Policies. I would like to introduce you to what we are doing and discuss possible next steps.

Kitsap Countywide Planning Policies

Centers of Growth Policy C.2.c.iii:

Activity and Employment Centers are areas of concentrated employment and are a magnet for significant numbers of people usually during daytime hours because of business and/or manufacturing activities. They may be located outside of Urban Growth Areas, consistent with the Growth Management Act. Industrial and business parks and Navy employment centers are in this category. Within Urban Growth Areas, the opportunity to include a proportional residential element should be determined on a case-by-case basis, considering the unique geography and economics of the area.

- Note: The Dickey Road industrial area is not currently designated as a center.

Countywide Economic Development Policy J.1.e:

Local governments are encouraged to utilize the Kitsap Economic Development Alliance (KEDA) as a resource to provide advice on economic development needs, the potential for retaining and expanding existing industries, including the U.S. Dept. of Defense, and attracting new industries, especially those that would improve wage and salary levels, increase the variety of job opportunities, and utilize the resident labor force.

Countywide Economic Development Policy J.2:

The role of government agencies in assuring coordinated, consistent efforts to promote economic vitality and equity throughout Kitsap County:

- a. The County and the Cities shall promote Urban Growth Areas and existing industrial sites as centers for employment.
- b. The County and the Cities shall encourage the full utilization/development of designated industrial and commercial areas. The County and the Cities shall promote revitalization within existing developed industrial and commercial areas to take advantage of the significant investments in existing buildings and infrastructure.

...

e. The County and the Cities shall collaborate with the KEDA and the Ports to establish a common method to monitor the supply of designated commercial and industrial sites and to ensure adequate land supply for the expansion of existing enterprises and the establishment of new economic enterprises. The monitoring method shall indicate environmental constraints, infrastructure availability and capacity, and shall use the Kitsap County Geographic Information System and Land Capacity Analysis as a regional database for this information.

Kitsap County Comprehensive Plan Policies

Land Use Goal 10. Maintain sufficient industrial land area in the Urban Growth Areas for future industrial use.

Economic Development Policy 3. Provide a diverse mix and appropriate range of commercial, industrial and business land uses that will encourage economic activity capable of providing living-wage jobs and reasonably scaled to the needs of the community.

Economic Development Policy 7. Encourage full utilization and development of industrially and commercially zoned areas.

Economic Development Policy 8. Promote revitalization within existing developed industrial and commercial areas.

Economic Development Strategy 1 – Business Climate (Supports policies 1-30)

- **Partnerships: In partnership with other jurisdictions, ports and agencies, review Buildable Lands Report in terms of targeted industry sector growth and utilize land use map for future growth, development and re-development.**

CapF and Utilities Policy 7. Apply the Urban Industrial designation in areas most conducive to industrial development; e.g., few or no natural limitations to development, reasonable accessibility to major streets and highways, available services and facilities.

Silverdale Goal 1. Provide sufficient capacity within the Urban Growth Area to properly accommodate a mix of residential, commercial, and industrial development to meet the extended population and employment projections for Silverdale.

Silverdale Policy 1. Monitor land supply over time to ensure a continued adequate supply of residential, commercial and industrial designated land to meet Silverdale's population and employment targets and to meet the needs of unexpected growth.

Silverdale Policy 14. Encourage full use and development of designated commercial and industrial areas prior to expanding those areas. Promote revitalization within existing developed areas to take advantage of the investment in existing buildings and infrastructure.

Sincerely,
Peter

Peter Best | Senior Planner
Kitsap County Department of Community Development
[Planning and Environmental Programs](#)
619 Division St, MS 36
Port Orchard, WA 98366
(360) 337-7098 | pbest@co.kitsap.wa.us

Comment 2.47D

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KITSAP COUNTY RENTAL TRENDS

Here's look at rental trends in Kitsap County based on surveys of complexes with 50 or more units. All numbers are courtesy [Apartment Insights](#).

 **\$1,323**

Average rent per unit

 **10.7%**

Rent increase from previous year

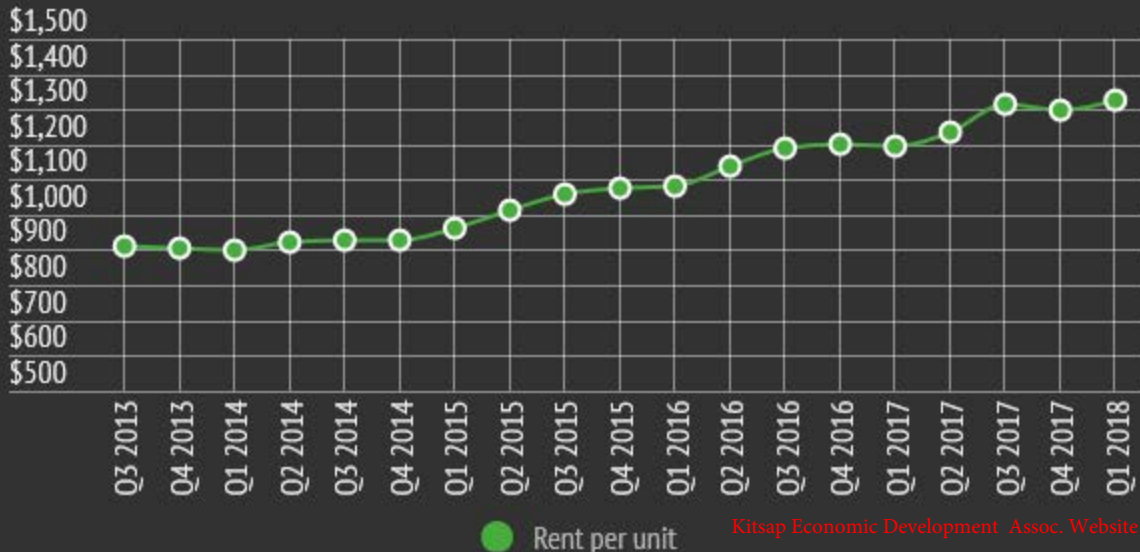
 **5.2%**

Of apartments vacant

Average apartment rent

Comment 2.47D

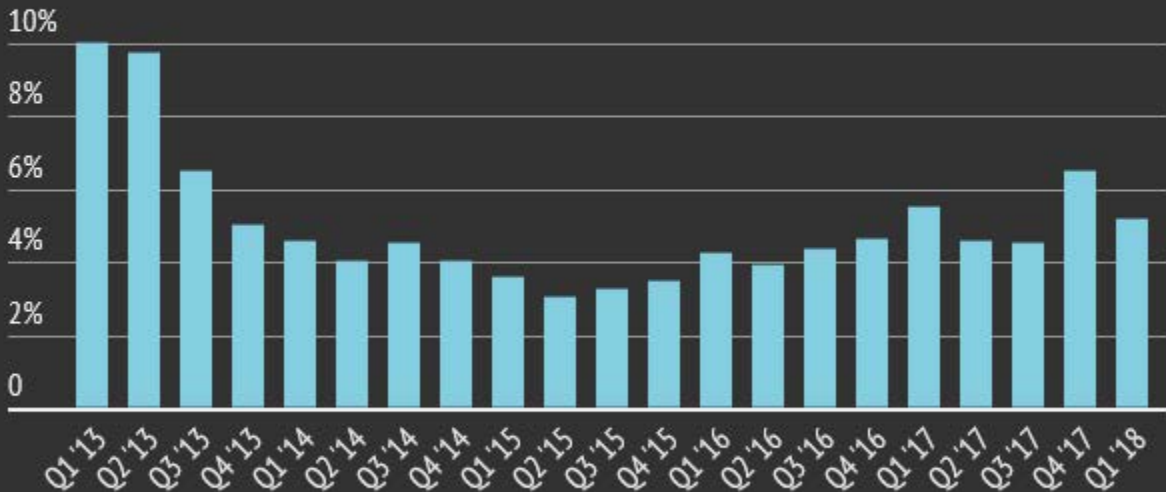
The average per-unit rent in Kitsap County increased 10.7 percent or \$128 in the past year. The chart below shows rents by quarter.



Apartment vacancies

Comment 2.47D

The vacancy rate in Kitsap dipped to 5.2 percent in the first quarter. This chart shows apartment vacancy rates by quarter.



Graphic by Tad Sooter / Kitsap Sun

Kitsap Economic Development Assoc. Website



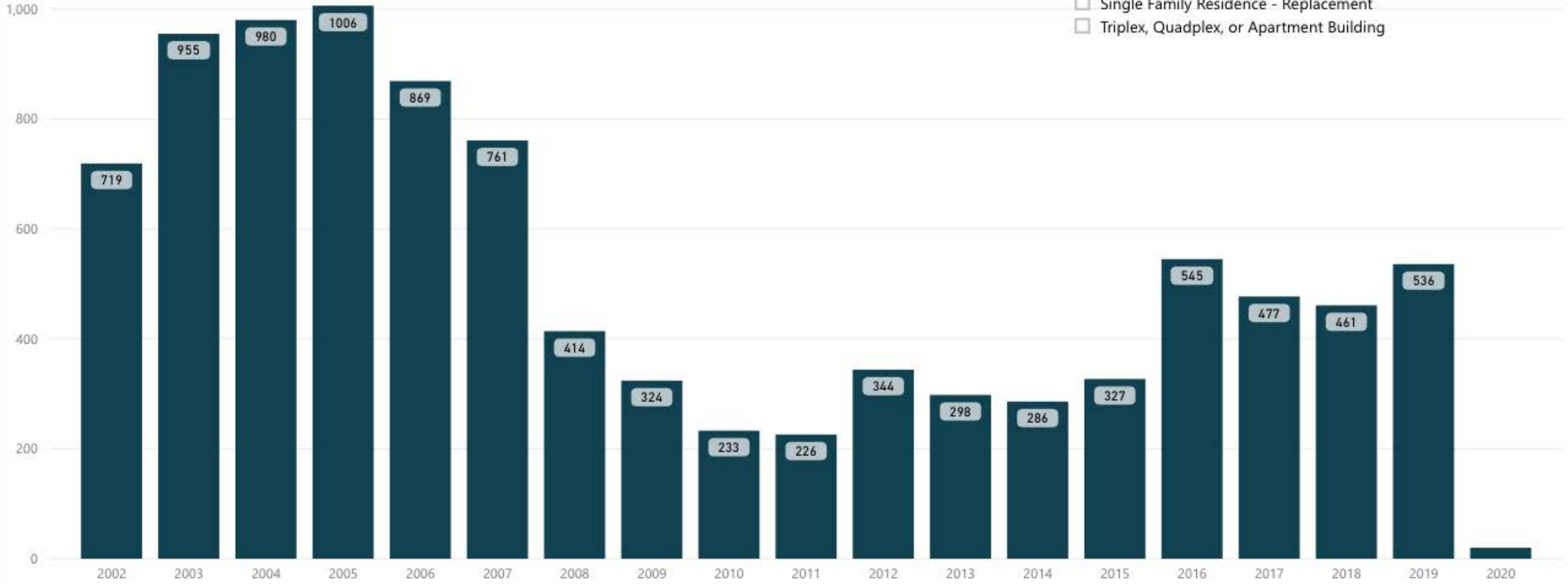
Comment 2.47D



SUBMITTED New Residential Construction - Housing Building Permits

Permit Type

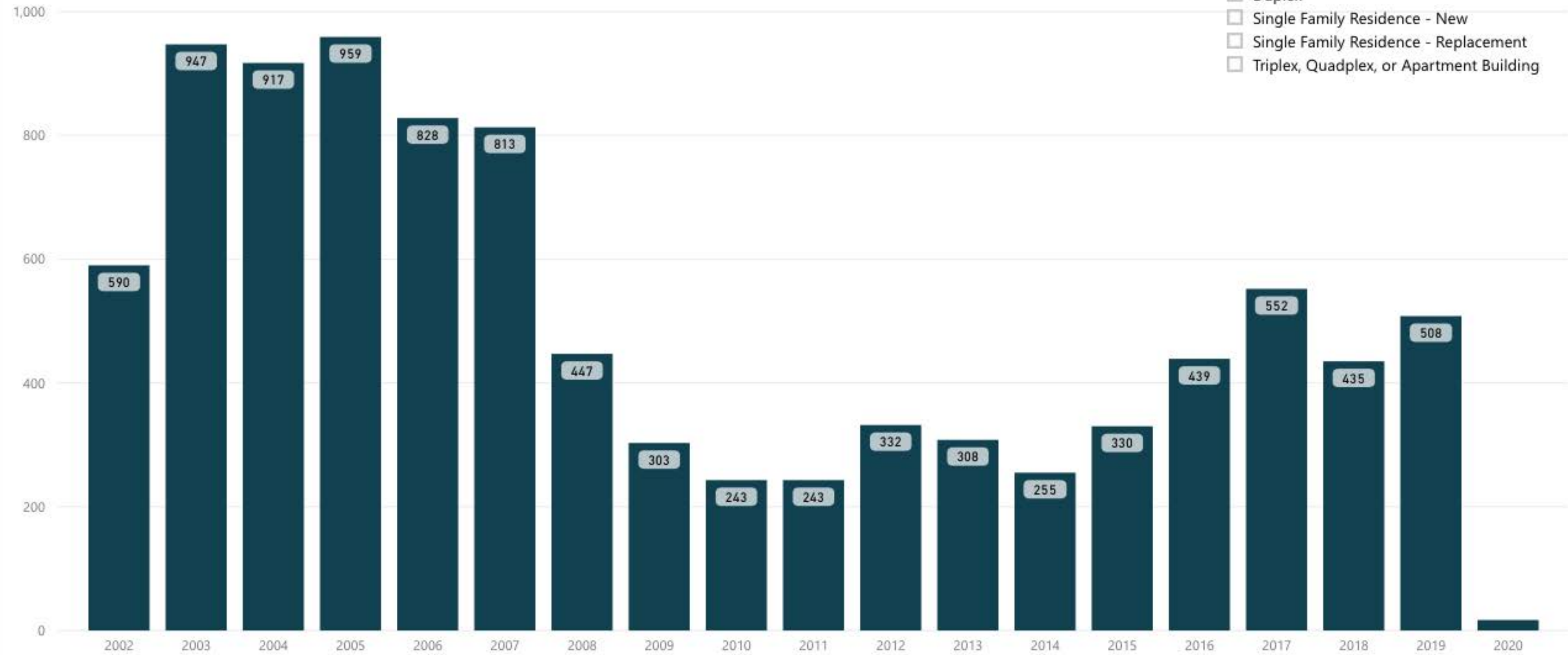
- Basic - Using an Approved Basic Single Family Residence Plan
- Duplex
- Single Family Residence - New
- Single Family Residence - Replacement
- Triplex, Quadplex, or Apartment Building



Permit Type

- Basic - Using an Approved Basic Single Fa...
- Duplex
- Single Family Residence - New
- Single Family Residence - Replacement
- Triplex, Quadplex, or Apartment Building

ISSUED New Residential Construction - Housing Starts



Median Days on Market



Kitsap: Residential

Each data point is 12 months of activity. Data is from January 19, 2020.

Median Sales Price



Kitsap: New Construction, Residential

Each data point is 12 months of activity. Data is from January 19, 2020.



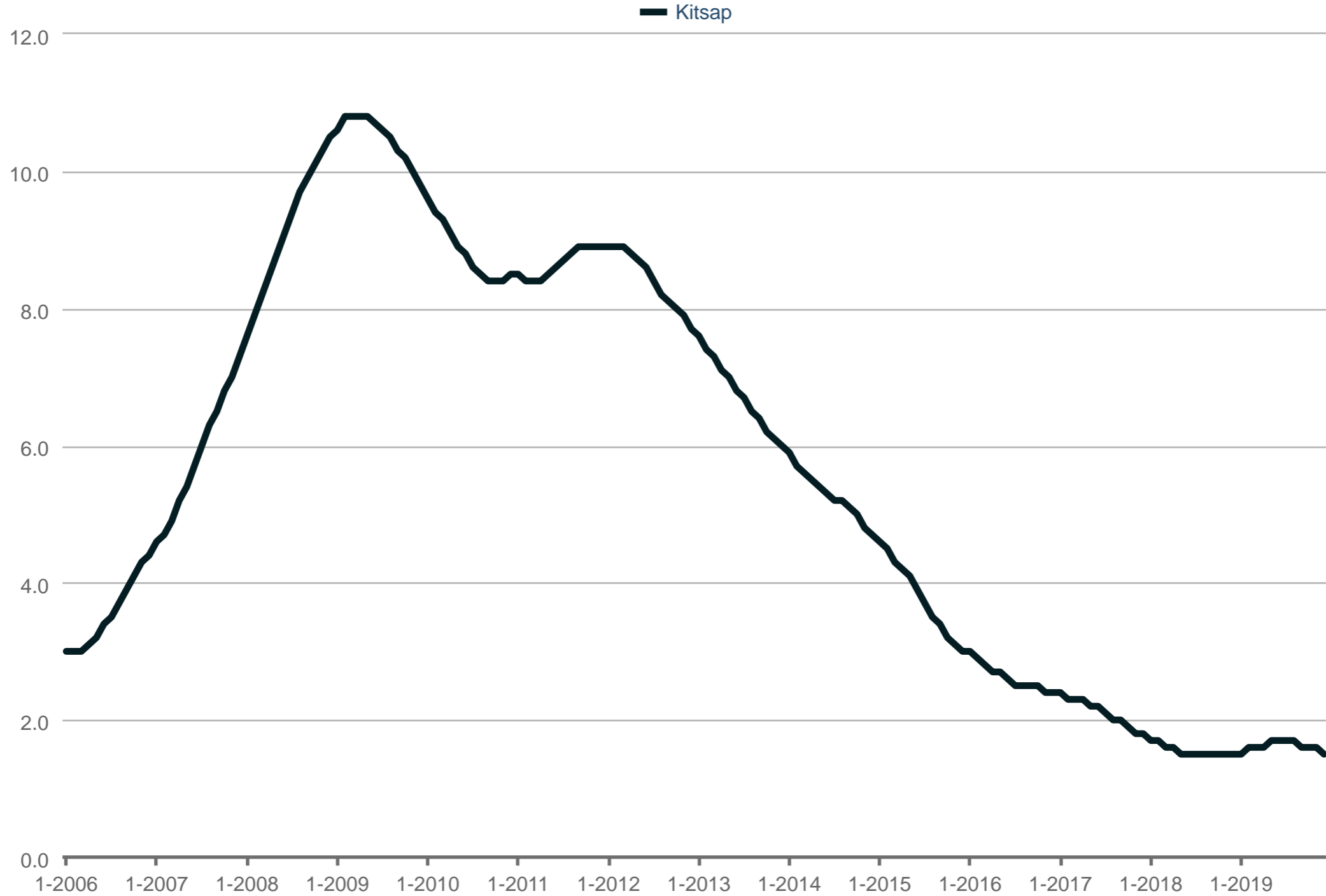
Median Sales Price



Kitsap: Residential

Each data point is 12 months of activity. Data is from January 19, 2020.

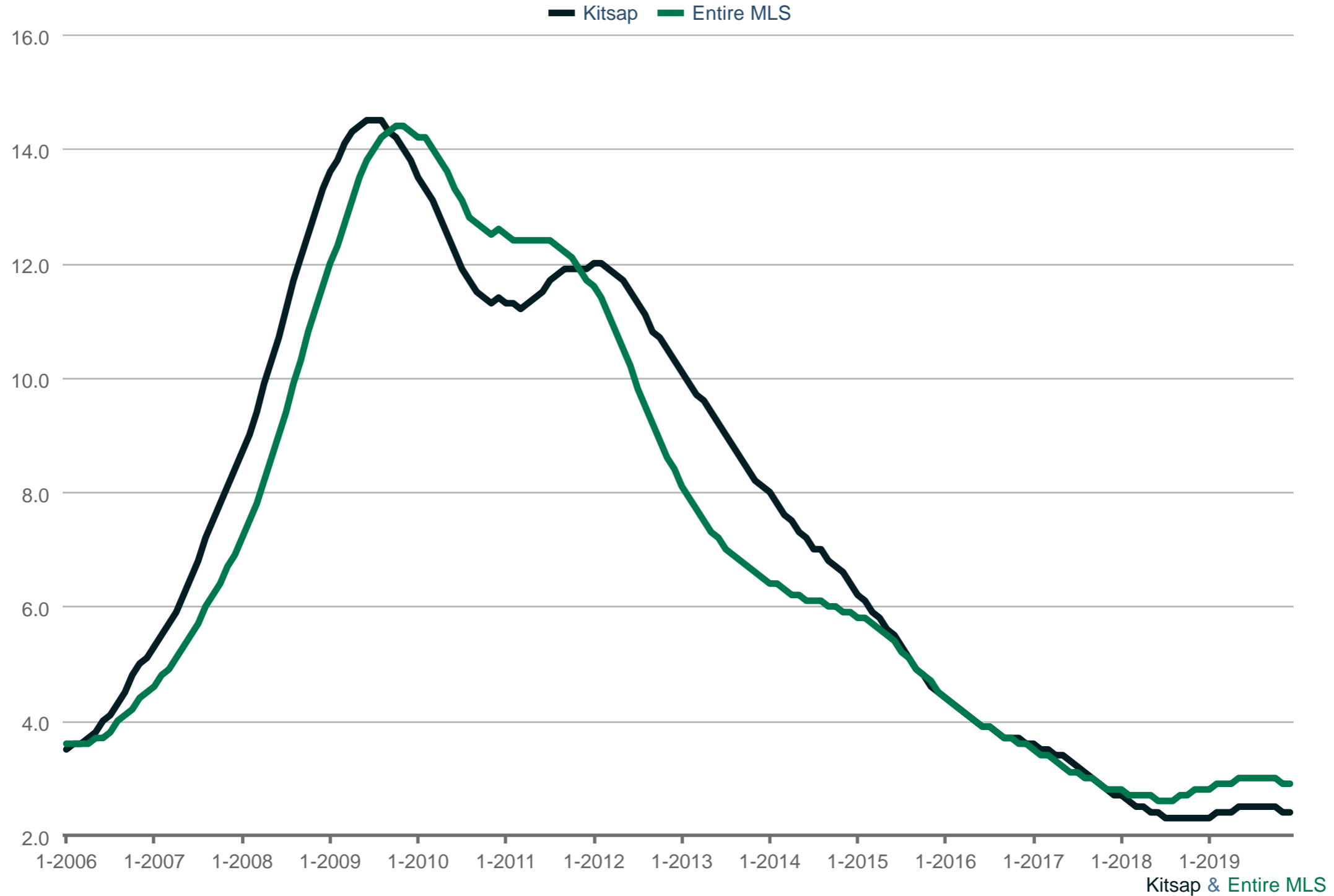
Months Supply of Inventory (Closed)



Kitsap: Residential

Each data point is 12 months of activity. Data is from January 19, 2020.

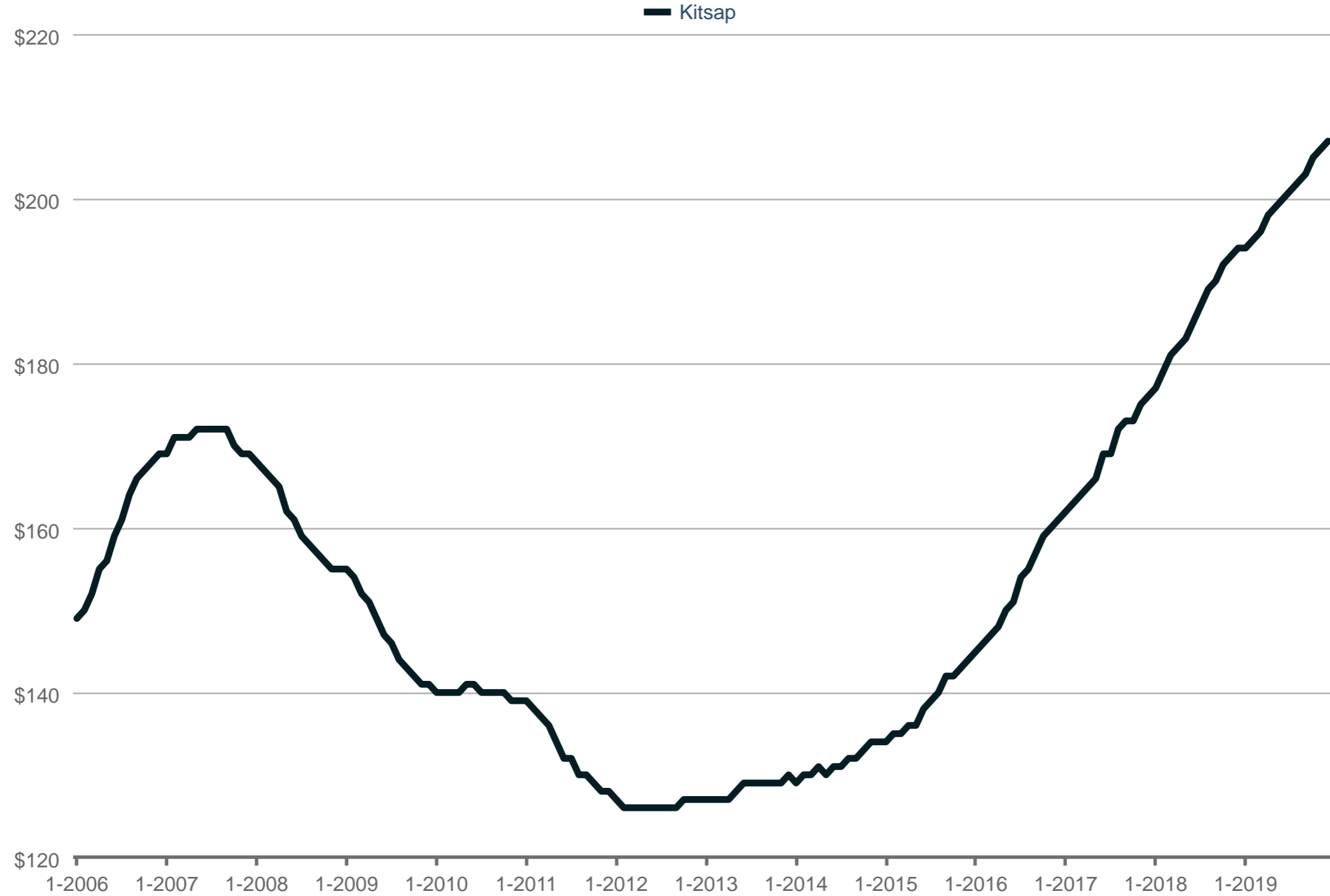
Months Supply of Inventory (Closed)



Each data point is 12 months of activity. Data is from January 19, 2020.



Median Price Per Square Foot



Kitsap: Residential

Each data point is 12 months of activity. Data is from January 19, 2020.

New Listings



Kitsap: Manufactured

Each data point is 12 months of activity. Data is from January 20, 2020.

Business Major Industries Summary

Geography: County - Kitsap

The total number of businesses in the demographic reports may be higher due to the roll-up of additional small business entities not otherwise contained in the database lists (under Business/Households tab).

2013A Major Industry	Employees	%	Establishments	%	Avg Size
Agricultural, Forestry, Fishing (SIC 01-09)	1,144	1.0%	298	2.8%	3.8
Mining (SIC 10-14)	96	0.0%	6	0.0%	16.0
Construction (SIC 15-17)	4,040	3.8%	938	9.1%	4.3
Manufacturing (SIC 20-39)	3,738	3.5%	380	3.6%	9.8
Transportation & Communications (SIC 40-49)	2,906	2.7%	387	3.7%	7.5
Wholesale Trade (SIC 50-51)	1,608	1.5%	401	3.8%	4.0
Retail Trade (SIC 52-59)	14,689	14.0%	1,673	16.2%	8.7
Finance, Insurance & Real Estate (SIC 60-69)	4,020	3.8%	836	8.1%	4.8
Services (SIC 70-89)	42,280	40.3%	5,169	50.2%	8.1
Public Administration (SIC 90-98)	30,166	28.8%	198	1.9%	152.3
2013A Agriculture/Forestry/Fishing					
(SIC 01-09)	Employees	%	Establishments	%	Avg Size
Agricultural Production - Crops (01)	84	7.3%	50	16.7%	1.6
Agricultural Production - Livestock and Animal Specialties (02)	36	3.1%	17	5.7%	2.1
Agricultural Services (07)	927	81.0%	213	71.4%	4.3
Forestry (08)	46	4.0%	10	3.3%	4.5
Fishing, Hunting and Trapping (09)	51	4.4%	8	2.6%	6.3
Total Agriculture/Forestry/Fishing (SIC 01-09)	1,144	100.0%	298	100.0%	3.8
2013A Mining					
(SIC 10-14)	Employees	%	Establishments	%	Avg Size
Metal Mining (10)	0	0.0%	0	0.0%	N/A
Coal Mining (12)	0	0.0%	0	0.0%	N/A
Oil and Gas Extraction (13)	4	4.1%	3	50.0%	1.3
Mining and Quarrying of Nonmetallic Minerals, Except Fuels (14)	92	95.8%	3	50.0%	30.6
Total Mining (SIC 10-14)	96	100.0%	6	100.0%	16.0
2013A Contract Construction					
(SIC 15-17)	Employees	%	Establishments	%	Avg Size
Building Cnstrctn - General Contractors & Operative Builders (15)	1,245	30.8%	385	41.0%	3.2
Heavy Cnstrctn, Except Building Construction - Contractors (16)	461	11.4%	42	4.4%	10.9
Construction - Special Trade Contractors (17)	2,334	57.7%	511	54.4%	4.5
Total Contract Construction (SIC 15-17)	4,040	100.0%	938	100.0%	4.3
2013A Manufacturing					
(SIC 20-39)	Employees	%	Establishments	%	Avg Size
Food and Kindred Products (20)	320	8.5%	21	5.5%	15.2
Tobacco Products (21)	0	0.0%	0	0.0%	N/A
Textile Mill Products (22)	29	0.7%	10	2.6%	2.8
Apparel, Finished Prdcts from Fabrics & Similar Materials (23)	83	2.2%	12	3.1%	6.9
Lumber and Wood Products, Except Furniture (24)	264	7.0%	45	11.8%	5.8
Furniture and Fixtures (25)	127	3.3%	17	4.4%	7.4
Paper and Allied Products (26)	18	0.4%	3	0.7%	6.0
Printing, Publishing and Allied Industries (27)	472	12.6%	58	15.2%	8.1

Chemicals and Allied Products (28)	69	1.8%	14	3.6%	4.9
Petroleum Refining and Related Industries (29)	0	0.0%	0	0.0%	N/A
Rubber and Miscellaneous Plastic Products (30)	15	0.4%	3	0.7%	5.0
Leather and Leather Products (31)	3	0.0%	1	0.2%	3.0
Stone, Clay, Glass, and Concrete Products (32)	66	1.7%	12	3.1%	5.5
Primary Metal Industries (33)	30	0.8%	6	1.5%	5.0
Fabricated Metal Prdcts, Except Machinery & Transport Eqpmnt (34)	99	2.6%	20	5.2%	4.9
Industrial and Commercial Machinery and Computer Equipment (35)	164	4.3%	32	8.4%	5.1
Electronic, Elctrcl Eqpmnt & Cmpnts, Excpt Computer Eqpmnt (36)	166	4.4%	21	5.5%	7.9
Transportation Equipment (37)	411	10.9%	30	7.8%	13.6
Mesr/Anlyz/Cntrl Instrmnts; Photo/Med/Opt Gds; Watches/Clocks (38)	1,229	32.8%	29	7.6%	42.3
Miscellaneous Manufacturing Industries (39)	173	4.6%	46	12.1%	3.7
Total Manufacturing (SIC 20-39)	3,738	100.0%	380	100.0%	9.8

2013A Transportation/Communications

Transportation/Utilities (SIC 40-49)	Employees	%	Establishments	%	Avg Size
Railroad Transportation (40)	1	0.0%	1	0.2%	1.0
Local, Suburban Transit & Interurban Highway Passenger Transport (41)	740	25.4%	23	5.9%	32.1
Motor Freight Transportation (42)	391	13.4%	104	26.8%	3.7
United States Postal Service (43)	364	12.5%	18	4.6%	20.2
Water Transportation (44)	118	4.0%	35	9.0%	3.3
Transportation by Air (45)	86	2.9%	23	5.9%	3.7
Transportation Services (47)	285	9.8%	76	19.6%	3.7
Communications (48)	397	13.6%	55	14.2%	7.2
Total Utilities (46 + 49)	524	18.0%	52	13.4%	10.0
Total Transport/Comm/Utilities (SIC 40-49)	2,906	100.0%	387	100.0%	7.5

2013A Wholesale Trade

(SIC 50-51)	Employees	%	Establishments	%	Avg Size
Wholesale Trade - Durable Goods (50)	920	57.2%	229	57.1%	4.0
Wholesale Trade - Nondurable Goods (51)	688	42.7%	172	42.8%	4.0
Total Wholesale Trade (SIC 50-51)	1,608	100.0%	401	100.0%	4.0

2013A Retail Trade

(SIC 52-59)	Employees	%	Establishments	%	Avg Size
Building Matrials, Hrdwr, Garden Supply & Mobile Home Dealers (52)	1,268	8.6%	85	5.0%	14.9
General Merchandise Stores (53)	1,790	12.1%	28	1.6%	63.9
Food Stores (54)	2,333	15.8%	194	11.5%	12.0
Automotive Dealers and Gasoline Service Stores (55)	1,624	11.0%	136	8.1%	11.9
Apparel and Accessory Stores (56)	549	3.7%	92	5.4%	5.9
Home Furniture, Furnishings and Equipment Stores (57)	598	4.0%	128	7.6%	4.6
Eating and Drinking Places (58)	3,814	25.9%	492	29.4%	7.7
Miscellaneous Retail (59)	2,713	18.4%	518	30.9%	5.2
Total Retail Trade (SIC 52-59)	14,689	100.0%	1,673	100.0%	8.7

2013A Finance/Insurance/Real Estate

(SIC 60 - 69)	Employees	%	Establishments	%	Avg Size
Depository Institutions (60)	806	20.0%	81	9.6%	9.9
Nondepository Credit Institutions (61)	265	6.5%	56	6.6%	4.7

Security & Commodity Brokers, Dealers, Exchanges & Services (62)	221	5.4%	60	7.1%	3.6
Insurance Carriers (63)	110	2.7%	22	2.6%	5.0
Insurance Agents, Brokers and Service (64)	509	12.6%	113	13.5%	4.5
Real Estate (65)	1,968	48.9%	440	52.6%	4.4
Holding and Other Investment Offices (67)	141	3.5%	64	7.6%	2.2
Total Finance/Insurance/Real Estate (SIC 60 - 69)	4,020	100.0%	836	100.0%	4.8

2013A Services

(SIC 70-89)

	Employees	%	Establishments	%	Avg Size
Hotels, Rooming Houses, Camps, and Other Lodging Places (70)	735	1.7%	56	1.0%	13.1
Personal Services (72)	1,317	3.1%	460	8.8%	2.8
Business Services (73)	3,797	8.9%	1,141	22.0%	3.3
Automotive Repair, Services and Parking (75)	908	2.1%	188	3.6%	4.8
Miscellaneous Repair Services (76)	372	0.8%	174	3.3%	2.1
Motion Pictures (78)	320	0.7%	59	1.1%	5.4
Amusement and Recreation Services (79)	1,390	3.2%	202	3.9%	6.8
Health Services (80)	7,767	18.3%	793	15.3%	9.7
Legal Services (81)	614	1.4%	209	4.0%	2.9
Educational Services (82)	6,710	15.8%	207	4.0%	32.4
Social Services (83)	2,437	5.7%	317	6.1%	7.6
Museums, Art Galleries and Botanical and Zoological Gardens (84)	74	0.1%	19	0.3%	3.8
Membership Organizations (86)	2,304	5.4%	512	9.9%	4.5
Engineering, Accounting, Research, Management & Related Svcs (87)	13,187	31.1%	645	12.4%	20.4
Services, Not Elsewhere Classified (89)	348	0.8%	187	3.6%	1.8
Total Services (SIC 70-89)	42,280	100.0%	5,169	100.0%	8.1

2013A Public Administration

(SIC 90 - 98)

	Employees	%	Establishments	%	Avg Size
Executive, Legislative & General Government, Except Finance (91)	1,018	3.3%	32	16.1%	31.8
Justice, Public Order and Safety (92)	1,433	4.7%	40	20.2%	35.8
Public Finance, Taxation and Monetary Policy (93)	39	0.1%	5	2.5%	7.7
Administration of Human Resource Programs (94)	824	2.7%	7	3.5%	117.7
Administration of Environmental Quality and Housing Programs (95)	194	0.6%	21	10.6%	9.2
Administration of Economic Programs (96)	593	1.9%	16	8.0%	37.0
National Security and International Affairs (97)	26,065	86.4%	77	38.8%	338.5
Total Public Administration (SIC 90-98)	30,166	100.0%	198	100.0%	152.3

2013A Unclassified

(SIC 99)

	Employees	%	Establishments	%	Avg Size
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Search Listings Quick Filters Reset

Listing Type
2 selected

Asset Class **Search Tag (optional)**
All selected (10) None selected

Sublease **Listing Status**
Include All selected (5)

Available SF
Min SF Max SF

Total Building SF
Min SF Max SF

Lot SF Acres
Min SF Max SF

Sale Price
\$ Min \$ Max

Lease Rate
\$ Min \$ Max

Total Rent Per Month
\$ Min \$ Max

Total Units
Min Home Max Home

Sale with Lease **Condo**
Yes No Include

CBA ID
CBA ID#

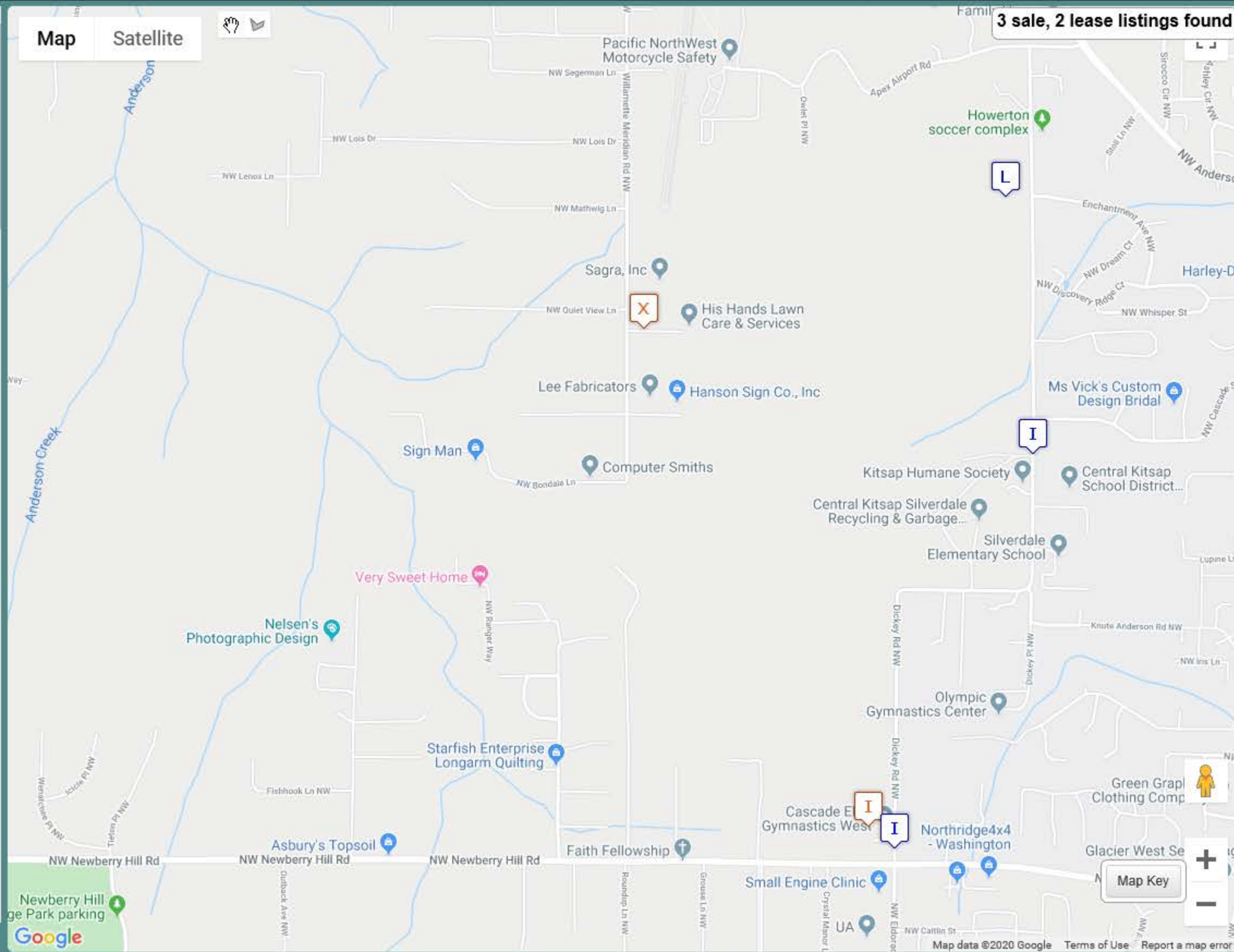
Area / Location

Price Options / Sale Type

Additional Search Options

Property

Construction



\$ High to Low

- Dickey Road Industrial**
Listing ID: 604991
0 Dickey Rd Silverdale, WA 98383
Land: 1,266,724 SF
For Sale: \$1,646,741
- 5600 Dickey Road NW**
Listing ID: 590805
5600 Dickey Rd NW Silverdale, WA 98383
Land: 501,811 SF
For Sale: \$1,500,000
- Dickey Road Land - Lot 066**
Listing ID: 619601
0 Dickey Rd Silverdale, WA 98383
Land: 190,357 SF
For Sale: \$300,000
- Apex Business Park Silverdale**
Listing ID: 594114
6397 NW Concrete Blvd Silverdale, WA 98383
3,000 SF
For Lease: \$0.90
Type: Gross
- Viper Warehouse**
Listing ID: 620231
5606 NW Viper Court Silverdale, WA 98383
9,800 SF
For Lease: \$0.90
Type: NNN

For Lease

**6397 NW Concrete Blvd
 Suite 6397**



Agent Information

Showing Instructions

Call Listing Agent	Yes
Use Discretion	No

Shane Ison
 (360) 373-0317
 shane@isonrealty.com

Ison Realty
 (360) 373-0317

Listing Information

Price

Blended Rent Min	\$ 0.90
Blended Rent Max	\$ 0.90
Shell Rent Min	\$ 0.90
Shell Rent Max	\$ 0.90
Total Monthly Rent	\$ 2,700.00
Lease Type	Gross
Leasing Ofc Com	5%
TI Allowance	NEG
Listing Status	Available
Days On Market	717
Asset Class	Office, Industrial
Search Tags	Build to Suit, Distribution, Gym/Rec Center, Industrial/Office, Industrial/Retail, Manufacturing, Mixed-Use, Warehouse, Data Center/Call Center, Research & Development, Lab, Business Park, Marijuana, Life Science, Bio Tech

Space Information

Suite/Space Info	6397
Available Sq/ft	3,000
Divisible To	1,500
Sub Lease	Yes
Sub Lease Terms	NEGOTIABLE
Availability Status	Available
Date Available	03/01/2019
Move In Terms	Immediately

Industrial Asset Class Info

Min Office SF	0
Max Office SF	3,000
Min Industrial SF	3,000
Max Industrial SF	3,000
Min Total SF	3,000
Max Total SF	6,000
Clearance Height Max	24
Semi-Dock Doors	2
Grade Level Door Count	2

Associated Files

Ison Realty
Http://www.IsonRealty.com
CBA Flyer

Property Information

Building Information

Building Status	Existing
# of Buildings	1
# of Floors	2
Clearance Height Min	14
Clearance Height Max	24
Total Building Sq/ft	5,500
Net Rentable Area	5,500
Total Office Sq/ft	2,500
Largest Continuous Sq/ft	5,500
Lot Sq/ft	6,098
Acres	0.14

Property Type

Property Type	Industrial
---------------	------------

Location

County	Kitsap
Cross Street	Williamette Meridian Rd NW
Market Area	Silverdale
Tax ID #	19250120342001
Zoning	Industrial

Construction

Year Built	2018
Completion Date	02/28/2018
Roof Type	Metal
Construction Type	Metal

SILVERDALE OFFICE / WAREHOUSE

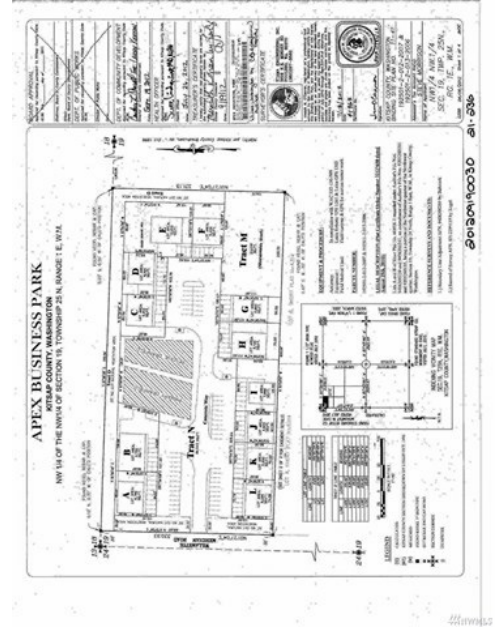
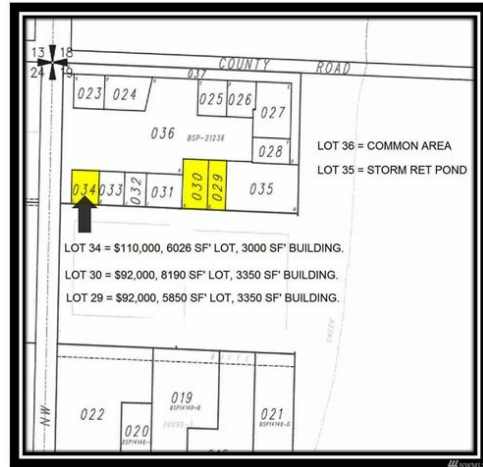
Public Comments: Brand new 3000 SF¹ industrial zoned Office/Warehouse located just outside Silverdale approx 2 miles to Hwy 3. Building is 3000 SF¹ at ground level with the ability to add a mezzanine and increase the usable SF¹ to approx. 5500. 2 ea - 14' roll up doors w/ sufficient lay down and parking areas. Owner will consider Build to Suite arrangement for a long term tenant and building can be demised down to 1500 SF¹. Available now at \$.90 PSF, mod gross lease.

Showing Instructions: Very easy access. Just call L/A for contractor key box code.

Search Tags: Build to Suit, Industrial/Office, Industrial/Retail, Manufacturing, Warehouse, Research & Development, Business Park,

Apex Business Park Silverdale Suite 6397

Listing Images



Viper Warehouse

5606 NW Viper Court Silverdale WA 98383

Listing #: 620231 | Status: Available | Last Modified: 1/6/2020 |

For Lease

5606 NW Viper Court Silverdale WA 98383



Comment 2.47D



Agent Information

Showing Instructions

Call Listing Agent	Yes
Use Discretion	No

	Jeff Coombe (360) 620-5300 jeff@jcmpm.com
	

JCM Property Management
(360) 620-5300

Listing Information

Price	
Blended Rent Min	\$ 0.90
Blended Rent Max	\$ 0.90
Shell Rent Min	\$ 0.90
Shell Rent Max	\$ 0.90
Total Monthly Rent	\$ 8,820.00
Lease Type	NNN
NNN Expense	\$ 0.15
Leasing Ofc Com	2.5%
Listing Status	Available
Days On Market	62
Asset Class	Industrial
Search Tags	Distribution, Industrial/Office, Manufacturing, Warehouse, Research & Development, Lab, Marijuana, Life Science, Bio Tech, Auto Repair, Storage

Space Information	
Available Sq/ft	9,800
Divisible To	5,000
Date Available	06/01/2020

Industrial Asset Class Info	
Min Industrial SF	9,800
Max Industrial SF	9,800
Min Total SF	9,800
Max Total SF	9,800
Bay Depth	70'
Semi-Dock Doors	2

Associated Files	
CBA Flyer	

Property Information

Building Information	
Building Status	Proposed
# of Buildings	1
# of Floors	1
Clearance Height Min	20
Clearance Height Max	30
Bay Depth	70'
Total Building Sq/ft	9,800
Lot Sq/ft	9,800
Acres	0.22

Property Type	
Property Type	Industrial

Location	
County	Kitsap
Cross Street	Dickey Road & Newberry Hill Road
Market Area	Silverdale
Tax ID #	5601-000-006-0105
Zoning	Industrial

Parking Information	
# Uncovered Spaces	14
Total Parking Spots	14
Parking Ratio	1/700

Construction	
Year Built	2020
Completion Date	06/01/2020
Roof Type	Metal
Construction Type	Metal

Public Comments: New warehouse ready for occupancy June 1, 2020. Located at the top of Newberry Hill Road near Silverdale and easy access to all Kitsap Military bases. High ceilings 14' high doors.

Public Building Comments: New warehouse building to be constructed with June 1, 2020 anticipated completion. Located at the top of Newberry Hill Road west of Highway 3. Easy access and close to Silverdale and all Kitsap County military bases.

Search Tags: Distribution, Industrial/Office, Manufacturing, Warehouse, Research & Development, Lab, Marijuana, Auto Repair, Storage,

Comment 2.47D

For Lease

Viper Warehouse

Listing Images



For Sale

5600 Dickey Rd NW Silverdale WA 98383



Listing Information

Price	
Listing Price	\$ 1,500,000
Selling Ofc Com	3%
Listing Status	Pending
Days On Market	826
Asset Class	Industrial

Investment Information

Utilities	
Sewer	Yes

Associated Files

CBA Flyer

Property Information

Building Information	
Building Status	Land
# of Buildings	0
Lot Sq/ft	501,811
Acres	11.52

Property Type

Property Type	Land
---------------	------

Location

County	Kitsap
Cross Street	Newberry Hill Rd
Market Area	Silverdale
Tax ID #	44490010030501
Zoning	Industrial

Agent Information

Showing Instructions

Call Listing Agent	No
Use Discretion	No

Richard Rucker
 (360) 731-0503
 rucker.group@gmail.com

The Rucker Group
 (360) 731-0503

Public Comments: Property is located in the path of progress. Most available zoned property with utilities in Silverdale is significantly more expensive per square foot. Additionally, there are very few parcels available for development of any use. Property has a view of Dyes Inlet. Also, parcel has a view of the Seattle sky line on a clear day.

Public Building Comments: Property is located in the path of progress. Property has a view of Dyes Inlet. Also, parcel has a view of the Seattle sky line on a clear day.

Search Tags: Industrial/Office, Industrial/Retail,

Dickey Road Industrial

0 Dickey Rd Silverdale WA 98383

Listing #: 604991 | Status: Available | Last Modified: 1/21/2020 |

Comment 2.47D

For Sale

0 Dickey Rd Silverdale WA 98383



Listing Information

Price	
Listing Price	\$ 1,646,741
\$/lot sf	\$ 1.30
Selling Ofc Com	2.5%
Listing Status	Available
Days On Market	409
Asset Class	Land
Search Tags	Condos

Investment Information

Land Asset Class Information	
Land Use Category	Industrial

Associated Files

flyer 091219
CBA Flyer

Property Information

Building Information	
Building Status	Land
# of Buildings	0
# of Units	0
Lot Sq/ft	1,266,724
Acres	29.08

Property Type

Property Type	Land
---------------	------


Location

County	Kitsap
Vicinity	Silverdale
Location Description	West of intersection of Dickey and Enchantment
Cross Street	Enchantment
Market Area	Silverdale
Tax ID #	18250140512006
Additional Parcels	18250140502007
Zoning	Industrial
Land Use Type	Industrial

Agent Information

Showing Instructions

Call Listing Agent	Yes
Use Discretion	No

	<p>Eric Bissell (425) 450-1121 ebissell@kiddermathews.com</p>
--	--

<p>Kidder Mathews (425) 454-7040</p>
--

Public Comments: Buildable site with timber. Great for owner/user. Zoned Industrial. Five minute drive to WA-3.

Public Building Comments: 30 Acres +/- zoned Industrial with marketable timber and utilities nearby. Approximately 1000 ft of Dickey Rd. frontage.

Search Tags: Forest,

Dickey Road Land - Lot 066

0 Dickey Rd Silverdale WA 98383

Listing #: 619601 | Status: Available | Last Modified: 1/6/2020 |

Comment 2.47D

For Sale

0 Dickey Rd Silverdale WA 98383



Listing Information

Price	
Listing Price	\$ 300,000
Selling Ofc Com	5%
Listing Status	Available
Days On Market	77
Asset Class	Industrial

Investment Information

Associated Files

CBA Flyer

Property Information

Building Information	
Building Status	Land
# of Buildings	0
Lot Sq/ft	190,357
Acres	4.37

Property Type

Property Type	Land
---------------	------

Location

County	Kitsap
Market Area	Silverdale
Tax ID #	19250140662008

Agent Information

Showing Instructions

Call Listing Agent	Yes
Use Discretion	No



Frank Leach
(360) 509-4346
fleach@oz.net

RE/MAX Platinum Services
(360) 662-5400

Public Comments: Great Industrial site minutes to Hwy 3 freeway access north and south, and to Silverdale, Bremerton, Poulsbo. Views of Cascades and Mt. Rainier and Dyes Inlet, Many possibilities, close to Silverdale Water Dist. and CK Fire Dept. headquarters. Property slopes to the east, Dickey Road to the west side. Call today!

Public Building Comments: Great Industrial site minutes to Hwy 3 freeway access north and south, and to Silverdale, Bremerton, Poulsbo. Views of Cascades and Mt. Rainier and Dyes Inlet. Close to Silverdale Water Dist. and CK Fire Dept. headquarters. Property slopes to the east, Dickey Road to the west side.

Regional Centers Framework Update

Adopted March 22, 2018



Puget Sound Regional Council

Regional Centers Framework Update
Adopted March 22, 2018

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1. Purpose and Background

Purpose

Centers are the hallmark of VISION 2040 and the Regional Growth Strategy. They guide regional growth allocations, advance local planning, inform transit service planning, and represent priority areas for PSRC's federal transportation funding.

In 2015, the Growth Management Policy Board adopted a scope of work to review the existing centers framework. The regional centers have been integral to regional planning for over 20 years, and this update represented an opportunity to reassess the system in place today and opportunities to recognize other places that are serving important roles in the region. The Regional Centers Framework outlines a revised structure and criteria for regional and countywide centers and direction to update policies and procedures to update to the regional centers framework.

This update will:

- Clearly define the types of central places—both larger and smaller—within the region that are the focus of growth, planning, and investment.
- Establish criteria and planning expectations that ensure centers are developing as thriving and connected communities with sufficient market potential to accommodate new jobs and residents.
- Provide for consistent designation of centers at the regional and countywide levels across the region.
- Address requirements for new centers and redesignation of existing regional centers.

Growth in centers has significant regional benefits, including supporting multimodal transportation options, compact growth, housing choices near jobs, climate goals, and access to opportunity. As important focal points for investment and development, regional centers represent a crucial opportunity to support equitable access to affordable housing, services, health, quality transit service, and employment, as well as to build on the community assets currently present within centers.

Implementation

This framework establishes key opportunities for the region to support VISION 2040's objective of encouraging development of compact, livable centers as an opportunity to accommodate a significant portion of the region's growth. Adoption of the framework in itself does not change regional or local policies, regulations, or funding mechanisms. Implementation of the framework will take several steps that are discussed in Section 12 of the framework:

- Updating new center designation procedures
- Developing new administrative procedures for monitoring of existing centers
- Updating VISION 2040, including guidance on growth expectations for centers
- Updating countywide planning policies with countywide criteria and designations
- Measuring performance and outcomes over time
- Completing additional analysis on social equity strategies for centers
- Completing additional review and consultation with tribes on the role of tribal lands in the centers framework
- Research funding opportunities for centers
- Research the number and distribution of centers

Regional Centers Framework Update
Adopted March 22, 2018

The framework proposal focuses on the criteria and process to designate and evaluate regional and countywide centers. The proposal does not recommend prioritizing funding based on center size or type. The proposal identifies different types and sizes of regional centers to better tailor expectations for future growth and development in centers.

Development of the board proposal has focused on providing opportunities for jurisdictions to designate new centers and flexibility to maintain existing centers, including at least two growth centers and one manufacturing/industrial center in each county. The centers framework should continue to maintain appropriate regional distribution and provide for opportunities to designate new centers.

Guiding Principles & Objectives

In the project scope of work, the Growth Management Policy Board adopted the following guiding principles for the project:

- *Support the Growth Management Act and VISION 2040.*
- *Focus growth consistent with the Regional Growth Strategy.*
- *Recognize and support different types and roles of regional and subregional centers.*
- *Provide common procedures across the region.*
- *Guide strategic use of limited regional investments.*
- *Inform future planning updates at regional, countywide, and local levels.*

Objectives to guide the project were established at a joint board session in 2016:

Growth: Centers attract robust population and employment growth—a significant and growing share of the region’s overall growth.

Mobility: Centers provide diverse mobility choices so that people who live and work in centers have alternatives to driving alone.

Environment: Centers improve environmental sustainability, protecting rural and resource lands, habitat, and other critical areas by focusing the majority of growth in urban areas with existing infrastructure.

Social Equity and Opportunity: Centers offer high access to opportunity, including affordable housing choices and access to jobs, to a diverse population.

Economic Development: Centers help the region maintain a competitive economic edge by offering employers locations that are well connected to a regional transportation network, and are attractive and accessible to workers.

Public Health: Centers create safe, clean, livable, complete and healthy communities that promote physical, mental, and social well-being.

The Regional Centers Framework Update project included significant outreach and committee and board discussions. The process was informed by staff-level Technical Advisory Group and Stakeholder Working Group, joint board sessions, county-level workshops, and ongoing outreach to local governments. A staff-level Stakeholder Working Group met from June 2016 through January 2017 and identified recommendations and alternatives for PSRC’s boards to consider. Their final report informed deliberation by the Growth Management Policy Board in 2017.

2. Regional Centers Designation Procedures

The **Designation Procedures for New Centers** contains detailed requirements for designation and review of regional growth centers and manufacturing/industrial centers. The procedures are adopted by the Executive Board.

As part of the implementation of this framework, PSRC will update the **Designation Procedures for New Centers** to incorporate the following procedural changes:

- When designating new regional centers, the PSRC boards will also consider:
 - Distribution of centers throughout the region, including by county, and whether new center locations would be advantageous for overall regional growth objectives. Centers should be distributed in locations consistent with the regional vision, and in areas that do not place additional development pressure on rural and resource lands. Environmental factors may be considered in designating new centers.
 - The overall number of centers in the region, supported by research on the number and distribution of centers.
- Application and review of new regional centers will be limited to major regional growth plan updates (VISION 2040 and its successor plans) and approximately every five years, following the results of performance monitoring. As an interim measure, the application period for new centers will remain open through the fall of 2019.
- Employment and/or activity thresholds for new regional centers will be reviewed and potentially updated when the regional growth plan is updated to account for overall growth in centers over time. Center designations should remain relatively stable over the long term, but will allow centers to change into new types when they have achieved higher levels of activity and other criteria.

3. Regional Growth Centers Eligibility and Criteria

Regional growth centers are locations of more compact, pedestrian-oriented development with a mix of housing, jobs, retail, services, and other destinations. The region's plans identify centers as areas that should receive a significant share of the region's population and employment growth compared with other parts of the urban area, while providing improved access and mobility—especially for walking, biking, and transit.

Eligibility Criteria for New Regional Growth Centers

The minimum eligibility requirements for new centers ensure consistency in centers designation and ensure that new regional growth centers meet the intent of VISION 2040 while allowing for flexibility. The **Designation Procedures for New Centers** will be updated to identify additional supporting documentation:

- **Local commitment.** Evidence center is a local priority and sponsor city/county has sustained commitment over time to local investments in creating a walkable, livable center.

Regional Centers Framework Update

Adopted March 22, 2018

- o **Planning.** Completion of a center plan (subarea plan, plan element or functional equivalent that provides detailed planning or analysis) that meets regional guidance in advance of designation. Environmental review that demonstrates center area is appropriate for dense development.
 - Assessment of housing need, including displacement risk, as well as documentation of tools, programs, or commitment to provide housing choices affordable to a full range of incomes and strategies to further fair housing
- o **Jurisdiction and Location.** New regional growth centers should be located within a city, with few exceptions. LINK light rail stations in unincorporated urban areas (including those funded through the Sound Transit 3 ballot measure) may be eligible for center designation at any scale, provided they are affiliated for annexation or planned for incorporation. Joint planning of unincorporated center areas is encouraged. Other unincorporated urban areas may be eligible for countywide center status, provided they are affiliated for annexation or planned for incorporation.
- o **Existing Conditions.** Existing infrastructure and utilities sufficient to support new center growth, a mix of both existing housing and employment, justification of size and shape. Recommend centers to be nodal with a generally round or square shape and avoid linear or gerrymandered shapes that are not readily walkable or connected by transit. Bicycle and pedestrian infrastructure, amenities, and a street pattern that supports walkability

Designation Criteria for New Regional Growth Centers

The Regional Centers Framework Update defines two distinct types of regional growth centers with tailored minimum criteria as described in this section. The type of regional center does not establish a distinction for the purpose of PSRC’s regional funding process. The criteria are expanded to include discussion of appropriate size, minimum transit service, market potential, and regional role. Transit service is an important factor for growth in centers. Local governments should work with transit providers to plan for appropriate transit service levels in centers. The center types will be used to inform future growth planning.

Regional Growth Centers	
<p>Urban Growth Center</p> <p>These centers have an important regional role, with dense existing jobs and housing, high-quality transit service, and planning for significant growth. These centers may represent areas where major investments – such as high-capacity transit – offer new opportunities for growth.</p>	<p>Metro Growth Center</p> <p>These centers have a primary regional role – they have dense existing jobs and housing, high-quality transit service, and are planning for significant growth. They will continue to serve as major transit hubs for the region. These centers also provide regional services, and serve as major civic and cultural centers.</p>
<p>Urban Growth Center Criteria</p> <p>Center must meet each the following criteria:</p> <ul style="list-style-type: none"> o Existing density. 18 activity units per acre minimum o Planned target density. 45 activity units per acre minimum 	<p>Metro Growth Center Criteria</p> <p>Center must meet each the following criteria:</p> <ul style="list-style-type: none"> o Existing density. 30 activity units per acre minimum o Planned target density. 85 activity units per acre minimum

<ul style="list-style-type: none"> ○ Mix of uses. Regional growth centers should have a goal for a minimum mix of at least 15% planned residential and employment activity in the center. ○ Size. 200 acres minimum - 640 acres maximum (may be larger if served by an internal, high capacity transit system) ○ Transit. Existing or planned¹ fixed route bus, regional bus, Bus Rapid Transit, or other frequent and all-day bus service. May substitute high-capacity transit mode for fixed route bus. Service quality is defined as either frequent (< 15-minute headways) and all-day (operates at least 16 hours per day on weekdays) –or- high capacity ○ Market potential. Evidence of future market potential to support planning target ○ Role. Evidence of regional role <ul style="list-style-type: none"> ● Clear regional role for center (serves as important destination for the county) ● Jurisdiction is planning to accommodate significant residential and employment growth under Regional Growth Strategy 	<ul style="list-style-type: none"> ○ Mix of uses. Regional growth centers should have a goal for a minimum mix of at least 15% planned residential and employment activity in the center. ○ Size. 320 acres minimum - 640 acres maximum (may be larger if served by an internal, high capacity transit system) ○ Transit. Existing or planned light rail, commuter rail, ferry, or other high capacity transit with similar service quality as light rail. Service quality is defined as either frequent (< 15-minute headways) and all-day (operates at least 18 hours per day on weekdays) –or- high capacity (e.g., ferry, commuter rail, regional bus, Bus Rapid Transit). Evidence the area serves as major transit hub and has high quality/high capacity existing or planned service. ○ Market potential. Evidence of future market potential to support planning target ○ Role. Evidence of regional role: <ul style="list-style-type: none"> ● Clear regional role for center (for example, city center of metropolitan cities, other large and fast-growing centers; important regional destination) ● Jurisdiction is planning to accommodate significant residential and employment growth under Regional Growth Strategy
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4. Manufacturing/Industrial Centers Eligibility and Criteria

Manufacturing/industrial centers preserve lands for family-wage jobs in basic industries and trade and provide areas where that employment may grow in the future. Manufacturing/industrial centers form a critical regional resource that provides economic diversity, supports national and international trade, generates substantial revenue for local governments, and offers higher than average wages.

¹ “Planned” transit means funded projects or projects identified in the constrained portion of Transportation 2040. The Transportation 2040 constrained project list incorporates projects in transit agency long-range plans where funding is reasonably expected during the 2040 planning horizon.

Regional Centers Framework Update
Adopted March 22, 2018

VISION 2040 calls for the recognition and preservation of existing centers of intensive manufacturing and industrial activity and the provision of infrastructure and services necessary to support these areas. These centers are important employment locations that serve both current and long-term regional economic objectives.

Manufacturing/industrial centers have very different characteristics and mobility needs than regional growth centers. For example, transit may not be viable for all types of manufacturing/industrial centers, but identifying transportation demand management strategies, including carpool and vanpools, can help reduce congestion impacts regardless of transit access. The criteria to designate manufacturing/industrial centers focuses on these and other factors to support the long-term industrial base of the region. Moving freight and goods to and through MICs is critical, on trucks, as well as other modes, such as marine, air and rail.

The [Industrial Lands Analysis \(2015\)](#) identified strategies to ensure an adequate supply of industrial land in the region, including protecting priority users of industrial land and limiting commercial and office uses that compete with industrial use. The centers designation criteria provide some flexibility for non-industrial uses in manufacturing/industrial centers, which may include amenities and services for employees, some commercial uses, and other types non-industrial uses.

Eligibility Criteria for New Regional Manufacturing/Industrial Centers

Minimum eligibility requirements ensure consistency in centers designation and ensure that new regional growth centers meet the intent of VISION 2040 while allowing for flexibility. The **Designation Procedures for New Centers** should be updated to identify additional supporting documentation:

Local commitment. Evidence center is a local priority had city/county has sustained commitment over time to local investments in infrastructure and transportation. Demonstrated commitment to protecting and preserving industrial uses, strategies and incentives to encourage industrial uses in the center, and established partnerships with relevant parties to ensure success of manufacturing/industrial center

Planning. Completion of a center plan (subarea plan, plan element or functional equivalent) that meets regional guidance in advance of designation. Where applicable, the plan should be developed in consultation with public ports and other affected governmental entities. Environmental review that the area is appropriate for development

Location. Manufacturing/industrial centers should be located within a city with few exceptions.

Existing Conditions. Adequate infrastructure and utilities to support growth, access to relevant transportation infrastructure, documentation of economic impact, and justification of size and shape of manufacturing/industrial center

Designation Criteria for New Regional Manufacturing/Industrial Centers

The Regional Centers Framework Update identifies two distinct pathways to designate new manufacturing/industrial centers. Minimum eligibility for regional designation is described in this section. The criteria are expanded to include discussion of appropriate employment type, core industrial zoning, industrial preservation strategies, and regional role. The center pathways may be used to inform future growth planning.

New Manufacturing/Industrial Centers	
<p>Industrial Employment Center</p> <p>These centers are highly active industrial areas with significant existing jobs, core industrial activity, evidence of long-term demand, and regional role. They have a legacy of industrial employment and represent important long-term industrial areas, such as deep-water ports and major manufacturing. The intent of this designation is to, at a minimum, preserve existing industrial jobs and land use and to continue to grow industrial employment in these centers where possible. Jurisdictions and transit agencies should aim to serve all MICs with transit.</p>	<p>Industrial Growth Center</p> <p>These regional clusters of industrial lands have significant value to the region and potential for future job growth. These large areas of industrial land serve the region with international employers, industrial infrastructure, concentrations of industrial jobs, and evidence of long-term potential. The intent of this designation is to continue growth of industrial employment and preserve the region's industrial land base for long-term growth and retention. Jurisdictions and transit agencies should aim to serve all MICs with transit.</p>
<p>Center must meet each the following criteria:</p> <ul style="list-style-type: none"> • Existing jobs: 10,000 minimum • Planned jobs: 20,000 minimum • Minimum 50% industrial employment • If MIC is within a transit service district, availability of existing or planned frequent, local, express, or flexible transit service. If MIC is outside a transit service district, documented strategies to reduce commute impacts through transportation demand management strategies consistent with the Regional Transportation Plan Appendix F (Regional TDM Action Plan) • Presence of irreplaceable industrial infrastructure² • At least 75% of land area zoned for core industrial uses³ • Industrial retention strategies in place • Regional role 	<p>Center must meet each the following criteria:</p> <ul style="list-style-type: none"> • Minimum size of 2,000 acres • Existing jobs: 4,000 minimum • Planned jobs: 10,000 minimum • Minimum 50% industrial employment • If MIC is within a transit service district, availability of existing or planned frequent, local, express, or flexible transit service. If MIC is outside a transit service district, documented strategies to reduce commute impacts through transportation demand management strategies consistent with the Regional Transportation Plan Appendix F (Regional TDM Action Plan) • At least 75% of land area zoned for core industrial uses • Industrial retention strategies in place • Regional role

² Industrial-related infrastructure that would be irreplaceable elsewhere, such as working maritime port facilities, air and rail freight facilities.

³ Zoning designations dominated by traditional industrial land uses such as manufacturing, transportation, warehousing and freight terminals. Commercial uses within core industrial zones shall be strictly limited.

5. Regional Center Redesignation Standards

Regional centers have been a central strategy of the regional plan for decades, although centers have been designated through different procedures depending on when they were first designated. An objective with the regional centers framework update is to establish a more consistent system between new and existing centers. Recognizing both that existing centers were designated through different processes and the objective of working toward greater consistency, PSRC will develop administrative procedures for review of existing centers. The procedures are anticipated to be drafted in the first half of 2018, with a review of existing centers to follow in 2018 and 2019 during the VISION 2050 planning process.

- **Initial redesignation.** The first evaluation of existing centers will occur in 2018-2020 as part of the VISION 2040 update. All designated regional centers as of 2017 are automatically redesignated, provided that they meet the following criteria:
 - Adopted center plan (subarea plan, plan element or functional equivalent) should be completed by 2020. Jurisdictions may request an extension from the Growth Management Policy Board if substantial progress on subarea planning has been made by 2020. Different approaches to subarea planning may be appropriate and input from other affect government entities, such as public ports, will be considered, but future updates should be equivalent to a subarea plan by 2025. Plans should include goals and policies that specifically address the center and should be adopted by the jurisdiction(s) with local land use authority for the center. Plan adoption should meet public notice and involvement requirements established under the Growth Management Act.
 - Designation of the regional center in the adopted local comprehensive plan and countywide planning policies.
- PSRC staff will work with cities and counties to identify the applicable center types and whether all the criteria are already met or could be met.
- **Monitoring review of regional growth centers.** A first monitoring review period, scheduled for 2025, will follow the next major comprehensive plan periodic update (due in 2023 and 2024) and will reoccur about every five years thereafter. At the first monitoring review in 2025, existing regional growth centers will be expected to fully meet eligibility and designation criteria similar to new centers:
 - **Local commitment.** Evidence center is a local priority and sponsor city/county has sustained commitment over time to local investments in creating a walkable, livable center.
 - **Planning.** An updated center plan (subarea plan, plan element or functional equivalent that provides detailed planning or analysis) that addresses regional guidance, and plans for a mix of housing and employment, bicycle and pedestrian infrastructure, amenities, and a street pattern that supports walkability.
 - Assessment of housing need, including displacement risk, as well as documentation of tools, programs, or commitment to provide housing choices affordable to a full range of incomes and strategies to further fair housing.

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 Adopted March 22, 2018

- **Location.** Existing regional growth centers not located within a city should be affiliated for annexation or in a location planned for incorporation. Joint planning of the center area is encouraged.
 - **Capital investments.** Capital investments by the local government in the center in the current or prior 6-year capital planning cycle, and commitment to infrastructure and utilities in the jurisdiction’s capital improvement program sufficient to support center growth, pedestrian infrastructure, and public amenities.
 - **Center criteria.** Consistent with designation criteria for size, planning, transit, market potential, and role for new regional growth centers in Section 3. Existing centers will remain designated if they do not meet the new center density criteria, provided that the center is consistent with other criteria identified in this section.
 - **Market study.** Regional growth centers that have existing density levels below the level required for new regional centers at the time of the review must complete a market study to evaluate the potential for and opportunities to best support center growth. The market study must consider a planning horizon reasonably beyond the monitoring period (2025). The market study should show how the center can meet targeted levels of growth within the planning period. The jurisdiction should demonstrate its work to address opportunities identified in the market study.
- **Monitoring review of manufacturing/industrial centers.** A first monitoring review period, scheduled for 2025, will follow the next major comprehensive plan periodic update (due in 2023 and 2024) and will reoccur about every five years thereafter. At the first monitoring review in 2025, existing manufacturing/industrial centers will be expected to fully meet eligibility and designation criteria similar to new centers:
- **Local commitment.** Evidence center is a local priority and sponsor city/county has sustained commitment over time to local investments in infrastructure and transportation. Demonstrated commitment to protecting and preserving industrial uses, strategies and incentives to encourage industrial uses in the center, and established partnerships with relevant parties to ensure success of the manufacturing/industrial center.
 - **Planning.** Completion of a center plan (subarea plan, plan element or functional equivalent) that addresses regional guidance and plans for access to transportation infrastructure and economic development. Where applicable, the plan should be developed in consultation with public ports and affected governmental entities.
 - **Location.** If existing manufacturing/industrial centers are not located within a city, joint planning and annexation/incorporation are encouraged as feasible.
 - **Capital investments.** Capital investments by the local government in the center in the current or prior 6-year capital planning cycle, and commitment to infrastructure and utilities in the jurisdiction’s capital improvement program sufficient to support center growth and planned transportation infrastructure.
 - **Center criteria.** Consistent with designation criteria for new manufacturing/industrial centers in Section 4. Existing centers will remain designated if they do not meet the new center existing or planned jobs criteria, provided that the center is consistent with other criteria identified in this section.

- **Market study.** Manufacturing/industrial centers that have existing employment levels below the level required for new centers at the time of the review must complete a market study to evaluate the potential for and opportunities to best support center growth. The market study must consider a planning horizon reasonably beyond the monitoring period (2025). The market study should show how the center can meet targeted levels of growth within the planning period. The jurisdiction should demonstrate its work to address opportunities identified in the market study.
 - The board will maintain flexibility in evaluating existing centers to consider when centers are very close to the existing conditions criteria, to account from economic recessions, progress and growth, local investments or the lack of investments, and regional importance of a particular area.
 - Criteria related to physical improvements should be included in center plans, but may need to be addressed over the long-term, such as developing a complete walkable street network.

6. Countywide Centers

Each county’s countywide planning policies include criteria and processes for countywide centers, though the approach currently varies significantly by county. Through the Centers Framework Update, designation of countywide centers remains delegated to a countywide process while providing a baseline of consistent regional standards for each county to use. PSRC reviews and certifies countywide planning policies, but PSRC’s role does not include review of countywide centers.

Countywide growth centers serve important roles as places for concentrating jobs, housing, shopping, and recreational opportunities. These are often smaller downtowns, high-capacity transit station areas, or neighborhood centers that are linked by transit, provide a mix of housing and services, and serve as focal points for local and county investment. Countywide industrial centers serve as important local industrial areas. These areas support living wage jobs and serve a key role in the county’s manufacturing/industrial economy. The checklist below represents basic standards expected for countywide centers in each county. Depending on county circumstance and priorities, countywide planning policies may include additional criteria (such as planning requirements or mix of uses) or other additional standards within this overall framework. Countywide center designations will be reviewed by an established timeframe and process set by the countywide planning body.

Countywide Centers	
Countywide Growth Center	Countywide Industrial Center
Center must meet each the following criteria:	Center must meet each the following criteria:
Identified as a countywide center in the countywide planning policies	Identified as a countywide center in the countywide planning policies
Located within a city or unincorporated urban area	Located within a city or unincorporated urban area

Countywide Centers	
Countywide Growth Center	Countywide Industrial Center
<p>(cont.)</p> <p>Demonstration that the center is a local planning and investment priority:</p> <ul style="list-style-type: none"> o Identified as a countywide center in a local comprehensive plan; subarea plan recommended o Clear evidence that area is a local priority for investment, such as planning efforts or infrastructure <p>The center is a location for compact, mixed-use development; including:</p> <ul style="list-style-type: none"> o A minimum existing activity unit density of 10 activity units per acre o Planning and zoning for a minimum mix of uses of 20 percent residential and 20 percent employment, unless unique circumstances make these percentages not possible to achieve. o Capacity and planning for additional growth <p>The center supports multi-modal transportation, including:</p> <ul style="list-style-type: none"> o Transit service o Pedestrian infrastructure and amenities o Street pattern that supports walkability o Bicycle infrastructure and amenities o Compact, walkable size of one-quarter mile squared (160 acres), up to half-mile transit walkshed (500 acres) 	<p>(cont.)</p> <p>Demonstration that the center is a local planning and investment priority:</p> <ul style="list-style-type: none"> o Identified as a countywide center in a local comprehensive plan; subarea plan recommended o Clear evidence that area is a local priority for investment, such as planning efforts, or infrastructure <p>The center supports industrial sector employment:</p> <ul style="list-style-type: none"> o Minimum 1,000 existing jobs and/or 500 acres of industrial land o Defined transportation demand management strategies in place o At least 75% of land area zoned for core industrial uses o Industrial retention strategies in place o Capacity and planning for additional growth o Important county role and concentration of industrial land or jobs with evidence of long-term demand

7. Local Centers and Other Types of Centers

VISION 2040 calls for central places in all jurisdictions to support a centers-based approach to development in the region. These places range from neighborhood centers to active crossroads in communities of all sizes. These centers play an important role in the region and help define our community character, provide local gathering places, serve as community hubs, and are often appropriate places for additional growth and focal points for services.

The Regional Centers Framework recognizes the importance of these places, but does not envision a regional or county designation for all types of local centers. The designation criteria outlined in this

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document may provide a path to regional or county designation for locations that continue to grow and change over time.

Per program eligibility requirements, rural centers that participate in PSRC’s Rural Town Centers and Corridors funding competition are located in either a freestanding city or town that is outside the region’s contiguous urban growth area or a county’s unincorporated rural area. These centers are designated through a local planning process, not through the Regional Centers Framework process.

8. Military Installations

Military installations are a vital part of the region, home to thousands of personnel and jobs and a major contributor to the region’s economy. While military installations are not subject to local, regional, or state plans and regulations, PSRC recognizes the relationship between regional growth patterns and military installations, and recognizes the importance of military employment and personnel all aspects of regional planning.

Recognition of military installations in the update to VISION 2040 can better acknowledge the role these installations play in the regional economy and in regional growth patterns. Designation criteria for installations can also help establish common expectations for how the region works with and supports military installations. Stakeholders throughout the process have emphasized the need to address base transportation access to benefit surrounding communities, as well as the installations. Per federal statutes, PSRC transportation funds cannot be spent on military installations, but surrounding communities may be eligible to receive funds for projects that connect to installations.

Designation Criteria for Types of Military Installations

PSRC’s Executive Board will identify *Major Military Installations* in the update to VISION 2040, subject to adoption of the plan by the General Assembly. Major installations are defined as installations with more than 5,000 enlisted and service personnel. As of 2017, four installations met the minimum size criteria: Joint Base Lewis-McChord in Pierce County, Naval Base Kitsap–Bangor and Naval Base Kitsap–Bremerton⁴ in Kitsap County, and Naval Station Everett in Snohomish County.

This recognition in the regional plan advances active collaboration between military installations, neighboring jurisdictions, and the region. The region recognizes military installations are major employers, associated with congestion, and that regional designation can help work to alleviate impacts. Through this recognition, regional expectations include:

- Ongoing coordination between the military installation, countywide planning forum, and neighboring jurisdictions regarding planned growth, regional impacts, and implementation of multimodal transportation options
- Support for multimodal commute planning and mode split goals for installation
- Completed Joint Land Use Study or similar coordinated planning effort

⁴ For the purpose of regional centers designation, jurisdictions may count military activity towards center thresholds when the installation is directly adjacent or surrounded by the center (such as Naval Base Kitsap-Bremerton and the downtown Bremerton regional growth center).

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Smaller military installations may continue to be recognized by countywide planning forums as a type of countywide center or equivalent. The minimum size criteria for countywide center designation will be as specified by RCW 36.70a.530 and identify “federal military installation[s], other than a reserve center, that employs one hundred or more full-time personnel.” As of 2017, five installations met the minimum criteria: Naval Base Kitsap Keyport, Seattle Coast Guard Station, Naval Base Kitsap Jackson Park, Camp Murray, and Naval Base Everett – Smokey Point Support Complex.

9. Planning Requirements

PSRC’s Plan Review Manual contains guidance and requirements for comprehensive plan certification, including center subarea plans. The **Regional Center Plans Checklist** in the **PSRC’s Plan Review Manual** addresses planning expectations for center subarea plans. PSRC will work with the Regional Staff Committee to update the Plan Review Manual to amend requirements and provide best practices, with consideration for local variability.

The **Regional Growth Center Plan Checklist** will be updated to address the following topics:

- o Affordable housing, including housing targets, needs assessment, affordable housing goals, and strategies to encourage new housing production with long-term affordability
- o Displacement risk analysis and strategies to prevent or mitigate displacement
- o Transit access, including transit service, transit-dependent populations, and safe and connected pedestrian and bicycle networks
- o Equitable community engagement
- o Access to opportunity, including employment and education opportunities and neighborhood quality of life
- o Environmental justice impacts
- o Specific transportation planning investments, programs, and resources identified.
- o Availability of public services, like K-12 education, to meet needs of households with children.

The **Regional Manufacturing/Industrial Center Plan checklist** will be updated to address the following topics:

- o Equitable community engagement
- o Access to opportunity, including employment and education opportunities
- o Environmental justice impacts
- o Expectations around core industrial uses, residential encroachment, transitional buffers, and commercial and office uses that do not support manufacturing/industrial function
- o Clearly articulated long-term commitment to protect and preserve manufacturing/industrial land uses and businesses in the center
- o Specific transportation planning investments, programs, and resources identified

10. Regional Support

Funding to Support Centers

Staff will research and identify other potential funding sources or programs to support development in centers. This may include housing in regional growth centers, economic development, other capital funds, additional state resources, marketing, and other strategies. PSRC should collaborate with other agencies and funders to identify additional funding sources for designated centers. PSRC will also explore funding for centers planning and technical assistance.

Regional Center Types

The Regional Centers Framework does not establish a distinction between different types of regional centers for the purpose of PSRC’s funding framework.

11. Performance Measures

In the VISION 2040 update, PSRC will work with local governments and other stakeholders to develop performance measures for centers as a whole to evaluate success of the overall framework. Like previous monitoring studies, PSRC will lead the effort, with support and review from local governments. Performance measures should focus on a limited number of centers and consider the project outcome statements to support growth, mobility, environment, social equity and opportunity, economic development, and public health. Metrics may include overall growth goals or mode split goals for centers, level of local or regional investment, greenhouse gas emissions reductions, or other measures as appropriate, such as housing affordability, mix of uses, and health and equity.

PSRC will continue to conduct ongoing monitoring of performance measures for individual centers. This may include progress towards growth targets and mode split goals, tracking implementation actions, or tracking other measures consistent with the designation requirements.

- PSRC will publish a centers performance monitoring summary every five years in order to stay on top of regional trends in centers development.
- PSRC will review centers for performance as part of the monitoring review and prior to regional plan update years, and consider possible changes or reclassification if the local jurisdiction is not taking steps to plan and support growth in center to meet targets or goals.

12. Implementation

Procedures and Planning Expectations. The board directs staff to prepare updates to the **Designation Procedures for New Centers, Regional Center Plans Checklist**, and develop **administrative procedures** for existing centers.

Plan Updates. The board directs staff to identify issues for VISION 2040 update⁵:

- Identification of military installations a regional geography
- Preservation of industrial land, both within designated manufacturing/industrial centers and in other industrial areas in the region
- Growth goals for regional centers

Countywide Planning Policies. The board requests updates to the countywide planning policies in each county during the next GMA update cycle (by 2023/2024) to implement countywide centers and achieve consistency with the regional framework. PSRC staff should work collaboratively with countywide groups on technical assistance and implementation of updated criteria.

⁵ The framework may inform an update to the Regional Growth Strategy but does not commit the region to any particular distribution of growth or definition of regional geographies.

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PSRC Work Plan. The board directs staff to develop an implementation work plan and schedule to engage with PSRC membership and other stakeholders as appropriate on additional analysis, research, and guidance. The work plan should incorporate the following tasks to support ongoing work on regional and countywide centers:

- **Performance measures.** In consultation with jurisdictions and other stakeholders, develop framework to track performance and outcomes over time and identify challenges or barriers over time. Performance measures should consider project outcome statements to support growth, mobility, environment, social equity and opportunity, economic development, and public health. Measures may include assessment of demographic characteristics, housing affordability, employment, amenities, and access to opportunity.
- **Update growth planning guidance.** Update the guidance paper on center targets to discuss changes to growth expectations for centers and the mix of employment and residential activity envisioned in regional growth centers.
- **Market studies.** Provide additional guidance on recommended components of market studies for centers.
- **Social equity**
 - Complete additional analysis and research on displacement and displacement risk in centers.
 - Provide additional resources and best practices addressing equitable community engagement, including opportunities for local planning staff and policy-makers to learn about tools that have been successfully used by cities and counties in the region.
 - Research and recommend a best practice approach to a comprehensive equity impact review tool to address social equity through policies and implementation decisions for centers throughout the region.
- **Tribal land & centers.** Complete additional review and consultation with tribes on the role of tribal lands in the centers framework.
- **Funding opportunities.** To achieve the vision of growth in centers, research other funding opportunities and opportunities to leverage existing designations. Collaborate with other agencies and funders to identify additional funding sources to designated centers. Explore funding, including a set-aside, for centers planning and technical assistance.
- **Military installations.** Staff will research other potential funding sources or programs to support improvement of transportation corridors serving recognized military installations. PSRC, countywide groups, and local jurisdictions should continue to work with state and federal partners to secure infrastructure resources, provide support for military installations, and address impacts on surrounding jurisdictions.
- **Economic measures.** Given their important role in the regional economy, PSRC should consider additional research on economic impact measures for manufacturing/industrial centers (e.g. revenue generators, export value).
- **Projects supporting centers.** Review and develop policy guidance on types of projects that support development in centers and corridors connecting centers.
- **Number and distribution of centers.** During the VISION 2040 update, research guidelines to manage the number and distribution of centers, factoring in minimum density standards, projected growth, jurisdictional size, location within the county, competition for market share, and allocation of limited regional resources.

Peter Best

From: Peter Best
Sent: Wednesday, January 15, 2020 3:43 PM
To: jjohn136@centurytel.net
Subject: 2019 Comprehensive Plan Amendment Comment - Answers to Questions

Dear Teresa,

Thank you for submitting the below online comment regarding the Downtown Kingston (Phase 2) Comprehensive Plan amendment currently being considered by the Kitsap County Planning Commission. You asked a number of questions and I wanted to provide you with direct responses below in red. This email will be entered into the formal record as a public comment.

I own 2 pieces of property on Pennsylvania Ave NE in Kingston. They are 26180 and 26160 Pennsylvania Ave NE. I received notice of the proposed changes. After reading through most of this I have a few questions.

Parking

I see that my 2 properties are included within the new proposed high capacity transit station area. I am very concerned that my on street parking will be compromised by this. The ferry commuters already park on the street at the bottom of Pennsylvania. What will prevent them from moving up the hill and blocking my parking? I don't see any provisions for making sure the existing property owners retain the parking they currently have. Over in Seattle in some areas they provide home owners with parking credentials to make sure that they retain their parking. I have friends near the UW and I know this is possible. I may have missed this in the report. Can you explain how you will protect current home owners and their parking on the streets?

The policies and design guidelines anticipate future area-wide parking management to address on-street parking and public parking.

ADU's etc

I know that my 2 lots are each zoned for more than one residence. When I looked at the matrix to see how or if any changes applied to my properties, I couldn't find a key to the abbreviations. I have no idea what ACUP means, etc. Maybe you can tell me please if my 2 lots would be able to have ADU's or other dwelling units built on them under this new proposal.

Yes, the proposed changes would make ADUs an allowed use on each of your lots. ACUP means "administrative conditional use permit."

Height changes

35 to 45 ft. Does this apply to my properties or just to the area called Old Town Waterfront on the map?

Your properties currently have an allowed maximum height of 45'. This amendment will not change the allowed maximum height on your properties.

Alleys

If I am able to put an ADU onto one or both properties, can I use the alley to access the parking? Can the parking be in the setback?

Your parcels are not on a designated alley, but there does appear to be an existing alley right-of-way and some sort of existing access drive behind your parcels. You should be able to use this access since it is in the right-of-way, but you would need to consult with Kitsap County Public Works for more details. Parking cannot be in a setback, but there are no required setbacks in your zone (see [KCC 17.420.054](#)).

Streets

4th St has never been paved although it is in use from Pennsylvania to the alley. (From the alley to Illinois it is just a walking trail.)

4th St is in use by the large condominium complex whose main entrance is off Illinois, and by the property owner at 26190 Pennsylvania.

I propose that 4th St be paved from Pennsylvania to the alley as part of the general improvements to Kingston both aesthetically and functionally.

You would need to discuss such an improvement with the Kitsap County Public Works department since it is in the right-of-way.

Thank you for your time and consideration on this. I appreciate it.

Cheers,
Peter

Peter Best | Senior Planner
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To: Kitsap County Staff, Planning Commission Members and County Commissioners

From: Betsy Cooper

I am writing today to respond to the Request for comments on the Phase II process of the 2019 Comprehensive Plan Amendments. As some of those that will review these comments may not know me, here is a bit of background on my involvement in Kingston and planning in the County to preface the comments.

I live in the Kingston UGA. I have a Land Use and regulatory background from my prior work for New York City and King County. I have been an active member of several County Land Use Advisory Committees since 2000 including the Kingston Citizen's Advisory Committee, 3 of the UGA steering committees, a Kitsap Co. public works advisory comment in the late 2000's and most recently the UVC Advisory Committee of 2019. I was part of the conversation when the County initially applied the UVC designation to a portion of downtown Kingston to promote residential density and enhanced commercial opportunity in the downtown core. However we all have watched the lack of infill development materialized.

I have two topics for which I offer comments:

1) Staff Recommendation of Pedestrian Oriented Commercial Street Frontage

When Commissioner Gelder and DCD Initiated the 2019 Advisory Committee to look at the barriers to develop and infill progress in the Kingston UVC, the major barrier unanimously identified was the requirement of commercial development requirements implemented simultaneously with multi-story development. While this concept was originally proposed in Kingston and one other place in the county, now I believe only Kingston has retained it. It is a common planning concept in developing areas where residential density is encouraged and those densities are expected to support an influx of commercial. However it appears there needs to be a certain residential density first, before the commercial component can be expected to thrive.

The current Staff recommendation of Pedestrian Oriented Commercial Street Frontage proposes that the ground floor commercial requirement remain on Rt104 (Main street) between W. Kingston Road and Washington, and segments between Central and First on cross streets of Ohio and Iowa, W. Kingston and Washington. This proposal would retain this current barrier to development of the properties. This proposal is not moving us in a direction that will support the infill we so dearly need and it continues the pressure felt on areas outside of the UGA to develop which is counter to the principles of the UGA.

While I understand the concern of Staff that this area is nominally the 'downtown' and should not be allowed to develop in way that would preclude a walkable town center, retaining the current UVC requirements will just thwart any development, not encourage a town center.

It is possible that perhaps the Main Street Corridor could be singled out (not the side streets proposed) as an area where the preference for commercial on the first floor could continue to be nurtured. I would propose that be done but a provision requiring ONLY that the floor height on the First floor of any multistory building to be required to be that which would accommodate a commercial retrofit in the future. That would allow that corridor to be retained for commercial if and when it is economically feasible for the market to bring it in. To require a full build out of all commercial requirements on the first floor of these buildings now, OR to require such commercial requirements to all those side streets is NOT an action that is in the best interest of County's goal to encourage residential building in its newly designated County-wide Regional Transportation centers.

Let's face it; the key is to get the residential population in the UGAs. The staff's recommendation will continue to stifle that.

Therefore I request you reject the Staff's proposal as presented and either retain the Advisory Committee's recommendations in whole or revise the Staff's recommendation to scale it aerial extent and require only a commercial floor height requirement and no other commercial requirements on the first floor so that residential development can move forward now with Commercial to follow when feasible.

2) Design Standards – Page 31 of Design in the Staff Report I believe it is noted in Attachment A 2 – Parking H

As part of the discussion in the 2019 Advisory Committee Discussions about barriers to infill development in Kingston's UVC one of the other major issues identified and unanimously agreed upon as critical to address to spur development was parking requirements. The committee discussed this at length and many ideas were shared and revisions were proposed to the staff. To my knowledge many or most of the ideas discussed are under consideration now. However at the Open House in Kingston on 12/19, in conversation with Peter Best several of us realized that there is one aspect of the current Design Standards that apply to the Kingston UVC that we did not discuss but that seems to be a major problem for Kingston. That is the current standards do not appear to allow for a standalone parking structure to be built in the Kingston UVC.

During the 2019 Advisory Committee meetings many of us were surprised when we learned that the "Kingston Design Standards" had been adopted as standards rather than the 'guidelines' they had been for many years. While that is not necessarily a negative thing, through our discussions this year it clear that some revisions to those standards should be considered. Taking on that task was clearly out of our Committee's preview so we just commented on some things for the County to consider BUT, we did not discuss the specific provisions about parking structure other than we all acknowledge that a structure may be the solution to parking needs in Kingston. There were conversations about how the Port might want to do some kind of joint venture with other land owners and that perhaps one structure could address parking requirements for many smaller properties in Kingston. However if the Design Standards current prohibit such a structure unless it is multi-use facility there appears to again be a fatal flaw in the code that could be a significant barrier to addressing development in Kingston.

Therefore I request that the County look at the current provisions in Kingston for Parking Structures and specifically in the UVC Design Standards and revise them to allow for ways to site a thoughtfully designed and strategically place parking structure.

I hope you will consider these comments in your deliberations.

Sincerely,

Betsy Cooper

Peter Best

From: David Wetter <dwetter@mindspring.com>
Sent: Tuesday, January 14, 2020 4:52 PM
To: Peter Best
Subject: Re: Downtown Kingston (Phase 2) Draft Amendments

Follow Up Flag: Follow up
Flag Status: Flagged

Peter,

Thank you for getting back to me. Regarding the sidewalk widths on option "1B", I believe the right-of-way in that area is about 80 feet wide. If so, I would suggest the sidewalks widths should allow for the future flexibility of a:

- 1) Drive Lane each way,
- 2) Parallel parking each direction and,
- 3) A center landscape/tree lane.

If the drive lane is 11 feet, the parallel parking lane is 9 feet and the landscape/tree lane is 6 feet, then the sidewalks can't exceed 17 feet in an 80 foot right-of-way.

Thanks,
Dave

Sent from my iPhone

On Jan 14, 2020, at 1:31 PM, Peter Best <pbest@co.kitsap.wa.us> wrote:

Hi, Dave.

Yes, I received them and have entered it into the public comments.

In response to your first question regarding sidewalk widths on Main St/SR-104. The Kingston Complete Streets plan includes 14' sidewalks in Option 1A and 20' sidewalks in Option 1B for the segment between Washington Blvd and Iowa Ave. Thank your for pointing this out, I see that further clarification is warranted. The intention was for the complete streets plan to govern sidewalk widths where applicable.

In response to your second question regarding access. The design guidelines section you reference states "Parcels **without alley access** and less than 150 feet in width **may shall** be required to share access **and coordinate internal circulation** with adjacent parcels **whenever possible**." For a parcels less than 150 feet in width and having no alley access, the first preference is to share access with an adjacent parcel and coordinate internal circulation. If that is not possible, then direct access to the street would be allowed.

Thank you for your comments. These will likely lead to suggested changes in the proposal.

Sincerely,
Peter

Comment 3.3

Peter Best | Senior Planner
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From: Dave Wetter <dwetter@mindspring.com>
Sent: Wednesday, January 8, 2020 1:06 PM
To: Peter Best <pbest@co.kitsap.wa.us>
Subject: FW: Downtown Kingston (Phase 2) Draft Amendments

Peter,

I never heard back from you on either of these two e-mails. Did you not receive them?

Please advise.

Thanks,
Dave

From: Dave Wetter [<mailto:dwetter@mindspring.com>]
Sent: Saturday, December 28, 2019 3:40 PM
To: 'Peter Best' <pbest@co.kitsap.wa.us>; 'almostcandid@me.com' <almostcandid@me.com>; 'Betsycooper1@gmail.com' <Betsycooper1@gmail.com>; 'jetw@windermere.com' <jetw@windermere.com>; 'jon@orminc.com' <jon@orminc.com>; 'kcacchair@gmail.com' <kcacchair@gmail.com>; 'Langwithn52@gmail.com' <Langwithn52@gmail.com>; 'mike@fphconstruction.com' <mike@fphconstruction.com>; 'Rlanning360@gmail.com' <Rlanning360@gmail.com>
Cc: 'Angie Silva' <ASilva@co.kitsap.wa.us>; 'Dave Ward' <dward@co.kitsap.wa.us>; 'Liz Williams' <lawilliams@co.kitsap.wa.us>
Subject: RE: Downtown Kingston (Phase 2) Draft Amendments

Peter,

A few more comments:

1) At the Open House, there were some comments about a pedestrian mall on the main street after 104 has been relocated to 1st street. The comments were about 20 foot wide sidewalks in that area. I don't know the origin of that comment because the proposed amendment clearly states a 12 foot wide preferred width and an 8 foot wide minimum. Could you please clarify?

2) Page 29, B On-Site circulation and parking 1(c): limits driveways on secondary roads to no more than one per 150 feet. Please note that there are several narrower properties on Illinois, Iowa and Ohio Avenues that have no alley and, consequently, under this rule would be denied a driveway. Is that the intention?

Comment 3.3

3) Page 31, Parking 2(h): “Line structured parking garages with a perimeter of tenant commercial spaces or mixed uses....”. As you may recall from our working group discussions, one of the options to address parking requirements on small commercial lots was the development of a consolidated parking garage. The most likely current locations based on land availability would be on Port property on the WSF site at Iowa and 1st street.

Both of these sites are outside of the proposed Pedestrian-Oriented Commercial Street Frontage plan on Appendix B. Consequently, they are less likely to be commercially viable. If the objective is to obscure the visual impact of a parking garage, the more practical solution might be to address the architectural treatment on the exterior of the first floor.

Please advise and let me know if these comments and the ones on my earlier e-mail below adequate or if I need to re-enter them on the website.

Thanks,
Dave

From: Dave Wetter [<mailto:dwetter@mindspring.com>]
Sent: Friday, December 27, 2019 2:50 PM
To: 'Peter Best' <pbest@co.kitsap.wa.us>; 'almostcandid@me.com' <almostcandid@me.com>; 'Betsycooper1@gmail.com' <Betsycooper1@gmail.com>; 'jetw@windermere.com' <jetw@windermere.com>; 'jon@orminc.com' <jon@orminc.com>; 'kcacchair@gmail.com' <kcacchair@gmail.com>; 'Langwithn52@gmail.com' <Langwithn52@gmail.com>; 'mike@fphconstruction.com' <mike@fphconstruction.com>; 'Rlanning360@gmail.com' <Rlanning360@gmail.com>
Cc: 'Angie Silva' <ASilva@co.kitsap.wa.us>; 'Dave Ward' <dward@co.kitsap.wa.us>; 'Liz Williams' <lwilliams@co.kitsap.wa.us>
Subject: RE: Downtown Kingston (Phase 2) Draft Amendments

Peter,

This “slightly revised version” included the added clause (below) by the staff on page 40 under design character:

“The ground floor of Buildings along pedestrian oriented commercial street frontages (see appendix B) shall be built for the commercial use or eventual conversion to commercial occupancy along the street frontage. These commercial spaces may be initially used for residential use.”

This clause caused quite a discussion between you and some of the above committee members who were at the 12-19-19 Kingston Open House. Some of the angst was around three points:

1) This particular issue was discussed in our working group as a barrier to development because cost of building to the commercial code for initial residential use was too high to make a project feasible. An example was discussed of a UVC project that was shelved after all the A & E work was completed, partially due to the fact of these commercial requirements.

2) The area of “pedestrian oriented commercial street frontages” (appendix B) appeared to all to be way in excess of what would be market driven. Particularly, on the side streets of Highway 104).

Comment 3.3

- 3) The clause is vague and needs to be more specific regarding the requirements to be commercial. For example, does that mean the initial development will require a:
- a) Commercial grade fire sprinkler system?
 - b) Commercial grade HVAC?
 - c) 10 foot ceiling on the ground floor?
 - d) 5/8 " drywall?

Does "...or eventual conversion to commercial occupancy..." mean that all of the above could be implemented at that later conversion date, even if that did not appear to be practical to the DCD staff?

Similar to a binding agreement to defer sidewalk construction until a later date, could a binding agreement concept or note to the title be considered for commercial upgrade when, and if, that commercial use becomes a reality?

Peter, the devil is always in the details. For this concept not to continue as a barrier to development, it needs to be more specific.

Thanks,
Dave

From: Peter Best [<mailto:pbest@co.kitsap.wa.us>]
Sent: Wednesday, November 27, 2019 2:03 PM
To: almostcandid@me.com; Betsycooper1@gmail.com; Dwetter@mindspring.com; jetw@windermere.com; jon@orminc.com; kcacchair@gmail.com; Langwithn52@gmail.com; mike@fphconstruction.com; Rlanning360@gmail.com
Cc: Angie Silva <ASilva@co.kitsap.wa.us>; Dave Ward <dward@co.kitsap.wa.us>; Liz Williams <lwilliams@co.kitsap.wa.us>
Subject: RE: Downtown Kingston (Phase 2) Draft Amendments

All:

Attached is a slightly revised version of the proposed amendment. There was one new provision (Section IV.A.1.c in this draft) that did not get transferred into the version sent to you on Monday. My apologies for any inconvenience this may cause. The rest of the document is unchanged.

I hope you all have a wonderful Thanksgiving.

Sincerely,
Peter

Peter Best | Senior Planner
Kitsap County Department of Community Development
[Planning and Environmental Programs](#)
619 Division St, MS 36
Port Orchard, WA 98366
(360) 337-7098 | pbest@co.kitsap.wa.us

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From: Peter Best

Sent: Monday, November 25, 2019 1:31 PM

To: almostcandid@me.com; Betsycooper1@gmail.com; Dwetter@mindspring.com; jetw@windermere.com; jon@orminc.com; kcacchair@gmail.com; Langwithn52@gmail.com; mike@fphconstruction.com; Rlanning360@gmail.com

Cc: Angie Silva (asilva@co.kitsap.wa.us) <asilva@co.kitsap.wa.us>; Dave Ward (dward@co.kitsap.wa.us) <dward@co.kitsap.wa.us>; Liz Williams <lwilliams@co.kitsap.wa.us>

Subject: Downtown Kingston (Phase 2) Draft Amendments

Importance: High

Dear Kingston UVC Workgroup Members:

As promised to the Workgroup, attached please find the Phase 2 draft amendments for Downtown Kingston. These will be publicly released with a full staff report on 12/10 when the public comment period begins. Please do not distribute the attached document beyond workgroup members until 12/10.

This document includes everything you recommended (in blue text) and additional Department recommendations (in red text). Department recommendations were developed using feedback from a variety of staff in the Departments of Community Development and Public Works and are intended to:

- Further clarify code and design standards.
- Improve administration.
- Improve consistency with other parts of Kitsap County Code and other recent planning activities for Kingston (i.e. Complete Streets Plan).
- Improve consistency with changes to the regional growth management plan (VISION 2050).
- Achieve the long-term vision for a pedestrian-oriented downtown Kingston.

In a few cases, the Department has provided an alternative recommendation to a workgroup recommendation. These are mostly a re-phrasing of the recommendation intended to achieve the same outcome or to expand the idea to an area larger than the UVC zone.

In a few cases, the Department has proposed additional provisions, including the following:

- Designated pedestrian-oriented commercial street frontages (Design Standards - Appendix B)
- Alley plan (Design Standards - Appendix C)
- Increasing building height in the Lindvog Commercial district from 35' to 45' for the same reasons as in the UVC portion of the other design districts.

Let me know if you have any questions. If you would like to get together as a group to discuss the Department recommendations, please let me know.

Sincerely,
Peter

Peter Best | Senior Planner
Kitsap County Department of Community Development
[Planning and Environmental Programs](#)
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Port Orchard, WA 98366
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Comment 3.3

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Kitsap County Department of Community Development

Staff Report and Recommendation Annual Comprehensive Plan Amendment Process for 2019 Downtown Kingston (Phase 2)

Summary

Report Date	12/13/2019
Hearing Date	1/7/2020
Amendment Type	County-sponsored Amendment
Description	<p>The proposed amendment includes the following changes to:</p> <p><u>Kitsap County Code</u></p> <ul style="list-style-type: none"> • Repeal redundant design standards • Define a high capacity transit station area around the Kingston ferry • Create new parking standards for high capacity transit station areas • Update allowed uses for the Kingston UVC zone <p><u>Design Standards for the Community of Kingston</u></p> <ul style="list-style-type: none"> • Improve clarity and administration as well as consistency with other development regulations • Update the applicability of the design standards • Incorporate the Kingston Complete Streets Plan • Designate pedestrian-oriented commercial street frontages • Designate alleys • Add off-site flexibility for some open space/landscaping requirements • Incentivize alternatives to surface parking and count some on-street parking • Increase allowed building heights in the Old Town/Waterfront and Lindvog Commercial design districts from 35-feet to 45-feet • Repeal the required palette of exterior colors <p><u>2016 Kitsap County Comprehensive Plan – Kingston Subarea Plan (Chapter 8)</u></p> <ul style="list-style-type: none"> • Reorganize, update, and retire relevant goals and policies
Geographic Area Affected	Kingston Unincorporated UGA
SEPA	Determination of Non-Significance
Department Recommendation	Adopt as proposed, including the Kingston UVC Workgroup recommendations with the Department’s modifications, additions, and alternatives

This report and recommendation are based on information available at the time of publication. If new relevant and material facts are discovered, this staff report will be revised and the department recommendation may change.

1. Background

The Kitsap County Comprehensive Plan describes the 20-year vision for unincorporated Kitsap County and how that vision will be achieved. The plan covers land use, economic development, environment, housing and human services, transportation, capital facilities and utilities as well as parks, recreation, and open space. The Comprehensive Plan is mandated by the Washington State Growth Management Act (GMA, RCW 36.70A).

A. Authority

The GMA mandates that Kitsap County's Comprehensive Plan and development regulations be reviewed and, if needed, revised at least every 8-years [RCW 90.70A.130(5)]. The most recent Kitsap County 8-year update concluded with the adoption of the 2016 Kitsap County Comprehensive Plan on June 27, 2016 by Ordinance 534-2016. The GMA also mandates that Kitsap County's Comprehensive Plan and development regulations be subject to continuing review and evaluation, allowing for annual amendments to the Comprehensive Plan and development regulations [RCW 36.70A.130(1)].

Kitsap County Code sets forth a process and criteria for making amendments to the Comprehensive Plan [KCC 21.08]. In making amendments, the County must consider:

- Whether the proposed amendments are consistent with and supports other plan elements and or development regulations, and if not, what additional amendments to the plan and/or development regulations will be required to maintain consistency;
- Whether the proposed amendment to the plan and/or regulation will more closely reflect the goals and policies of the Comprehensive Plan;
- Whether the proposed amendment is consistent with the Kitsap County-wide Planning Policies; and
- Whether the proposed amendment complies with the requirements of the GMA.

The revised final docket adopted by the Board of County Commissioners on August 21, 2019 (Resolution No. 133-2019) allows for consideration of this amendment during Kitsap County's annual Comprehensive Plan amendment process for 2019.

B. Proposed Amendment

The proposed amendment is provided in Attachment A, which includes the recommendations of the Kingston UVC Workgroup as modified by all of the Department's recommendations. This amendment expands on and implements the Phase 1 (Kingston UVC) amendment adopted in 2018, is intended to remove regulatory barriers to achieving the existing adopted vision for downtown Kingston, and includes the following substantive changes:

- **Kitsap County Code**
 - Repeal redundant design standards in Chapter 17.480 (UVC Design Criteria) and move non-redundant design standards to Chapter 17.470 (Multifamily Design Criteria) and the Design Standards for the Community of Kingston (Amendments #1 & #2)
 - Define a high capacity transit station area around the Kingston ferry terminal consistent with the assumptions for the proposed VISION 2050 regional growth Plan (Amendment #5)
 - Create new parking standards for high capacity transit station areas with generally reduced parking requirements, increased flexibility for off-site parking, and incentivized alternatives to surface parking (Amendments #6 & #7)
 - Update KCC 17.410.044 (allowed uses) for the Kingston UVC zone (Amendment #8)

- **Design Standards for the Community of Kingston** (all in Amendment #2, Exhibit 1)
 - Revisions to improve the clarity of the design standards, administration of the design standards, and consistency of the design standards with other development regulations (throughout Exhibit 1)
 - Update the applicability of the design standards to: (Introduction, Section A)
 - Remodels and additions
 - Single-family dwellings, duplexes, accessory dwelling units, and accessory living quarters
 - Incorporate the existing adopted Kingston Complete Streets Plan (throughout Exhibit 1)
 - Designate pedestrian-oriented commercial street frontages and require first floors are built for commercial use or eventual conversion to commercial use after initial residential use (Sections II.A.1.a, IV.A.1.c, & IV.B.1.e; Appendix B)
 - Designate alleys and clarify access priorities (Sections II.A.1.a & III.B.1.c; Appendix C)
 - Increase flexibility for some open space and landscaping requirements to be located off-site (Section II.A.4.c)
 - Incentivize alternatives to surface parking and count certain on-street parking toward commercial parking requirements (Sections III.B.2.i & IV.C.1.a)
 - Increase maximum allowed building heights in the Old Town/Waterfront and Lindvog Commercial Design Districts from 35-feet to 45-feet with a required 10-foot upper story setback for buildings over 35-feet high (Sections IV.A.2 & IV.C.1.a)
 - Repeal the required palette of exterior colors (Section IV.D.2; Appendix D)

- **2016 Kitsap County Comprehensive Plan – Kingston Subarea Plan** (Chapter 8)
 - Reorganize, update, and retire goals and policies relevant to the code described above (Amendment #9)

C. Geographic Description

This amendment to the Kitsap County Comprehensive Plan affects the Downtown portion of the Kingston Unincorporated Urban Growth Area.

2. Department Recommendation

Having analyzed the proposed amendment and other alternatives, if applicable, the Department recommends:

- Adoption of the amendment:
 - as proposed above
 - as described in Alternative ___ below
 - with revisions described below
 - with conditions described below
- Deferral of the amendment to a future docket
- Denial of the amendment

A. Revisions

None.

B. Conditions

None.

C. Rational

The primary reasons for considering these amendments are to:

- Implement existing policies in the Kingston Subarea Plan which call for various reviews and updates.
- Address the identified barriers that have stagnated development in downtown Kingston, which are impeding the achievement of the existing vision for a pedestrian-oriented downtown and accommodation of planned growth in Kingston.
- Increase the availability as well as the diversity in size and cost of market rate housing in downtown Kingston
- Improve consistency with regional and countywide planning policies.
- Improve the clarity, administration, and consistency of the Kingston Subarea Plan and development regulations.

3. Other Alternatives Considered

No formal alternatives were proposed during the development of this amendment. The Kingston UVC Workgroup considered preliminary alternatives for many topics as part of their deliberative process but reached consensus on all recommendations and therefore did not propose formal alternatives.

One issue that could be viewed as having formal alternatives is the one instance where the Department's recommendations and the Workgroup's recommendations directly disagree:

- Designation of pedestrian-oriented commercial street frontages (Amendment #2, Exhibit 1 - Section IV.A.1.c and Appendix B)

The Workgroup specifically recommended against designating commercial street frontages for first floor commercial development, even if those commercial spaces could be put first into residential use before eventual conversion to commercial use as Kingston grows. Given existing development patterns in the downtown area, the Workgroup felt the market would likely maintain and build commercial street frontages in at least most of the small core area of downtown without the need for regulations that could be a barrier to development being responsive to market demands like in recent years when mixed-use development had been required throughout the UVC zone. This mixed-use requirement was a barrier to development in the UVC zone and was removed during the Phase 1 amendments in 2018.

The Department agrees with the Workgroup that additional residential development needs to occur in downtown Kingston before the market would likely support much additional commercial development (see Market Assessment and Priority Policy Barriers in Attachment C2 with showing little commercial development; and growth projection for only 23,600 square feet of retail space throughout Kingston between 2013 and 2025 in Attachment C7). The Department has recommended the designation of pedestrian-oriented commercial street frontages as a means to preserve the opportunity for a small core area of pedestrian-oriented commercial streets within the downtown over the long-term while providing flexibility for property owners to maximize occupancy of their buildings.

4. Analysis

Amendments to the Comprehensive Plan must be consistent with the criteria outlined in Kitsap County Code (KCC) Chapter 21.08. Applicable criteria are analyzed below. A summary of the State Environmental Policy Act (SEPA) review of this amendment is located at the end of this section.

A. General Decision Criteria (KCC 21.08.070.A)

For each proposed amendment to the Comprehensive Plan, the Planning Commission in reaching its recommendation, and the Board of Commissioners in making its decision, shall develop findings and conclusions, which demonstrate:

1. *How circumstances related to the proposed amendment and/or the area in which the property affected by the proposed amendment is located have substantially changed since the adoption of the Comprehensive Plan or applicable development regulations;*

Staff Analysis: The Kingston Subarea Plan in the 2016 Comprehensive Plan contains policies (e.g. Policies 36, 37, 41, 42, 50, & 51) to consider updates to the Design Standards for the Community of Kingston, parking requirements, and code provisions to support redevelopment in Kingston. This amendment implements those policies.

The following substantial changes in circumstances are related to this amendment:

- Policy and regulatory barriers were identified by the Kingston UVC Workgroup that have impeded the ability to achieve the existing vision for the development and planned growth applicable to downtown Kingston.
 - Staff from the Kitsap County Departments of Community Development and Public Works identified administrative clarifications and improvements to the Kitsap County Code and the Kingston Subarea Plan as well as the need to integrate the Kingston Complete Streets Plan (adopted 8/22/2016) into the Design Standards for the Community of Kingston.
 - Revisions of the regional growth strategy proposed in VISION 2050, which is the regional growth plan for Central Puget Sound. Specifically, the designation of the Kingston Unincorporated Urban Growth Area (UGA) as a High Capacity Transit Community assumes a high capacity transit station area will be designated around the Kingston ferry terminal.
2. *How the assumptions upon which the Comprehensive Plan is based are no longer valid, or there is new information available which was not considered during the adoption of, or during the last annual amendment to, the Comprehensive Plan or development regulations; and*

Staff Analysis: See 4.A.1 above.

3. *How the requested redesignation is in the public interest and the proposal is consistent with the Kitsap County Comprehensive Plan.*

Staff Analysis: Not applicable – this is not a redesignation request.

B. Additional Decision Criteria (KCC 21.08.070.B)

In addition to the findings and conclusions above, for each proposed text amendment, the Planning Commission in reaching its recommendation, and the Board of Commissioners in making its decision, shall develop findings and conclusions which consider:

- 1. Whether the proposed amendment is consistent with and supports other plan elements and/or development regulations and, if not, what additional amendments to the plan and/or development regulations will be required to maintain consistency;*

Staff Analysis: The proposed amendment increases the consistency of Kitsap County Code with the existing vision for downtown Kingston in the Kingston Subarea Plan, implements Kingston Subarea Plan polices (e.g. Policies 36, 37, 41, 42, 50, & 51) to update various code provisions, and integrates the Kingston Complete Street Plan into code as required by Kingston Subarea Plan policies. The proposed amendment is consistent with and supports other plan elements.

Land capacity estimates based on the proposed amendments indicate the population and employment capacity of the Kingston UGA will not change significantly. The population capacity of the UGA is estimated to increase by 58 people to 2,831 (1.3% below the UGA target). The employment capacity of the UGA is estimated to decrease by 33 jobs to 718 (14.7% above the UGA target). Countywide capacities are estimated to remain within +/- 5% of adopted targets. Kitsap County considers planned capacities within the +/- 5% margin of error of adopted targets to be in balance. While employment capacity is technically outside of the County's margin of error, this amendment will decrease the capacity surplus from 20.3% to 14.7% and the UGA employment capacity is only 56 jobs above the County's margin of error. Therefore, due specifically to the small numbers associated with this surplus capacity, the Department believes that further adjustments to the UGA regarding employment capacity are not warranted at this time and should be reviewed during the next required update of the Comprehensive Plan due in 2024.

The updated land capacity estimates are largely consistent with the assumptions used for the Kitsap County Capital Facilities Plan and will not require new improvements beyond those already planned.

2. *Whether the proposed amendment to the plan and/or regulation(s) will more closely reflect the goals, objectives and policies of the Comprehensive Plan and reflect the local circumstances of the county;*

Staff Analysis: The proposed amendment more closely reflects the goals, objectives, and policies of the Comprehensive Plan, including the Kingston Subarea Plan, and the local circumstances of downtown Kingston. As noted above, many of the amendments implement actions spelled out in existing subarea plan policies and/or address regulatory barriers to achieving the existing vision and planned growth for a pedestrian-oriented downtown in Kingston. The proposed amendment is expected to increase the availability as well as the diversity in size and cost of market rate housing in downtown Kingston.

The proposed amendment also more closely aligns development regulations with the purpose statement of the Kingston UVC zone, which is:

- To foster a development pattern offering direct, convenient pedestrian, bicycle, and vehicular access between residences and businesses, in order to facilitate pedestrian and bicycle travel and reduce the number and length of automobile trips.
- To provide for a compatible mix of single-family, multifamily housing and neighborhood commercial businesses and services, with an emphasis on promoting multi-story structures with commercial uses generally located on the lower floors and residential housing generally located on upper floors.
- To promote a compact growth pattern to efficiently use developable land within UGAs, to enable the cost-effective extension of utilities, services and streets, to enable frequent and efficient transit service, and to help sustain neighborhood businesses.
- To foster the development of mixed use areas that are arranged, scaled and designed to be compatible with surrounding land.

3. *Whether the proposed amendment is consistent with the Kitsap County-wide Planning Policy;*

Staff Analysis: The proposed amendment is consistent with the [Kitsap County-wide Planning Policies](#) (CPPs; adopted on 5/11/2015 by Kitsap County Ordinance 522-2015). The proposed amendment is specifically consistent with the following relevant CPPs:

CW.1 (RE: Growth Patterns)

Roles of Cities and Urban Growth Areas/Urban Communities

- a. The primary role of Kitsap's urban communities is to encourage growth, through new development, re-development and in-fill. Population growth should be directed to Cities, urban growth areas and centers with a transportation system that connects people with jobs and housing.

b. Each of Kitsap's urban communities should foster its unique vision as a high quality place to live and work, through urban design, historic preservation, and arts that improve the natural and human-made environments; promote healthy lifestyles; contribute to a prosperous economy; and, increase the region's resiliency in adapting to changes or adverse events.

...

CCOD.4 (RE: Contiguous, Compatible, and Orderly Development)

Community design and development: Strategies should promote orderly development that reflects the unique character of a community and encourages healthy lifestyles through building and site design and transportation connectivity. In addition, sustainable economic and environmental development techniques should be utilized to enhance the quality of life:

- a. Utilize design strategies to ensure that changes in the built environment provide continuous and orderly development.
- b. Encourage development that reflects unique local qualities and provides an economic benefit to the community.
- c. Design mixed use developments and local street patterns to improve the environment for overall mobility and accessibility to and within the development through multi-modal transportation options that serve all users.
- d. Design of transportation networks should fit within the context of the built and natural environment, enhancing the community, connectivity, and physical activity in the area community wide and specifically in designated growth centers and high transit areas.

...

T.1 (RE: Transportation)

Strategies to optimize and manage the safe use of transportation facilities and services:

...

- d. The County and the Cities should develop and implement access management regulations that provide standards for driveway spacing and delineation, and encourage the joint use of access points where practical.
- e. The County and the Cities shall actively seek opportunities to share facilities, expertise, and transportation resources, such as multiple use park & ride/parking lots or shared traffic signal maintenance responsibility.

T.4 (RE: Transportation)

Recognizing that the County and the Cities each encompass a range of development and density patterns, each jurisdiction shall designate its Centers consistent with the criteria set forth in Element C of the Countywide Planning Policies. The following policies relate to planning guidelines to support transit and pedestrian travel appropriate to each type of urban and rural development or re-development:

- a. The County and the Cities shall each prepare development strategies for their Designated Centers that encourage focused mixed use development and mixed type housing to achieve densities and development patterns that support multi-modal transportation.
- b. In Urban Growth Areas, comprehensive plans should promote pedestrian- and transit oriented development that includes access to alternative transportation and, in the interest of safety and convenience, includes features, such as lighting, pedestrian buffers, sidewalks, and access enhancements for physically challenged individuals.

...

AH.1 (RE: Affordable Housing)

...

d. The County and the Cities should each identify specific policies and implementation strategies in their Comprehensive Plans and should enact implementing regulations to provide a mix of housing types and costs to achieve identified goals for housing at all income levels, including easy access to employment centers.

e. The County and the Cities shall incorporate a regular review of public health, safety, and development regulations pertaining to housing implementation strategies to assure that:

...

ii. regulations are streamlined and flexible to minimize additional costs to housing.

AH.2 (RE: Affordable Housing)

Recognizing that the market place makes adequate provision for those in the upper economic brackets, each jurisdiction should develop some combination of appropriately zoned land, regulatory incentives, financial subsidies, and/or innovative planning techniques to make adequate provisions for the needs of middle and lower income persons.

AH.4.c (RE: Affordable Housing)

Provision of affordable housing for households below 120% countywide median income should include:

...

ii. provision for a range of housing types such as multi-family, single family, accessory dwelling units, cooperative housing, and manufactured housing on individual lots and in manufactured housing parks;

iii. housing design and siting compatible with surrounding neighborhoods;

...

ED.2 (RE: Economic Development)

The role of government agencies in assuring coordinated, consistent efforts to promote economic vitality and equity throughout Kitsap County:

...

b. The County and the Cities shall encourage the full utilization/development of designated industrial and commercial areas. The County and the Cities shall promote revitalization within existing developed industrial and commercial areas to take advantage of the significant investments in existing buildings and infrastructure.

4. *Whether the proposed amendment complies with the requirements of GMA, state and local laws and other applicable inter-jurisdictional policies or agreements; and*

Staff Analysis: The proposed amendment is consistent with the requirements of GMA, state and local laws and other applicable inter-jurisdictional policies and agreements, including specifically the following relevant requirements:

- The Kitsap County Comprehensive Plan must be consistent with the Multicounty Planning Policies (MPPs) adopted by the Puget Sound Regional Council, to which Kitsap County is a member. [see [WAC 365-196-305\(8\)](#)] To be consistent with the following MPP and the planning assumptions for the High Capacity Transit Community designation for the Kingston UGA, the County should establish a High Capacity Transit Station Area within the Kingston UGA around the Kingston Ferry Terminal.

MPP-RGS-1

Implement the Regional Growth Strategy through regional policies and programs, countywide planning policies and growth targets, and local plans.

5. *An explanation of why language should be added to the Comprehensive Plan or why existing language should be modified or deleted.*

Staff Analysis: The proposed amendment to the Comprehensive Plan and Kitsap County Code will:

- Implement the policies of the Kingston Subarea Plan;
- Better achieve the existing vision and planned growth for a pedestrian-oriented downtown in Kingston;
- Increase the availability as well as the diversity in size and cost of market rate housing in downtown Kingston;
- Improve consistency with regional and Countywide planning policies; and
- Improve the administration of the Code, including the Design Standards for the Community of Kingston.

C. State Environmental Policy Act (SEPA)

The Kitsap County SEPA official issued a SEPA threshold determination of non-significance (DNS; Attachment B1) for this amendment after having reviewed the SEPA environmental checklists prepared for this amendment (Attachment B2). The SEPA official's review found that this amendment is not related to or dependent on any of the other proposed Comprehensive Plan amendments and therefore an independent SEPA threshold determination was made regarding this amendment.

Notice of this SEPA threshold determination was:

- Filed with the Washington State Department of Ecology [SEPA Register](#);
- Published in the Kitsap Sun newspaper; and
- Will be integrated with other public announcements.

The SEPA threshold determination and environmental checklist was also distributed to agencies with jurisdiction, the Department of Ecology, affected tribes, and each local agency or political subdivision whose public services would be changed as a result of implementation of the proposal.

5. Public Involvement and Outreach

Kitsap County's public involvement and outreach in support of this amendment has exceeded the requirements of the Growth Management Act (RCW 36.70A) and Kitsap County Code (KCC 21.08).

A. Prior Public Involvement and Outreach

Prior public involvement and outreach regarding the 2019 docket, including this amendment, has included the following:

- An [Online Open House](#) with information about previous, current, and upcoming phases of the 2019 amendment process.
- A public comment period (11/1/2018 – 12/11/2018) and a public hearing by the Kitsap County Board of Commissioners (12/10/2019) while setting the initial docket of amendments. Notifications and announcements regarding this comment period and public hearing included the following:
 - Legal notice published in the Kitsap Sun newspaper (11/30/2018);
 - Broadcast announcements via email, text message, Facebook.com, Twitter.com, and Nextdoor.com; and
 - Formal letters to Tribes with usual and accustomed area in Kitsap County.
- Legal notice announcing the docket of amendments was published in the Kitsap Sun newspaper (1/5/2019).

Prior public involvement and outreach regarding the development of this amendment has included the following:

- The appointment of the Kingston UVC Workgroup in 2018 by Commission Gelder and Workgroup meetings in 2018 and 2019 (see Attachment C2).
- Several meetings with the Kingston stakeholders group, which is coordinated by the Kingston Chamber of Commerce.
- The formal consideration process associated with the Phase 1 amendment in 2018, which set some policy direction for this Phase 2 amendment.

B. Current Public Involvement and Outreach

This staff report provides, in full, the proposed amendment and analysis of the amendment for review by the public and the Kitsap County Planning Commission.

Visit the [Online Open House](http://tinyurl.com/kitsap2019cpa) (<http://tinyurl.com/kitsap2019cpa>) to learn more about the 2019 annual amendment process, important dates and deadlines, and how to participate in the process, including:

- Attending an open house in North Kitsap, Central Kitsap, or South Kitsap.
- Attending Planning Commission meetings.
- Testifying at the Planning Commission's public hearing.
- Submitting written comments.

A new comment period regarding the proposed amendment (Attachment A), this staff report, and the SEPA determination (Attachment B1) opened on the date this report was published. To be included in the official record, written comments must be submitted to the Department of Community Development before the deadline using one of the following methods:

- Entered [online via computer or mobile device](#) (preferred method).
- Emailed to CompPlan@co.kitsap.wa.us.
- Mailed to 614 Division St - MS36, Port Orchard, WA 98366.
- Dropped off at the Permit Center at 619 Division St, Port Orchard.
- Dropped off at one of the scheduled open houses.
- Submitted to the clerk at a scheduled public hearing.

Notifications and announcements regarding this comment period and the Planning Commission's public hearing will include:

- Legal notice published in the Kitsap Sun newspaper.
- Broadcast announcements via email, text message, Facebook.com, Twitter.com, and Nextdoor.com.
- Notice signs posted on site-specific amendment properties.
- Notices mailed to property owners near site-specific amendments and geographically specific amendments covering smaller areas.
- Formal letters to Tribes with usual and accustomed area in Kitsap County.

C. Future Public Involvement and Outreach

Additional public involvement and outreach are anticipated to occur during October through December when the Kitsap County Board of Commissioners will be considering the amendments for adoption.

6. **Staff Contact**

Report prepared by:



Peter Best, Senior Planner
 (360) 337-7098
 PBest@co.kitsap.wa.us

Report approved by:



Dave Ward, Manager
 Planning & Environmental Programs
 Department of Community Development

7. **Attachments**

A. Proposed Amendment

- Amendments to Kitsap County Code and the Comprehensive Plan
- Exhibit 1 – Amendments to the Design Standards for the Community of Kingston
- Exhibit 2 – Proposed High Capacity Transit Station Area Map

B. State Environmental Policy Act (SEPA)

1. SEPA Determination
2. SEPA Checklist

C. Supplemental Materials

1. Informational Maps

- Map 1 – Vicinity
- Map 2 – Aerial Photo (2017)
- Map 3A – Critical Areas
- Map 3B – Critical Aquifer Recharge Areas
- Map 4 – Current Land Use Designation Map
- Map 5 – Current Zoning Classification Map
- Legend for Maps 1-3
- Legend for Map 4
- Legend for Map 5

- Map 6A – Non-Motorized Existing Facilities
 - Map 6B – Non-Motorized Network with Implementation of Complete Streets Projects
 - Map 7 – Current Allowed Maximum Building Heights
 - Map 8 – Existing Uses (Kingston UVC Zone)
 - Map 9 – Current Zoning (Kingston Urban Growth Area)
2. Process Used for Developing the Amendment
 - Development Process Summary
 - Summary of Preliminary Market Assessment
 - Summary of Preliminary Infrastructure Assessment
 - Summary of Preliminary Policy & Regulatory Assessment
 - Workgroup’s List of Potential Barriers and Prioritization Exercise Results
 3. An Introduction: Principle Development Regulations Subject to the 2018 UVC Review and Summary of Existing Development Standards
 4. Summary Comparison of Allowed Uses by Community
 5. Summary Comparison of Parking Standards by Community
 6. Parking Information
 - Article: “People over Parking: Planners are Reevaluating Parking Requirements for Affordable Housing”, in *Planning* (October 2018)
 - Kingston Stakeholders Parking Committee – Historical Summary
 - Kingston Stakeholders – 2007 Downtown Parking Survey Data
 - Kingston Complete Streets – 2015 Parking Study Summary and Data
 7. Retail Projections (2013-2025)
 8. PSRC Planning for Whole Communities Toolkit Excerpts
 - Parking Management
 - Pedestrian-Oriented Design
 9. 2019 Kingston UGA Land Capacity Estimate
 10. Kingston Complete Streets Plan (Not attached due to size. The full report is available online here: <https://www.kitsapgov.com/pw/6APages/Transportation-Planning.aspx>)

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Attachment A

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Amendments

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Readers Guide

Each of the major sections of this document begin with an introduction to help provide context for the reader. Information notes are also provided to the right of certain proposals to provide additional context for the reader. These introductions and informational notes will not be codified.

In the amendments below, the existing language is provided, proposals to remove content are indicated with ~~strikeouts~~, and proposals to add new content are indicated with underlines.

Changes recommended by the Kingston UVC Workgroup, an appointed citizen advisory group, are indicated in **blue text**. Additional changes recommended by the Department of Community Development, including changes to the Workgroup’s recommendations, are indicated in **red text**. Where needed for clarity, tables are provided with alternative recommendations.

Kingston Design Standards

Introduction

Currently there are two highly redundant and somewhat conflicting design standards applicable to the UVC zone in KCC 17.480 and in a separate document (adopted by ordinance) titled the Design Standards for the Community of Kingston. In order to clarify the design standards, the following are proposed in this amendment:

- Multi-family design standards in KCC 17.480 that are currently adopted by reference in KCC 17.470 for use in the UCR, UM, and UH zones are preserved by moving them to KCC 17.470. (See Amendments #1.A – 1.D)
- Design standards in KCC 17.480 that are not redundant and do not conflict with the Design Standards for the Community of Kingston are incorporated into the Design Standards for the Community of Kingston. (See Amendment #2.B and Exhibit 1)
- Remove the redundant and conflicting design standards in KCC Chapter 17.480 by repealing the chapter. (See Amendment #1.E)

Additional revisions to the design standards are proposed in Exhibit 1 that:

- Clarify when the design standards are applicable to redevelopment.
- Clarify which standards are applicable to single-family residences, duplexes, accessory dwelling units (ADUs), and accessory living quarters (ALQs).
- Increase the maximum allowed building height to 45-feet and adds mitigation measures to maintain pedestrian scale.
- Incentivize alternatives to surface parking and add more flexibility to parking standards.
- Clarify standards regarding alley access.
- Clarify when some standards are required or suggested.
- Clarify headings and organization to improve readability.

To make the Design Standards for the Community of Kingston easier to access, they are proposed to be added to the Kitsap County Code as an appendix to Title 17 (see Amendment #2).

Amendment #1 – Remove Redundant UVC Design Standards (KCC)

A. Kitsap County Code Section 17.470.020 ‘Applicability – How to use the design criteria’, adopted by Ordinance 534 (2016), is amended as follows:

17.470.020 Applicability – How to use the design criteria.

A. Applicability.

- The “requirements sections” in the following design criteria apply to each multifamily project requiring conditional use review under Chapter 17.540 or 17.550.

~~2. In addition to the requirements set forth in this chapter, the “requirements sections” set forth in Sections 17.480.160 and 17.480.180 to 17.480.240 shall apply to each multifamily project requiring review under subsection (A) of this section.~~

Commented [PB1]: Informational Note:
These sections are moved into this chapter below and will remain in effect.

1 B. How to Use the Design Criteria. The “requirements sections” state the design criteria
 2 that each project shall meet. These design criteria are intended to supplement the
 3 development standards of the UCR, UM and UH zones. Where the provisions of this
 4 chapter conflict with the provisions of Chapters 17.210 (UCR), 17.220 (UM),
 5 and 17.230 (UH), the provisions of the zoning district shall apply. The “guidelines”
 6 which follow each requirement statement are suggested ways to achieve the design intent.
 7 Each guideline is meant to indicate the preferred conditions, but other equal or better
 8 design solutions will be considered acceptable by the director or hearing examiner, so
 9 long as these solutions meet the intent of these sections. They are to be applied with an
 10 attitude of flexibility, recognizing that each development site and project will have
 11 particular characteristics that may suggest that some guidelines be emphasized and others
 12 de-emphasized. However, while alternative solutions can be proposed, none of the
 13 criteria in the requirement statements can be disregarded.

14
 15
 16 B. Kitsap County Code Section 17.480.160 ‘Multifamily – Site design – Parking location and
 17 design’, adopted by Ordinance 534 (2016), is recodified as Kitsap County Code Section
 18 17.470.090.

19
 20
 21 C. Kitsap County Code Section 17.480.180 ‘Multifamily – Site design – Screening’, adopted by
 22 Ordinance 534 (2016), is recodified as Kitsap County Code Section 17.470.100.

23
 24
 25 D. Kitsap County Code Section 17.480.240 ‘Multifamily – Signs’, adopted by Ordinance 534
 26 (2016), is recodified as Kitsap County Code Section 17.470.110.

27
 28
 29 E. Kitsap County Code Chapter 17.480 ‘Urban Village Center Design Criteria’, last amended by
 30 Ordinance 550 (2018), is repealed.

31
 32
 33 **Amendment #2 – Revise and Publish Kingston Design Standards in Kitsap County Code**
 34 **(KCC)**

35
 36 A. The Design Standards for the Community of Kingston, adopted by Ordinance 250 (2000), are
 37 amended as provided in Exhibit 1.

38
 39
 40 B. NEW SECTION. The Design Standards for the Community of Kingston, as amended in
 41 Section A above, are added (as a linked PDF document in its original formatting) as
 42 Appendix C1 to Kitsap County Code Chapter 17.700 ‘Appendices’ as follows.

43
 44 Appendix C1 - Design Standards for the Community of Kingston
 45
 46

Amendment #3 – Update Internal References to the Design Standards (KCC)

A. Kitsap County Code Section 17.260.020 ‘Uses permitted and design standards’, last amended by Ordinance 550 (2018), is revised as follows:

17.260.020 Uses permitted and design standards.

- A. Uses Permitted: Section 17.410.044, Commercial, industrial, parks and public facility zones use table.
- B. Design Standards: Section 17.420.054, Commercial, industrial, and parks zones density and dimensions table.
 - 1. Density;
 - 2. Lot dimensions;
 - 3. Lot coverage standards;
 - 4. Height regulations;
 - 5. Setbacks.
- C. Chapter 17.105, Interpretations and Exceptions.
- D. Chapter 17.440, Master Planning.
- E. Chapter 17.450, Performance Based Development.
- ~~F. Chapter 17.480, Urban Village Center Design Criteria.~~
- ~~F G.~~ Chapter 17.490, Off-Street Parking and Loading.
- ~~G H.~~ Chapter 17.500, Landscaping.
- ~~H I.~~ Chapter 17.510, Sign Code.
- ~~I J.~~ Chapter 17.580, Transfer of Development Rights.

B. Kitsap County Code Section 17.410.050 ‘Footnotes for zoning use tables’, last amended by Ordinance 550 (2018), is revised as follows:

17.410.050 Footnotes for zoning use tables.

- A. Where noted on the preceding use tables, the following additional restrictions apply:
 - ...
 - 30. The Design Standards for the Community of Kingston sets forth policies and regulations for properties within the downtown area of Kingston. All development within this area must be consistent with these standards in KCC 17.700.C1. ~~A copy of the Design Standards for the Community of Kingston may be referred to on the Kitsap County web page or at the department of community development front counter.~~

[Note: This amendment is shown in an abbreviated form to keep this document concise. The rest of the footnotes in this section will be inserted, without further amendment, into the final ordinance prior to adoption.]

1 C. Kitsap County Code Section 17.420.060 ‘Footnotes for tables’, last amended by Ordinance
2 559 (2018), is revised as follows:

3
4 **17.420.060 Footnotes for tables.**

5 A. Where noted on the preceding tables, the following additional provisions apply:

6 ...

7 5. The Design Standards for the Community of Kingston sets forth policies and
8 regulations for properties within the downtown area of Kingston. All development within
9 this area must be consistent with these standards in KCC 17.700.C1. ~~A copy of the
10 Design Standards for the Community of Kingston may be referred to on the Kitsap
11 County web page or at the department of community development front counter.~~

12
13 *[Note: This amendment is shown in an abbreviated form to keep this document concise.
14 The rest of the footnotes in this section will be inserted, without further amendment, into
15 the final ordinance prior to adoption.]*

16
17
18 **Amendment #4 – Move Definitions from Design Standards to Title 17 - Zoning (KCC)**

19
20 A. Kitsap County Code Section 17.110.570 ‘Parking space, compact’, adopted by Ordinance
21 534 (2016), is renumbered as Section 17.110.567.

22
23
24 B. NEW SECTION. A new section is added to Kitsap County Code Chapter 17.110
25 ‘Definitions’, as follows:

26
27 **KCC 17.110.568 Pedestrian-oriented facade.**

28 “Pedestrian-oriented facade” means the ground floor frontage of a building design, which
29 offers an interesting appearance to attract pedestrian interest in the locality and
30 encourages pedestrian access.

31
32
33 C. NEW SECTION. A new section is added to Kitsap County Code Chapter 17.110
34 ‘Definitions’, as follows:

35
36 **KCC 17.110.569 Pedestrian-oriented space/plaza.**

37 “Pedestrian-oriented space/plaza” means the area between a building and a public street
38 or pedestrian path that promotes visual and pedestrian access onto the site and that
39 provides amenities and landscaping to enhance the public’s use of the space for passive
40 activities, such as resting, reading, picnicking, and window shopping. The area should be
41 visible from the public right-of-way and accessible to pedestrians, including those with
42 handicaps.

1 D. NEW SECTION. A new section is added to Kitsap County Code Chapter 17.110
2 'Definitions', as follows:

3
4 **KCC 17.110.570 Pedestrian-friendly street.**

5 "Pedestrian-friendly street" means any street designed for safe use by both pedestrians
6 and vehicles. A pedestrian-friendly street includes sidewalks or walkways, landscaping,
7 lighting, and other street amenities benefiting pedestrians.
8

9
10 E. NEW SECTION. A new section is added to Kitsap County Code Chapter 17.110
11 'Definitions', as follows:

12
13 **KCC 17.110.571 Pedestrian walkways.**

14 "Pedestrian walkways" means formal standardized public walkways and informal paths
15 worked into a site's landscape design that provide a means for pedestrians to travel
16 through the community along street sidewalks or other public routes.
17

18
19 F. NEW SECTION. A new section is added to Kitsap County Code Chapter 17.110
20 'Definitions', as follows:

21
22 **KCC 17.110.697 Streetscape.**

23 "Streetscape" means the visual and functional supporting elements of a roadway design
24 that provide aesthetic interest and comfort to the pedestrian. Street amenities serve to
25 define the public space of a sidewalk as well as the adjacent roadway corridor.
26 Pedestrian amenities include pedestrian-oriented plazas, furniture, lighting, and art.
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Parking Standards

Introduction

“Pedestrian-oriented” is the central theme of the existing adopted vision for downtown Kingston. With regarding to parking, this means:

- Surface parking is minimized.
- Parking is largely located underground, in parking garages, or shared-use lots.
- A substantial amount of parking could be provided and managed through a future public parking management program, parking improvement district, or other public entity.
- A critical mass of residential and commercial uses are supported by high capacity transit and a high proportion of bicycle and pedestrian modes for short trips, thus requiring less parking.

The proposed amendments are intended to revise current parking standards, which were generally not crafted for a pedestrian-oriented downtown, so they better align with the existing vision for downtown Kingston.

Kingston will be designated a High Capacity Transit Community in the Central Puget Sound regional growth management plan, called VISION 2050, which is intended to coordinate growth management across Kitsap, Pierce, Snohomish, and King Counties. High capacity transit is a key element of the plan’s framework and most of downtown Kingston is within a high capacity transit station area, which is the area within a half-mile of the ferry terminal. The proposed amendments also intend to incorporate the high capacity transit framework into the County’s parking standards, which would be immediately applicable to Kingston and could be applicable to other specified areas of unincorporated Kitsap County as high capacity transit is planned and implemented in the future.

Amendment #5 – Define High Capacity Transit Station Areas (KCC)

A. NEW SECTION. A new section is added to Kitsap County Code Chapter 17.110 ‘Definitions’, as follows:

KCC 17.110.340 High Capacity Transit Station Area.

“High capacity transition station areas” include only those portions of urban growth areas within:

A. One-half mile of the following public ferry terminals:

1. Kingston – Washington State Ferry and Kitsap Transit Fast Ferry (see boundary in KCC 17.700.E1).

B. NEW SECTION. The Kingston High Capacity Transit Station Area Map, provided in Exhibit 2, is added (as a linked PDF document in its original formatting) as Appendix E1 to Kitsap County Code Chapter 17.700 ‘Appendices’ as follows:

Appendix E1 – High Capacity Transit Station Area - Kingston

Amendment #6 – Location of Parking Facilities & On-Street Parking (KCC)

A. Kitsap County Code Section 17.490.020 ‘General provisions’, adopted by Ordinance 540 (2016), is amended as follows:

17.490.020 General provisions.

A. Parking analyses shall be provided for all proposed uses as outlined on relevant permit application checklists.

B. More Than One Use on One or More Parcels. In the event several uses occupy a single structure or parcel of land, the total requirements for off-street parking shall be the sum of the requirements of the several uses computed separately. If the director finds that a portion of the floor area not less than a contiguous one hundred square feet in a retail store will be used exclusively for storage of merchandise which is not being displayed for sale, he may deduct such space in computing parking requirements, but the owners shall not thereafter use the space for any other purpose without furnishing additional off-street parking as required by Section 17.490.030.

C. Joint Use of Facilities. The off-street parking requirements of two or more uses, structures, or parcels of land may be satisfied by the same parking or loading space used jointly, if approved by the director, to the extent that it can be shown by the owners or operators of the uses, structures, or parcels that their operations and parking needs do not overlap in point of time. ~~If the uses, structures, or parcels are under separate ownership, the right to joint use of the parking space must be evidenced by a deed, lease, contract, or other appropriate written document to establish the joint use.~~ For joint use facilities (i.e. shared-use parking and shared access facilities), a parking agreement shall be required consistent with KCC 17.490.020(I).

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D. **Location of Parking Facilities.**

Workgroup Recommendation	<p>1. <u>In all zones (except the Urban Village Center), off-street parking spaces for dwellings shall be located on the same lot with the dwelling. Other required parking spaces shall be located on the same parcel or on another parcel not farther than three hundred feet from the building or use they are intended to serve, measured in a straight line from the building. <u>Off-premise parking arrangements must be noticed in the title of the development site and off-premise parking site and obtaining such arrangement is the sole responsibility of the applicant.</u></u></p> <p>2. <u>In the Urban Village Center zone, required parking spaces shall be located on the same parcel or on another parcel no farther than eight hundred feet from the building or use they are intended to serve, measured in a straight line from the building. Off-premise parking arrangements must be noticed in the title of the development site and off-premise parking site and obtaining such arrangement is the sole responsibility of the applicant.</u></p>
DCD Recommendation	<p>1. <u>Within high capacity transit station areas, required parking spaces shall be located on the same parcel or on another parcel no farther than eight hundred feet from the building or use they are intended to serve, measured in a straight line from the main entrance of the building.</u></p> <p>2. <u>In all other areas, off-street parking spaces for dwellings shall be located on the same lot with the dwelling. Other required parking spaces shall be located on the same parcel or on another parcel not farther than three hundred feet from the building or use they are intended to serve, measured in a straight line from the building the main entrance of the building.</u></p> <p>3. <u>Off-site parking shall be connected to the building or use they are intended to serve by streets improved with sidewalks or by walkways.</u></p> <p>4. <u>For off-site parking, a parking agreement shall be required consistent with KCC 17.490.020(I).</u></p>

Commented [PB2]: Informational Note:
In a high-capacity transit and pedestrian-oriented downtown, parking is intentionally limited and managed to reflect lower car ownership, less land area for parking, and more trips completed through walking and biking instead of moving cars short distances. Therefore, off-site parking can be further from its associated use. 800 feet is approximately 2.5 blocks in downtown Kingston.

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E. Use of Parking Facilities. Required parking space shall be available for the parking of operable passenger automobiles of residents, customers, patrons, and employees only, and shall not be used for the storage of vehicles or materials, or for the parking of trucks used in conducting the business or use.

F. Parking in Required Front, Side, Rear Yards or Setbacks. Unless otherwise provided, required parking and loading spaces shall not be located in a required yard or setback, except for development of single-family dwellings or duplexes. Automobile sales may be allowed in no more than twenty-five percent of the front yard setback, as shown on an approved site plan.

G. Off-site Employee Parking. Off-site employee parking may be used to reduce the number of on-site parking spaces. ~~Off site parking for employees shall be evidenced by a deed, lease, contract or other appropriate written document.~~

Commented [PB3]: Informational Note:
This sentence replaced by subsection D.4 in the DCD recommended alternative above.

1 H. Development of and Maintenance Standards for Off-Street Parking Areas. In
2 addition to requirements of Chapters 17.490 and 17.500 and the Kitsap Stormwater
3 Design Manual, every parcel of land hereafter used as a public or private parking area,
4 including commercial parking lots, shall be developed as follows:

5 1. An off-street parking area for more than five vehicles shall be effectively
6 screened by a sight-obscuring fence, hedge, or planting, on each side that adjoins
7 property situated in any residential zone, or the premises of any school or like
8 institution;



9 2. Lighting shall be directed away from adjoining properties. Not more than one
10 foot candle of illumination shall leave the property boundaries;

11 3. Except for single-family and duplex dwellings, groups of more than two parking
12 spaces shall be so located and served by a driveway that their use will require no
13 backing movements or other maneuvering within a street or right-of-way other than
14 an alley;

15 4. Areas used for standing and maneuvering of vehicles shall have durable and
16 dustless surfaces maintained adequately for all-weather use, and so drained as to
17 avoid flow of water across sidewalks;

18 5. Except for parking to serve residential uses, parking and loading areas adjacent
19 to or within residential zones or adjacent to residential uses shall be designed to
20 minimize disturbance of residents;

21 6. Service drives to off-street parking areas shall be designed and constructed to
22 facilitate the flow of traffic, to provide maximum safety of traffic ingress and egress,
23 and to provide maximum safety of pedestrians and vehicular traffic on the site. The
24 number of service drives shall be limited to the minimum that will allow the property
25 to accommodate and service the traffic to be anticipated. Service drives shall be
26 clearly and permanently marked and defined through the use of rails, fences, walls, or
27 other barriers or markers on frontage not occupied by service drives. Service drives
28 to drive-in establishments shall be designed to avoid backing movements or other
29 maneuvering within a street, other than an alley;

30 7. Service drives shall have a minimum vision clearance area formed by the
31 intersection of the driveway centerline, the street right-of-way line, and a straight line
32 joining said lines through points twenty feet from their intersection;

33 8. Parking spaces along the outer boundaries of a parking area shall be contained by
34 a curb or bumper rail so placed to prevent a motor vehicle from extending over an
35 adjacent property line, pedestrian walkway, or a street; and

36 9. When the parking standards require ten or more parking spaces, up to thirty
37 percent of these may be compact car spaces, as identified in Section 17.490.040.
38 Compact spaces shall be clearly labeled on the parking space.



10. Parking for bicycles should be provided at a ratio of one space per ten vehicle spaces, and shall be required at a ratio of one space per twenty vehicle spaces. Bicycle facilities shall be adjacent to buildings and protected from weather.

I. Parking Agreement.

1. For off-site parking, shared-use parking, or shared access to parking, a covenant, easement or other contract approved by the director for shared parking and/or access between the cooperating property owners shall be enacted and recorded by the County with the County Auditor as a deed restriction on all associated properties (i.e. the property with the use and the property providing the required parking) that cannot be modified or revoked without the approval of the director. The parking agreement shall:

a. Provide that the land comprising the required parking facilities shall not be encroached upon, used, sold, leased, or conveyed for any purpose except in conjunction with the building or use which the required parking serves;

b. For commercial uses, provide for directional signage to off-site public or visitor parking.

c. Assign maintenance provisions for the parking facilities and landscaping;

d. If shared use is allowed, indicate prime hours of operation for shared uses;

e. If shared use is allowed, designate potential times of overflow, and a parking plan which will be implemented in the event of overflow.

2. If any of the above requirements are violated, the affected property owners must provide the full amount of required off-street parking for each use, in accordance with conditions of approval, unless a satisfactory alternative remedy is approved by the director.

Commented [PB4]: Informational Note:
This new provision is adapted from the King County Metro Right Size Parking Model Code.

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Workgroup Rec.	J. On-Street Parking Areas. In the Urban Village Center zone, on-street parking spaces within the right-of-way adjacent to the lot may be counted to satisfy the minimum off-street parking requirements for commercial development, as approved by the Department of Public Works.
DCD Rec.	[DCD recommends moving this proposed language, with minor edits, to Chapter III, Section B.2 (Parking) of the Design Standards for the Community of Kingston (see Exhibit 1) where it would be applicable to all three of the downtown Kingston design districts instead of just the UVC zone.]

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Amendment #7 – Number of Spaces Required (KCC)

Kitsap County Code Section 17.490.030 ‘Number of spaces required’, last amended by Ordinance 541 (2017), is amended as follows:

17.490.030 Number of spaces required.

Off-street parking spaces shall be provided as follows:

<i>Note: These 2 columns contain existing language (with proposed changes).</i>		<i>Note: These 2 columns provide alternatives for what would be 1 additional column in this table.</i>	
		Workgroup Rec.	DCD Rec.
Land Use	Minimum Parking Spaces Required in all Zones (Except as Modified to the Right)	Urban Village Center Zone Modifications	High Capacity Transit Station Area Modifications
Residential			
Single-Family (attached or detached)	During subdivision, 2 per unit + 0.5 per unit on street or set aside; for historical lots or lots with no standing requirement, 3 per unit. 1 additional space for accessory dwelling units or accessory living quarters. Garages are not calculated towards any parking requirement.	2 per unit. 1 additional space per guest house, accessory dwelling unit or accessory living quarter. Garages are calculated towards parking requirement.	2 per unit. 1 additional space per guest house, accessory dwelling unit or accessory living quarter. Garages are calculated towards parking requirement.


Commented [PB5]: Informational Note:
In a high-capacity transit and pedestrian-oriented downtown, parking is intentionally limited and managed to reflect lower car ownership and less land area for parking. Parking spaces in garages would be counted like they are for multi-family housing.

Multifamily (Condos/Townhouses/Apartments) and Cottage Housing	1.5 per unit + 0.5 per unit on street or set aside	<u>1 space per dwelling unit that is a studio or 1 bedroom unit, and 2 spaces for all other dwelling units.</u>	<u>Units with 1 or fewer bedrooms: 1 space per unit + 0.5 spaces per unit set aside;</u> <u>Units with 2 or more bedrooms: 1.5 spaces per unit + 0.5 spaces per unit set aside</u>
Senior Housing	0.5 per unit; 1 per on-duty employee		
Institutional/Educational/Other			
Bed and Breakfast	1 per sleeping unit		
Motels and Hotels	1 per bedroom; and spaces to meet the combined requirements of the uses being conducted such as hotels, restaurants, auditoriums, etc.		
Club/Lodges	Spaces to meet the combined requirements of the uses being conducted such as hotels, restaurants, auditoriums, etc.		
Hospitals and Institutions	1 per bed; 1 per 2 employees; 1 per 2 guests		
Places of Worship	1 per 4 seats or 8 feet of bench length in the main auditorium		
Library and Gallery	1 per 250 gross square feet		
Preschool-Kindergarten	1 per employee; 1 per 6 children		
Elementary/Middle or Junior High School	1 per employee; 2 per classroom		
High School	1 per employee and teacher; 1 per 10 students		
Colleges, Technical School	1 per 3 seats in classroom; 1 per employee and teacher		
Stadium, Arena, Theater	1 per 4 seats or 8 feet of bench length in the main auditorium		

Commented [PB6]: Informational Note:
Differentiates parking by unit size to reflect car ownership patterns and encourage a mix of unit sizes, diversify housing inventory, and make smaller units more affordable.

Commented [PB7]: Informational Note:
Specifies a portion of parking as set aside to encourage shared parking spaces and improve parking lot efficiency.


Bowling Alley	6 per alley		
Dance Hall, Skating Rink	1 per 200 gross square feet		
Self Storage	1 per 3,000 gross square feet		
Commercial/Retail/Office			
Restaurants/Bars/ Taverns	If under 5,000 square feet of gross floor area – 1 per 200 square feet of gross floor area; If 5,000 or more square feet of gross floor area – 20 plus 1 per each additional 200 square feet of gross floor area	1 per 400 square feet of gross floor area	1 per 400 square feet of gross floor area
Retail stores generating relatively little automobile traffic (e.g., appliance, furniture, hardware and repair stores)	1 per 400 square feet of gross floor area		1 per 800 square feet of gross floor area
Retail and personal service establishments generating heavy automobile traffic (e.g., department, drug, and auto parts stores, fitness centers, supermarkets, ice cream parlors, bakeries and beauty and barber shops)	1 per 200 square feet of gross floor area	1 per 400 square feet of gross floor area	1 per 400 square feet of gross floor area
Espresso Stands, Drive-In, and Fast Food Restaurants	1 per 80 square feet of gross floor area		
Professional Office	1 per 300 square feet of gross floor area	1 per 400 square feet of gross floor area	1 per 400 square feet of gross floor area
Shops and stores for sales, service	1 per 600 square feet of gross floor area		

or repair of automobile, machinery and plumbing, heating, electrical and building supplies			
Mortuaries, Funeral Homes, Crematories	1 per 75 square feet of assembly area	1 per 400 square feet of gross floor area	<i>[Note: DCD recommends keeping as is]</i>
Medical and Dental Office or Clinic	1 per 200 square feet of gross floor area	1 per 300 square feet of gross floor area	1 per 300 square feet of gross floor area
Bank, Financial Institutions	1 per 400 square feet of gross floor area		
Industrial			
Marinas and Moorage Facilities	1 per 4 moorage slips		
Warehouse, Storage, and Wholesale Facilities	1 per 2 employees; 1 per company vehicle parked on site at night (if applicable); 1 per 300 square feet of office space	1 per 2 employees; 1 per company vehicle parked on site at night (if applicable); 1 per 400 square feet of gross floor area	1 per 2 employees; 1 per company vehicle parked on site at night (if applicable); 1 per 400 square feet of office space
Manufacturing, Research, Testing, Processing and Assembly Facilities	1 per 1,000 square feet		
Winery/Brewery	1 per 800 square feet of gross floor area		

Commented [PB8]: Informational Note:
Changes the Working Group's proposal from "gross floor area" to "office space".

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- 1 A. ~~Relaxation of Deviation from~~ Required Spaces.
 2 1. The director may authorize a reduction up to twenty-five percent to the amount of
 3 required ~~off-street~~ parking if a project proponent demonstrates that, due to the unusual
 4 nature of the proposed use, it is reasonable that the ~~off-street~~ parking required by this
 5 section exceeds any likely need, or that trip demand reduction programs or public transit
 6 availability serves to further reduce parking demand.
 7

Workgroup Rec.	<p>2. <u>In all zones (except the Urban Village Center) an increase over ten percent or a reduction greater than twenty-five percent from the minimum parking ratio shall be processed pursuant to Chapter 17.560.</u></p> <p>3. <u>In the Urban Village Center zone, a reduction greater than twenty-five percent from the minimum parking ratio may be granted by the director if the reduction is supported by a parking and traffic impact statement and the development:</u></p> <ul style="list-style-type: none"> a. <u>Is within a seven-minute walk to a regional transit or ferry terminal and transit-supportive elements are provided (for example, participation in Kingston Ride or other Kitsap Transit program, covered bus stop or rideshare waiting area, covered bike parking, or car share program offered to residents); or</u> b. <u>Includes construction of a structured parking facility (e.g. an  r building or multi-level parking garage); or</u> c. <u>Utilizes spaces in a joint-use structured parking facility; or</u> d. <u>Utilizes underground parking to achieve the off-street parking requirement.</u>
DCD Rec.	<p>2. An increase over ten percent or a reduction greater than twenty-five percent from the minimum required parking ratio shall be processed pursuant to Chapter 17.560-except in high capacity transit station areas a reduction greater than twenty-five percent may be granted by the director if the reduction is supported by a parking and traffic impact analysis and the development:</p> <ul style="list-style-type: none"> a. Provides a car share program, shuttle program, or regional transit pass/subsidy program to all residents that is adequate to offset the parking reduction; b. Implements recorded lease/deed restrictions that limit the combined total number of vehicles owned by tenants to the number of parking spaces available for tenants; c. Participates in a public parking management program or a parking improvement district with adequate capacity to offset the parking reduction; d. Implements an alternative Transportation Management Plan with measures adequate to offset the parking reduction that has been approved by the director and recorded on the title of the affected properties; or c. Substantially replaces on-site surface parking with parking underground and/or in a structured parking facility (e.g. an under building or multi-level parking garage) located on-site and/or, if otherwise allowed, off-site.

Commented [PB9]: Informational Note:
 This new provision is adapted from the Workgroup recommendation and the King County Metro Right Size Parking Model Code.

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Comment 4.1

CITY OF PORT ORCHARD
Development Director

216 Prospect Street, Port Orchard, WA 98366
Voice: (360) 876-4991 • Fax: (360) 876-4980
nbond@cityofportorchard.us
www.cityofportorchard.us

August 30, 2019

Peter Best, Senior Planner
Kitsap County Dept of Community Development
619 Division Street
Port Orchard, WA 98366

Re: 2019 Kitsap County Comprehensive Plan Amendment
Silverdale/Kingston UUGA Association and Future Incorporation

Dear Mr. Best:

The City received the above-referenced Notice of Application (NOA) and SEPA Checklist, and has reviewed the comprehensive plan amendment and staff report available to the public. The City would like to express its support for this amendment, which associates the unincorporated urban growth areas of Silverdale and Kingston with future cities that are expected to incorporate within the 20-year planning horizon. This is consistent with the discussions between Kitsap jurisdictions concerning Vision 2050 allocations, and our request to PSRC to shift population allocations as part of VISION 2050 and to reclassify Kingston as a High Capacity Transit Community.

Thank you for considering the City's comments on this amendment to the Kitsap County comprehensive plan. Please feel free to contact me if you have any questions.

Sincerely,

A handwritten signature in black ink, appearing to read 'NB', with a stylized flourish extending to the right.

Nicholas Bond, AICP
City Development Director

January 21, 2020

Peter Best, Senior Planner
Planning and Environmental Programs
Kitsap County Department of Community Development
619 Division Street MS-36
Port Orchard, WA 98366-4682

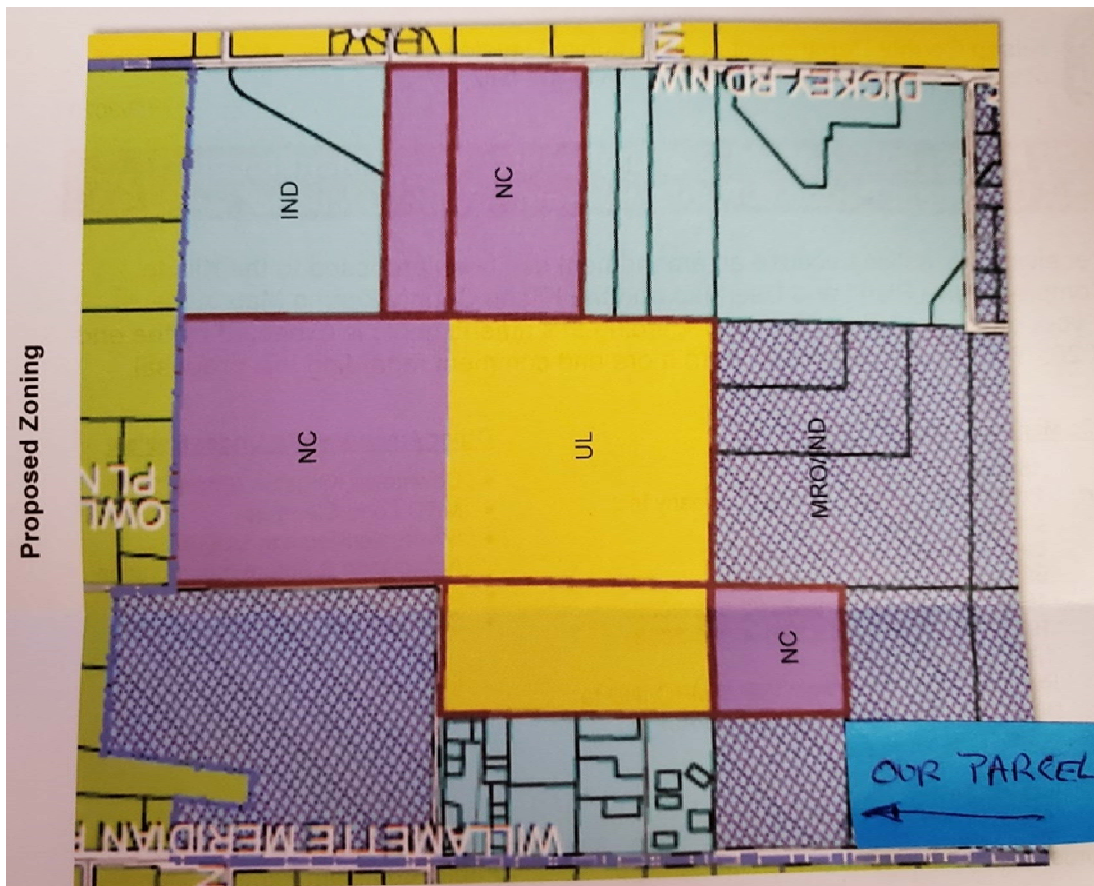
Subject: 2019 Annual Comprehensive Plan Amendment - MRO

Dear Peter,

This letter is submitted to provide our comments regarding the MRO Zone Clean-up being considered on one of our properties. I have also provided a separate letter in response to "Site-Specific Map Amendment in Your Area" – Permit # 18-00495 (as identified in the same county notice).

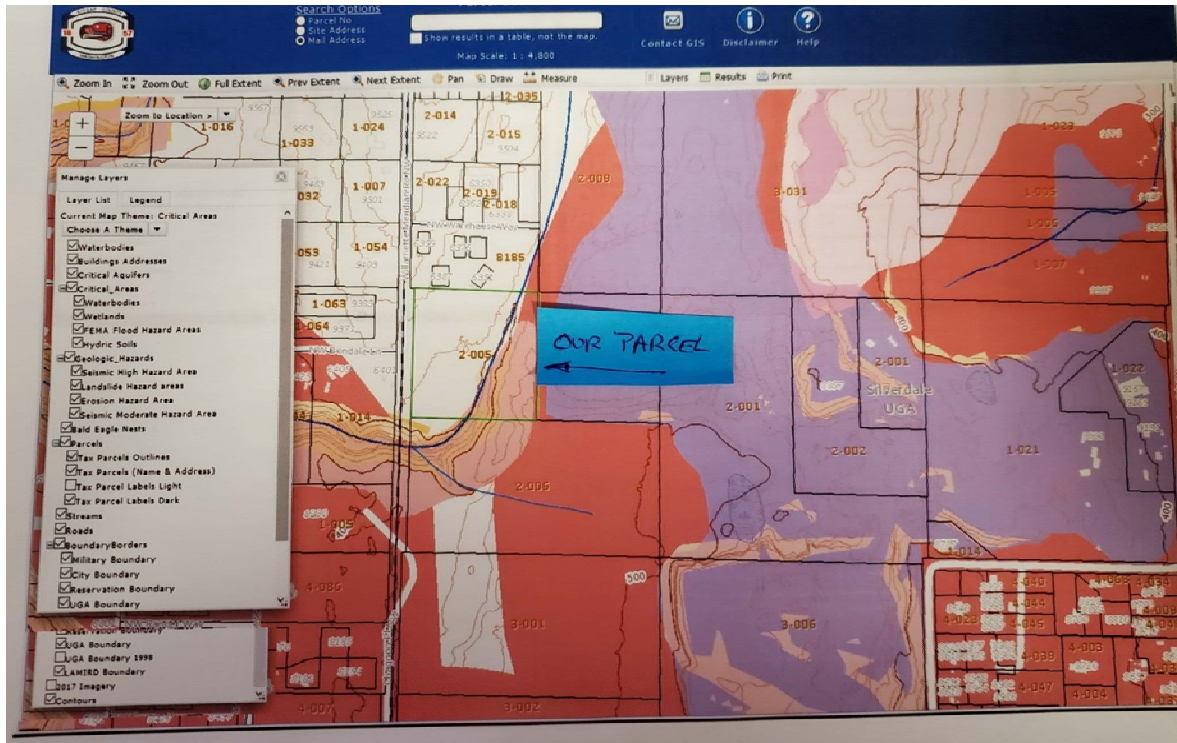
We are strongly opposed to our property being classified as MRO.

Our tax id number is 192501-2-005-2006. We purchased this property, as industrial zoned property, in 2017. Our North property line abuts an existing industrial park. Our East line abuts property being considered for a residential rezone (permit #18-00495) and our West Property line abuts Willamette Meridian. Our Southern property line abuts property that is currently Industrial / MRO.



Comment 5.1

More than half of our Southern border includes a creek and critical areas. In addition, Silverdale Water has run a large water main through our property which supports the neighborhood to our North. Based on these attributes, it is not feasible to develop this property as MRO. This property is best suited for the Industrial Zoning (as it was designated when we purchased the property) or Residential (5-9 DU/Ac or 10-30 DU/Ac) Zoning. This is consistent with what has been developed on the properties to the North.



Our hope is that your office reviews the critical areas and waterbody overlays, as well as, visit with Silverdale Water to review their concerns on their existing water main.

We request this property remain in Industrial Zoning or 5-9 DU/Ac and 10-30 DU/Ac zoning which is currently being considered for the property to our East. At a minimum, we believe the MRO designation is not appropriate for this property and the designation needs to be removed. MRO development is not economically feasible and not possible without significant ecological damage to the critical areas surrounding this property.

Sincerely,

Dave Wixson
Manager
DCRW Properties LLC



William T. Lynn
Direct: (253) 620-6416
E-mail: blynn@gth-law.com

April 12, 2019

Jim Bolger
Assistant Director
Kitsap County Department of Community
Development
619 Division Street, MS-36
Port Orchard, WA 98366

Laura Zippel
Deputy Prosecuting Attorney
Kitsap County Prosecutor's Office
614 Division Street, MD-34A
Port Orchard, WA 98366

RE: Sagra, Inc. - 5997 NW Altitude Lane

We represent Sagra, Inc. and write on its behalf concerning the building at the above-referenced address. The building was built under a building permit lawfully issued by Kitsap County and was constructed in accordance with the requirements of that permit. A copy of the Certificate of Occupancy issued September 20, 2018 is attached to this letter. Sagra's president, Rick Smith, was recently advised that the unoccupied portions of the building could not be used for the intended industrial/warehouse purpose because the property is located within the mineral resource overlay (MRO) and the use is not permitted in that designation. Mr. Smith was also told the County might issue a tenant-specific temporary allowance for a particular use. Neither of these is an acceptable or lawful outcome. Sagra is permitted by law to use the building for its intended use.

The important point is that the County building permit was issued and no appeals were taken from the issuance of that permit. That means that the building and its intended use are no longer subject to challenge or modification as a matter of law under the Land Use Petition Act (LUPA), RCW Ch. 36.70C. The building permit was subject to LUPA because it was a "governmental approval required by law before real property may be improved, developed, modified, sold, transferred, or used." RCW 36.70C.020(2)(a) Under very well established legal authorities, such a decision not challenged becomes final even if a local government later decides it should not have issued the approval or wishes to add additional restrictions. *Chelan County v. Nykreim*, 146 Wn.2d 904 (2002).

The *Nykreim* case involved an attempt by Chelan County to withdraw its previously approved boundary line adjustment when it later determined the BLA had unlawfully created an additional lot. The Supreme County overturned the attempted withdrawal despite the additional lot.

Reply to:

Tacoma Office
1201 Pacific Ave., Suite 2100 (253) 620-6500
Tacoma, WA 98402 (253) 620-6565 (fax)

Seattle Office
600 University, Suite 2100 (206) 676-7500
Seattle, WA 98101 (206) 676-7575 (fax)

Comment 5.2

Gordon Thomas Honeywell^{LLP}
April 12, 2019
Page 2

Leaving land use decisions open to reconsideration long after the decisions are finalized places property owners in a precarious position and undermines the Legislature's intent to provide expedited appeal procedures in a consistent, predictable and timely manner.

Nykreim at 933.

This is by no means a holding unique to the *Nykreim* case. *Nykreim* followed *Wenatchee Sportsman Assoc. v. Chelan County*, 141 Wn.2d 169 (2000) where the Supreme Court held that if a land use permit approval is not timely appealed it becomes immune from attack and must be given the full affect, even if it is subsequently determined that the permit was unlawfully issued. Since that time the Supreme Court and other courts have affirmed and reinforced that rule. *Habitat Watch v. Skagit County*, 155 Wn.2d 397 (2006); *James v. Kitsap County*, 154 Wn.2d 574 (2005); *Samuel's Furniture, Inc. v. Washington Dept. of Ecology*, 147 Wn.2d 440 (2003). Note that the County got the benefit of that legal concept in the *James* case.

We believe this is straight-forward. The County issued a building permit and a Certificate of Occupancy. No challenge was taken and that building may now lawfully be used for its intended purposes.

We are addressing this issue to both of you since we recognize it raises obvious legal issues. I look forward to discussing this with either or both of you as appropriate.

Very truly yours,



William T. Lynn

WTL:lb

Enclosure

cc: Rick Smith, Sagra, Inc.



Certificate of Occupancy Kitsap County

Department of Community Development

This Certificate is issued pursuant to the requirements of Section 110 of the International Building code certifying that at the time of issuance this structure was in compliance with the various ordinances regulating building construction or use. For the following:

Owner : SAGRA INC Building Permit Number : 17-01638

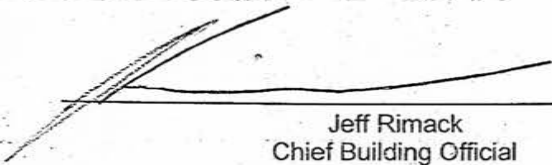
Group : B IBC/S-2 IBC Use : Warehouse Zoning : IND

Code Edition : 2015 Building Code Edition Construction Type : V-B Occupant Load : 44

Site Address : 5997 NW ALTITUDE LN Owner Address :

2336 S. Fawcett Ave.
TACOMA, WA 98402

(City/State/Zip)
9/20/18
Date Issued


Jeff Rimack
Chief Building Official

*See the building permit for any special stipulations and conditions applicable to the occupancy of this structure.